ARLINGTON COUNTY SITE PLAN REVIEW COMMITTEE ARVA APARTMENTS

2201 ARLINGTON BOULEVARD

1: Architecture

2: Transportation

PERSHING DRIVE SPECIAL GLUP (GENERAL LAND USE PLAN)

North Wayne Street/Mews Urban Design Improvements

Among the improvements which could serve to enhance this edge of the site, is a shared roadway that could serve as public access for pedestrians and bicyclists to the Arlington Boulevard Trail and a garage access point for the residences along the mews. With careful design, this area can be safe, inviting and useful for both residents of the site, residents of adjacent sites and the broader Lyon Park neighborhood.



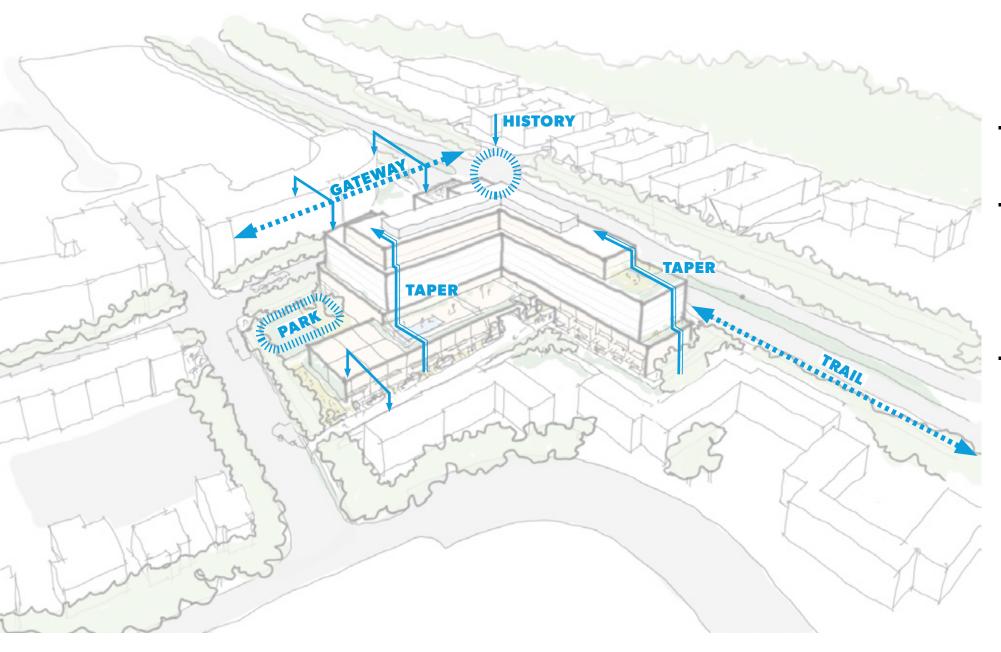
Figure 6 -19 North Wayne Street/Mews Massing

GLUP KEY GOALS

- Improve public spaces
 - New park
 - Wainwright road to trail
- Gateway to neighborhood
- Provide density while transition heights
- Embrace Site History

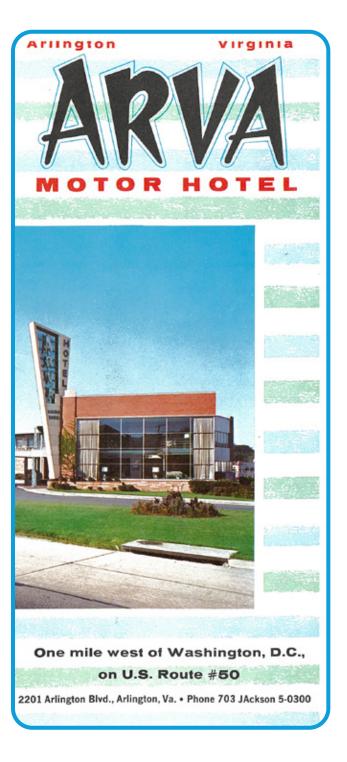
GLUP KEY GOALS

- Improve public spaces
 - New park
 - Wainwright road to trail
- Gateway to neighborhood
- Provide density while transitioning heights (taper)
- Embrace Site History

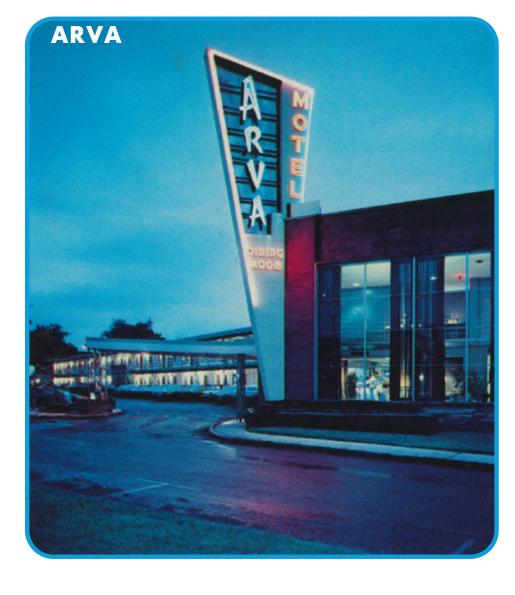


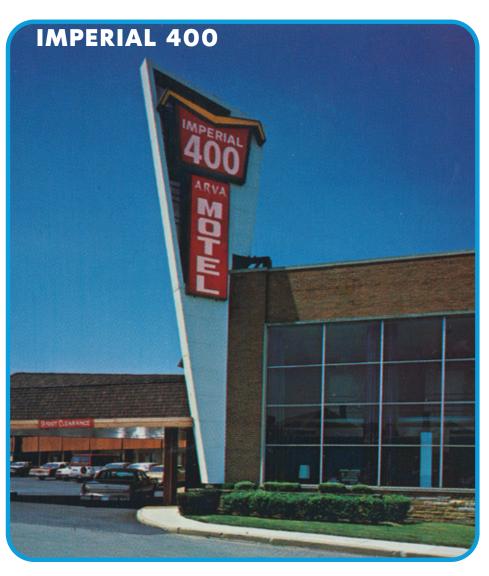
SITE PLAN













Per independent structural evaluation of facade and sign:

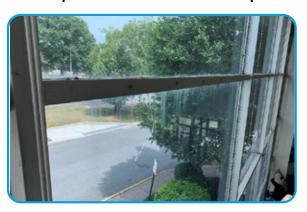
- Curtain wall mullions are insufficient to resist the wind loads required per load
- Signage interior steel angle frames have buckled out of plane due to overstress of compression load due to wind loads

STRUCTURAL EVALUATION OF HISTORIC COMPONENTS



CURTAIN WALL OBSERVATIONS:

- Cracks were found in multiple glass panels of the curtain wall and ranged from 10" long to full height
- In each glass panel, the gaskets were showing severe cracks and deterioration
- Water infiltration could be compromising the curtain wall and supporting structure
- Glass panels are tilted out of plane







BLADE SIGN OBSERVATIONS:

- Water damage could be found where the carport and signage meet
- Steel panels have corrosion at the edges of the steel panels
- At the upper portion of the sign, the panels show signs of distortion and discoloration
- At the base of the signage structure, the steel angle frame has buckled







STRUCTURAL EVALUATION OF HISTORIC COMPONENTS



CURTAIN WALL EVALUATION & RECOMMENDATIONS:

- The existing curtain wall mullions are constructed of aluminum alloy 6063-T5, with a presumed wind load design of 15 psf based on the approximate age of the building
- Using the properites of the assumed aluminum alloy and the current wind load requirements per code, the mullions are not sufficient to resist the required wind loads
- Longitudinal cracks in the curtain wall's glass panels are most likely stress cracks due to the difference in temperature of interior and exterior area and/or lateral movements of the frame under wind loads
- Water infiltration could result in deterioration of the curtain wall where it attaches to the roof structure
- McMullan recommends replacement in lieu of restoration that will provide protection against wind and water infiltration and provide better thermal properties, ultimately meeting current energy goals and design loads to ensure longevity for an additional 75 years

BLADE SIGN EVALUATION & RECOMMENDATIONS:

- The blade sign is constructed of steel panels hung from an interior steel angle frame, which is attached to the building's lobby corner column
- A steel angle at the base of the signage was observed to be buckled out of plane, most likely due to the overstress of compression load due to wind loads
- Due to the observed deterioration and warping of the sign panels, the reconstruction of the blade sign will not yield a smooth appearance, with distortions and seams visible
- McMullan recommends replacement in lieu of restoration that will strengthen the sign to resist current wind loads and ensure a uniform appearance, ultimately meeting current design loads to ensure longevity for an additional 75 years

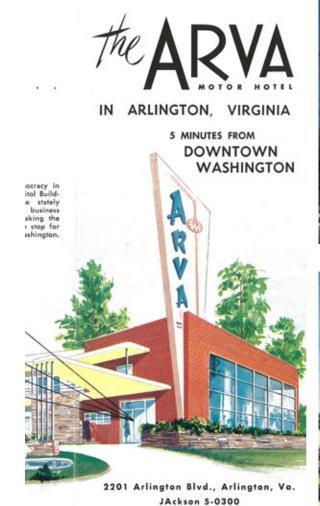
Conclusion

The elements in the GLUP Study that have been identified as having historic importance, are not in good condition due to age and typical weather exposure and will require repairs, restoration, and strengthening to reuse. The restoration of these elements is limited and will modify the original look and design of the elements. Therefore, it is recommended that these elements be replaced with new elements using current materials that meet today's code and structural load requirements, while maintaining the historic character and matching scale, proportion, and layout where possible.

Please feel free to contact our office with any questions regarding the information provided.

Sincerely,

McMullan **Consulting Engineers**















ARVA MOTEL

MODERNE STYLE

MID-CENTURY MODERN STYLE

CONTEMPORARY EXPRESSION

With regard to the <u>Moderne</u> style, architectural historian Richard Guy Wilson characterized the style: eclectic co-existence of "traditionalism and modernism"

LYON PARK CONTEXT - CO-EXISTENCE OF STYLE







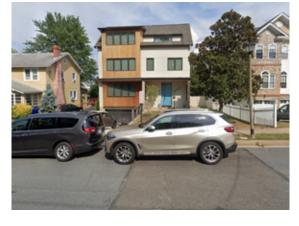




























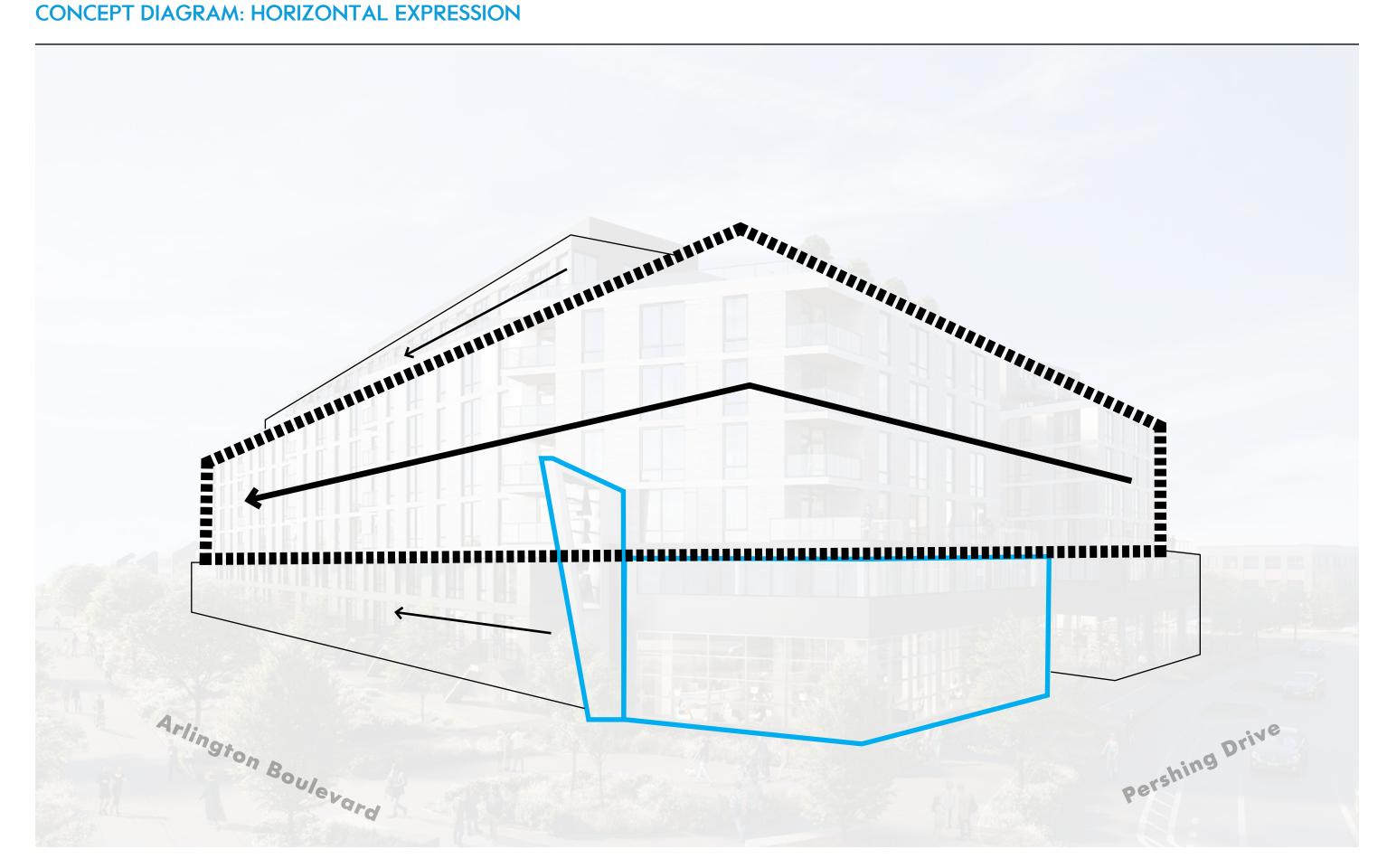




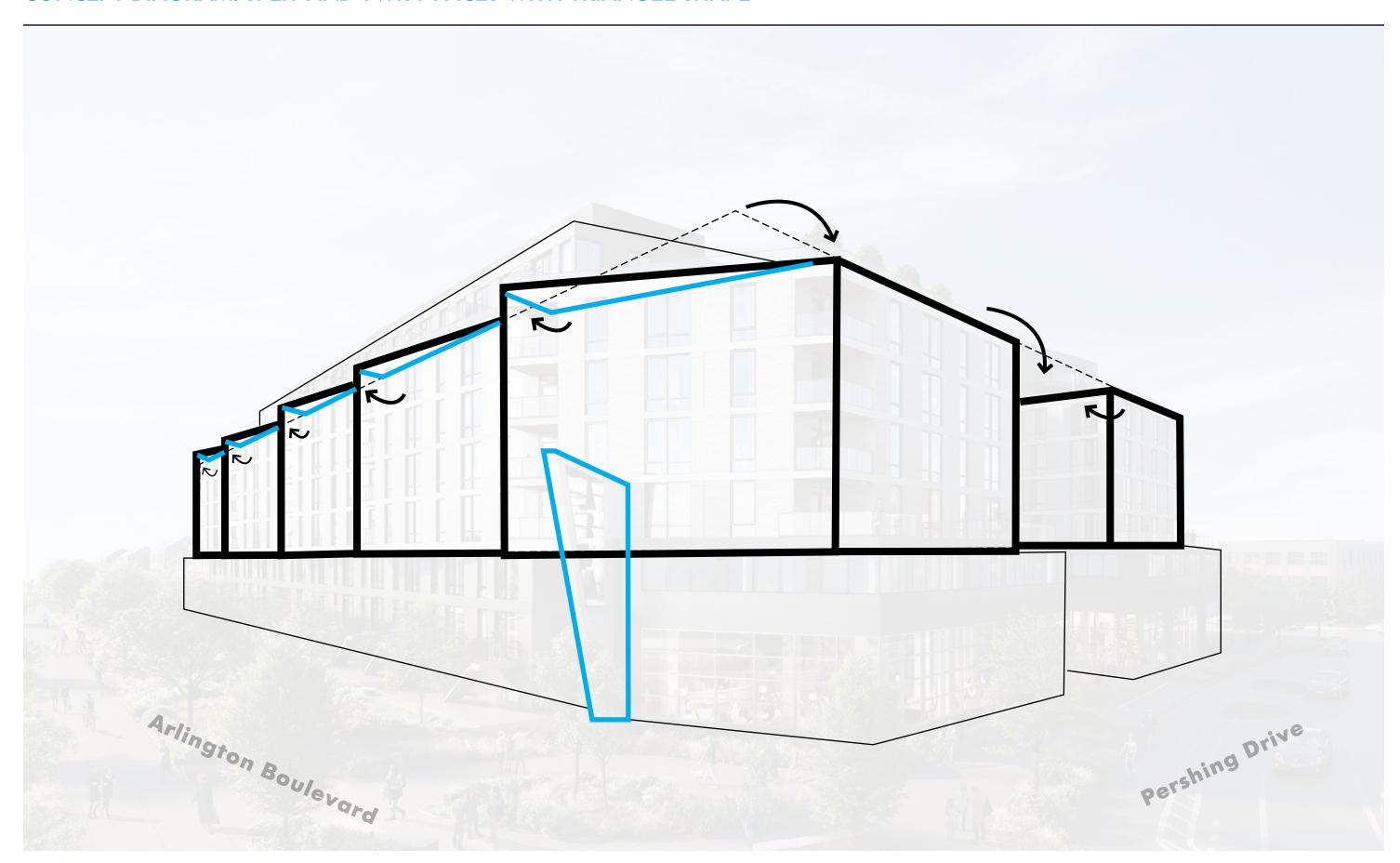


CONCEPT DIAGRAM: CELEBRATE IDENTITY

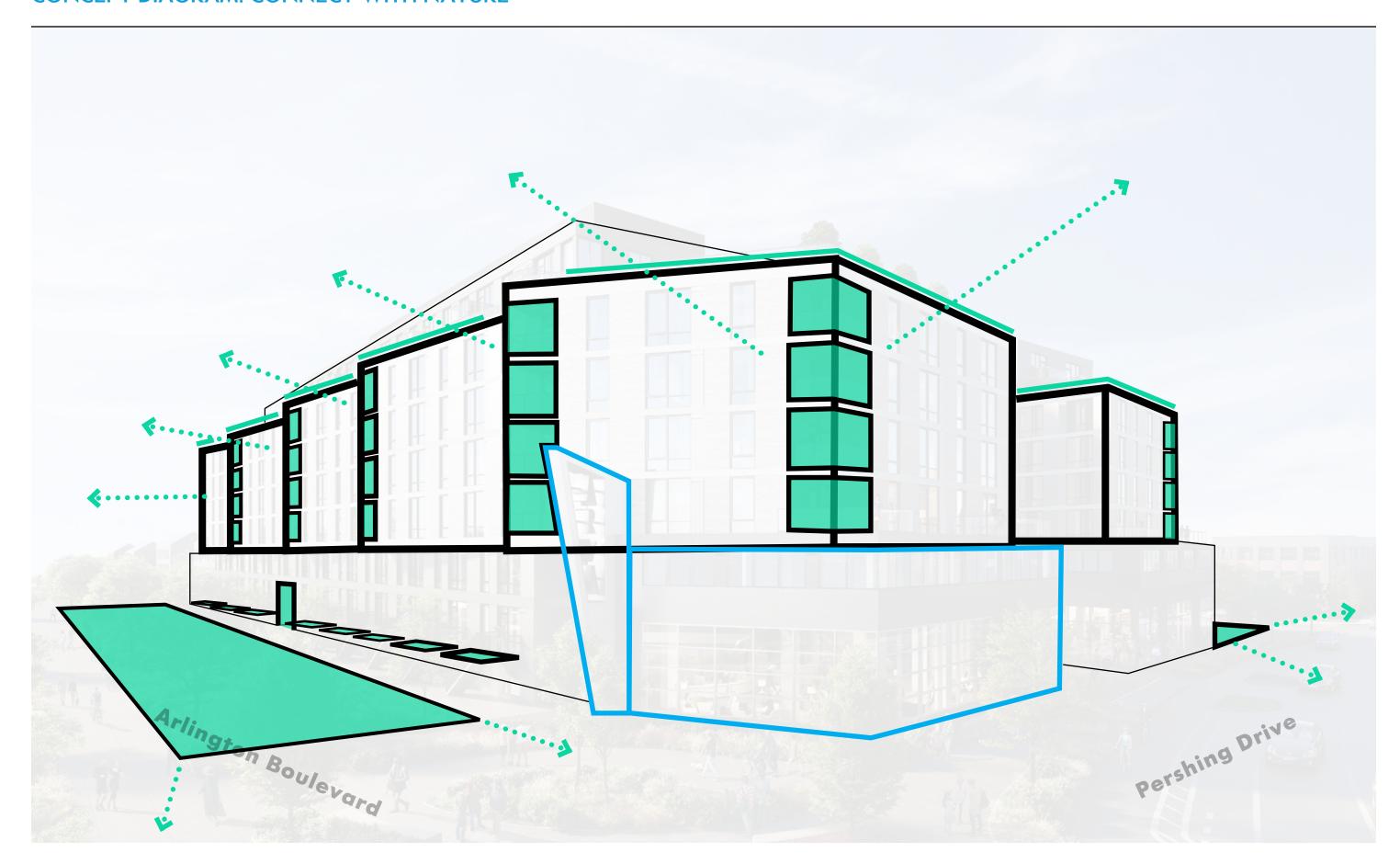




CONCEPT DIAGRAM: SPLIT AND TWIST FACES WITH TRIANGLE SHAPE



ARCHITECTURE **CONCEPT DIAGRAM: CONNECT WITH NATURE**



STUDIOS LandDesign. Bowman GOROVE SLADE

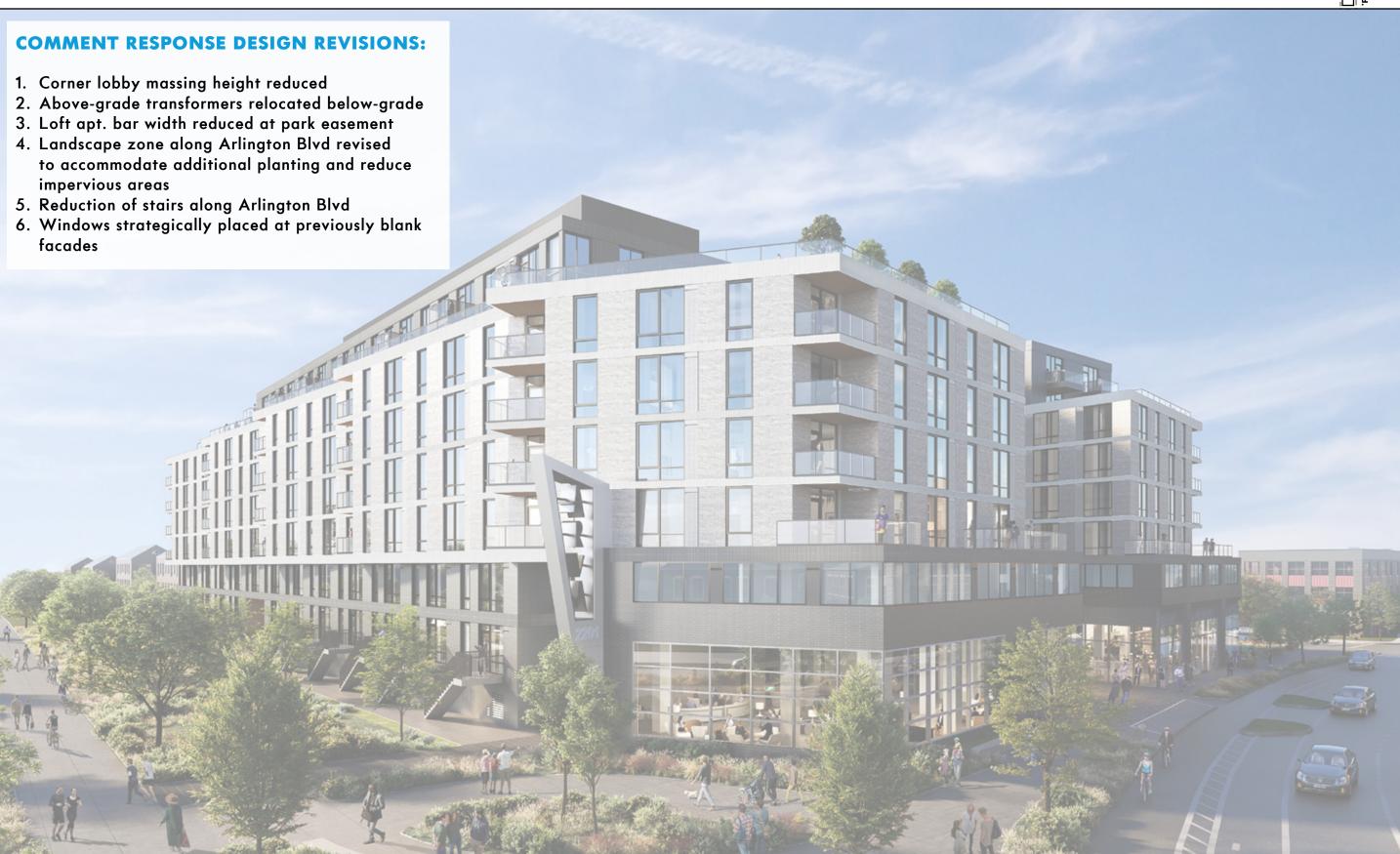
LOBBY CORNER AT ARLINGTON BLVD AND N PERSHING DRIVE











LOBBY CORNER AT ARLINGTON BLVD AND N PERSHING DRIVE





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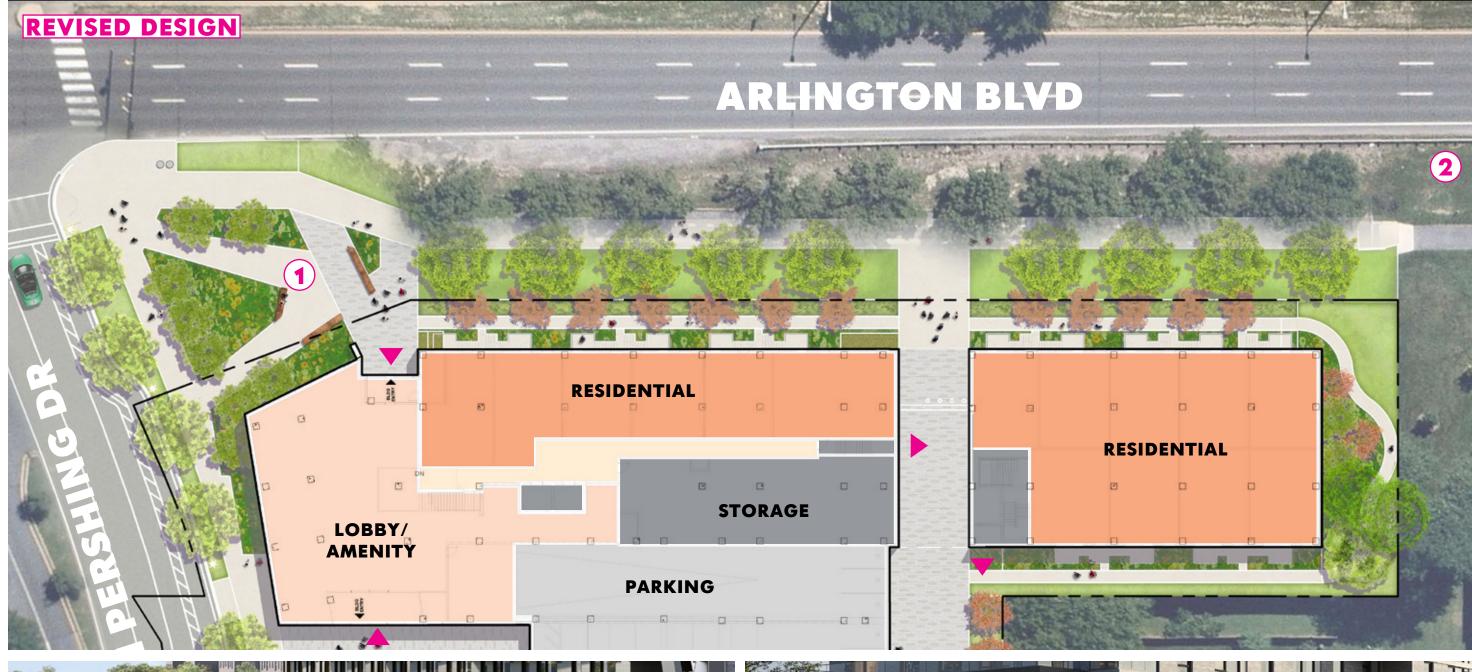




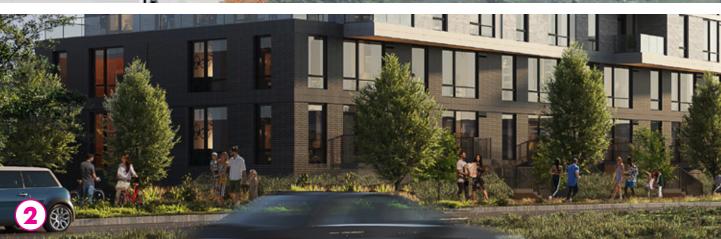


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LANDSCAPE AND UNIT STAIRS AT ARLINGTON BLVD







STUDIOS LandDesign. Bowman GOROVE SLADE

ARCHITECTURE LOBBY ENTRY AT THRU-BLOCK ON N PERSHING DRIVE





STUDIOS LandDesign. Bowman GOROVE SLADE

LOBBY ENTRY AT THRU-BLOCK ON N PERSHING DRIVE



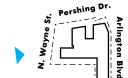


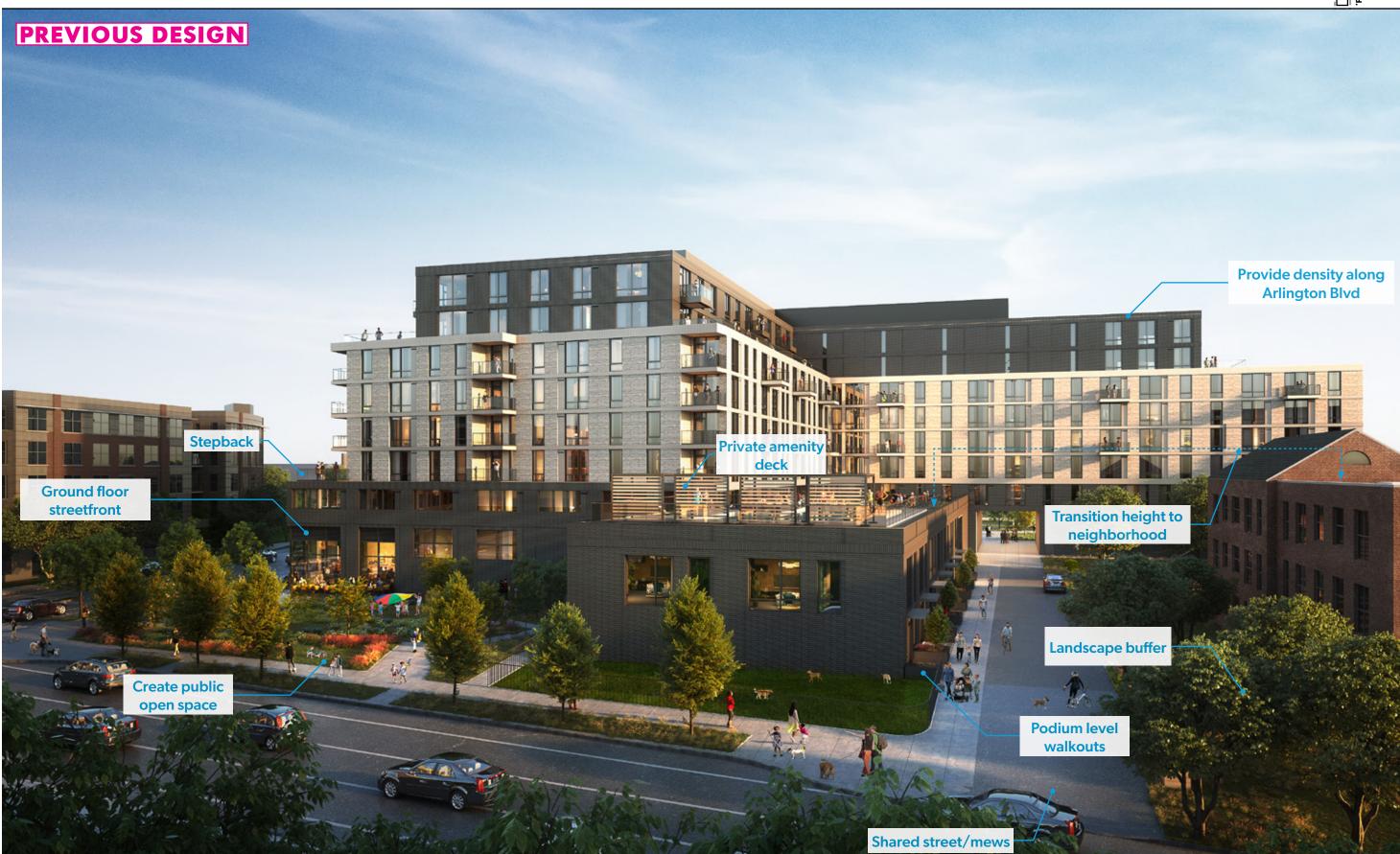
STUDIOS LandDesign. Bowman GOROVE SLADE

OPEN SPACE AT N PERSHING DRIVE & N WAYNE ST



ARCHITECTURE **AERIAL VIEW OVER MEWS AND COURTYARD**





STUDIOS. LandDesign. Bowman GOROVE SLADE

ARCHITECTURE AERIAL VIEW OVER MEWS AND COURTYARD

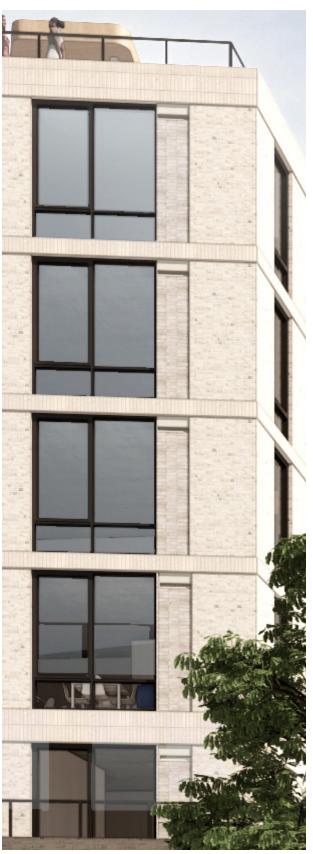




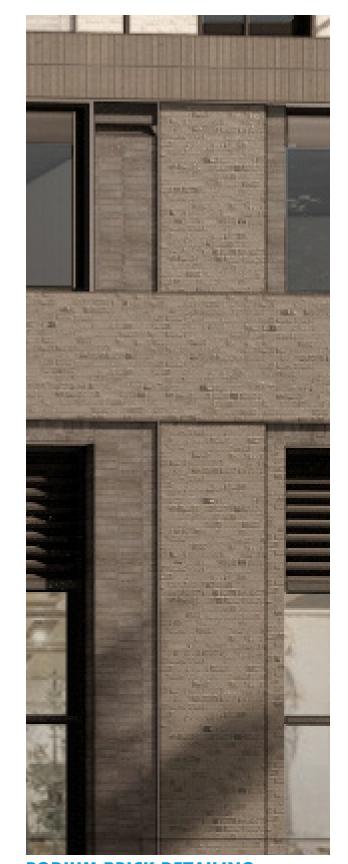
ARCHITECTURE **BRICK DETAILING**



CONTEXT BRICK DETAILING



TOWER BRICK DETAILING



PODIUM BRICK DETAILING

STUDIOS LandDesign. Bowman GOROVE SLADE

LOFTS AT TRAIL ON ARLINGTON BLVD

ARCHITECTURE



STUDIOS. LandDesign. Bowman GOROVE SLADE

ARCHITECTURE LOFTS AT TRAIL ON ARLINGTON BLVD





ARCHITECTURE ARLINGTON BLVD LOOKING NORTH





STUDIOS. LandDesign. Bowman GOROVE SLADE

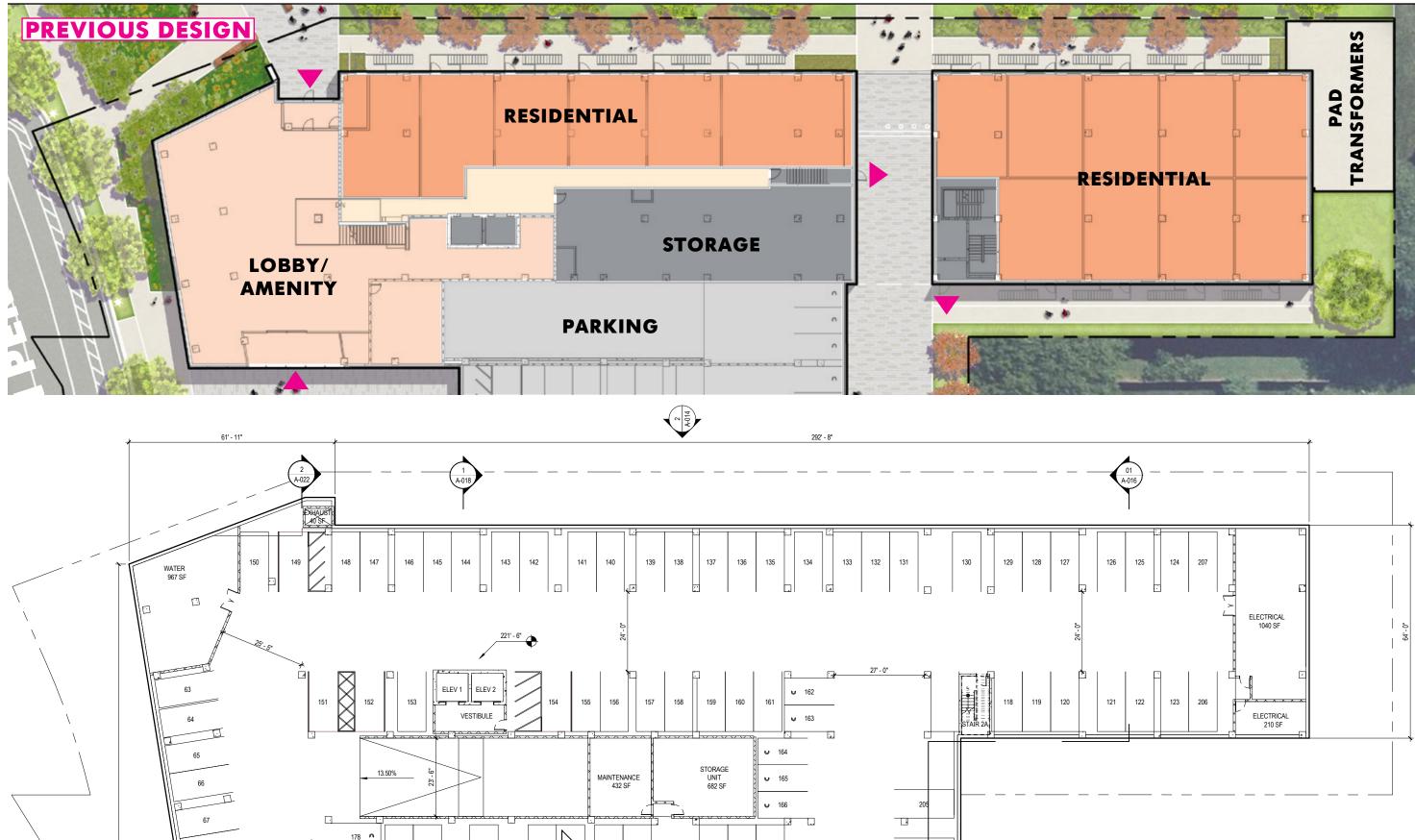


ARLINGTON BLVD LOOKING NORTH

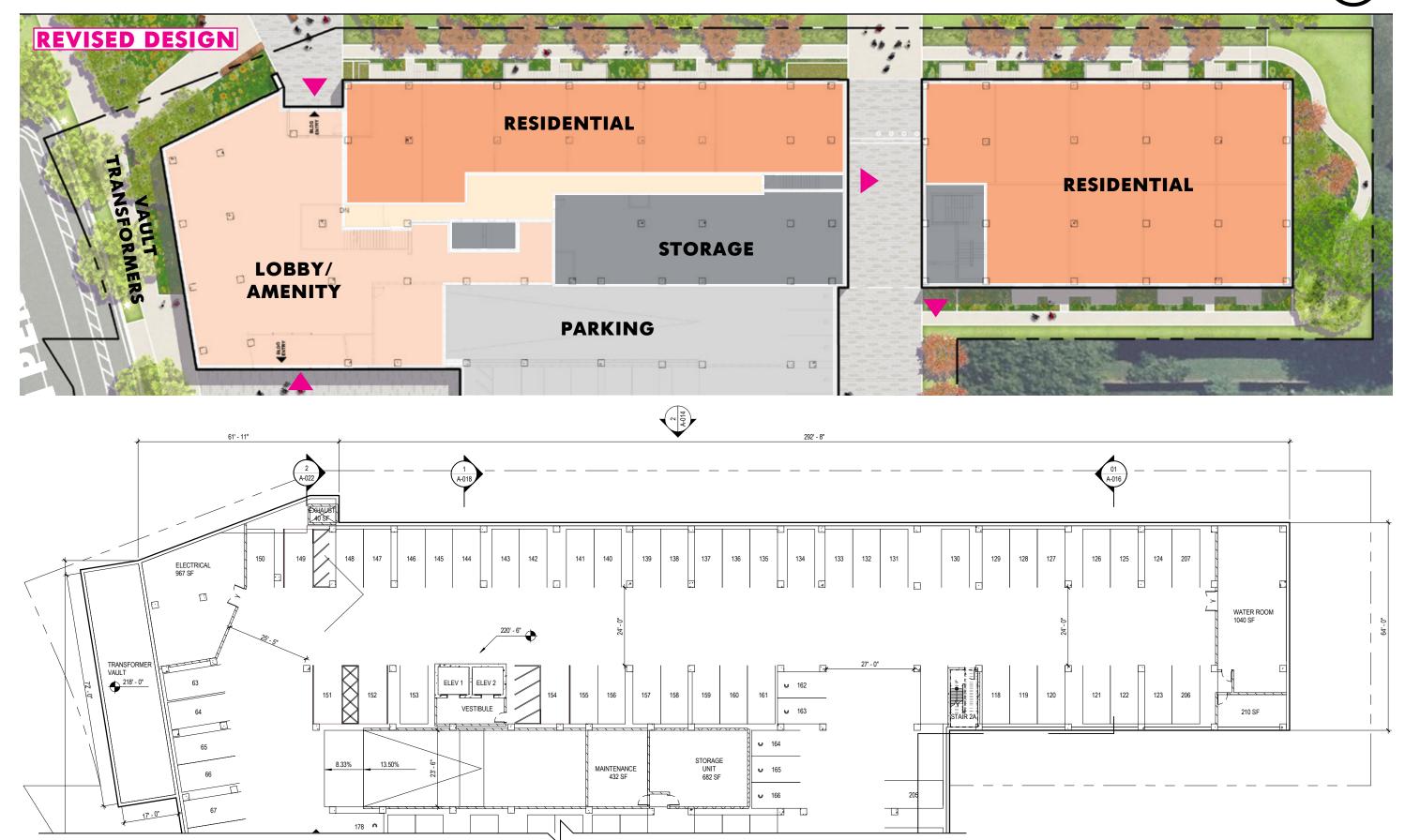
ARCHITECTURE



ARCHITECTURE **BELOW GRADE GARAGE & TRANSFORMERS**



ARCHITECTURE **BELOW GRADE GARAGE & TRANSFORMERS**



LOBBY CORNER AT ARLINGTON BLVD AND N PERSHING DRIVE



STUDIOS LandDesign. Bowman GOROVE SLADE

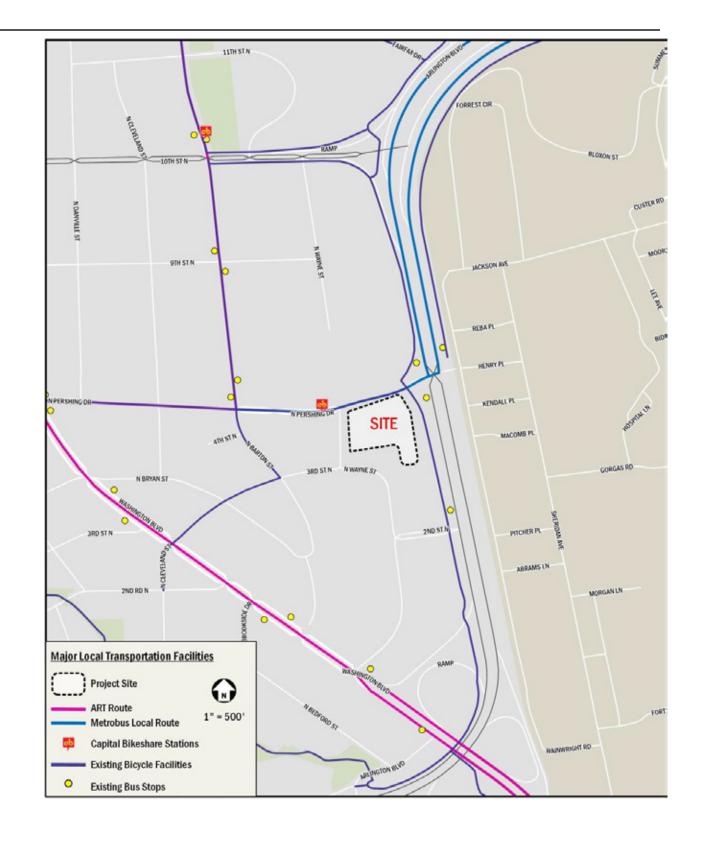
Transportation Overview

• 15 bus stops within ¼ mile

TRANSPORTATION

OVERVIEW

- Surrounding pedestrian and bicycle network
 - Improvement in pedestrian facilities in and around site
 - Access to several existing on- and off-street bicycle facilities
 - Recommended improvements per Special GLUP Study
- 207 proposed vehicle parking spaces
 - 201 residential spaces (0.8 parking ratio)
 - 6 retail spaces
- 102 long-term bike spaces and 16 short-term bike spaces
- Will include Transportation Management Plan

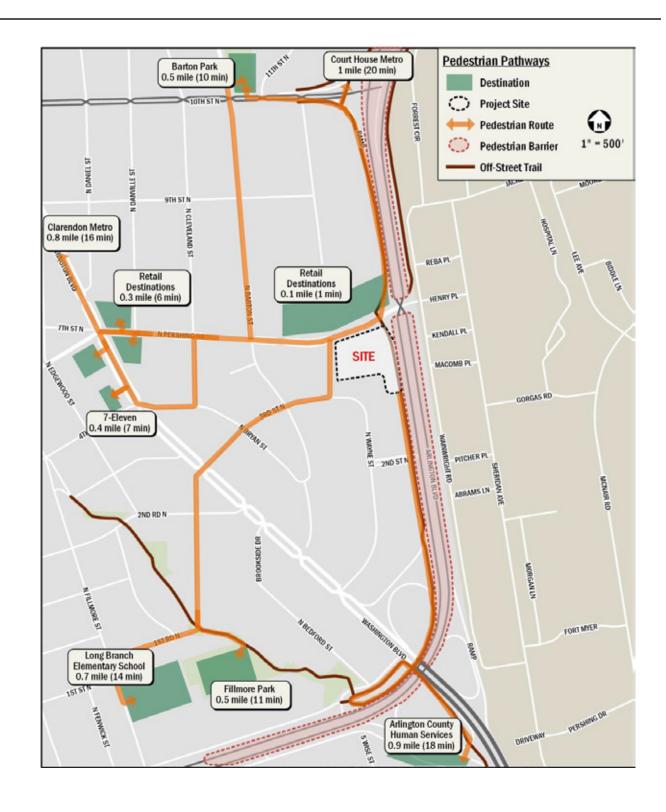


Pedestrian Facilities

TRANSPORTATION

PEDESTRIAN FACILITIES

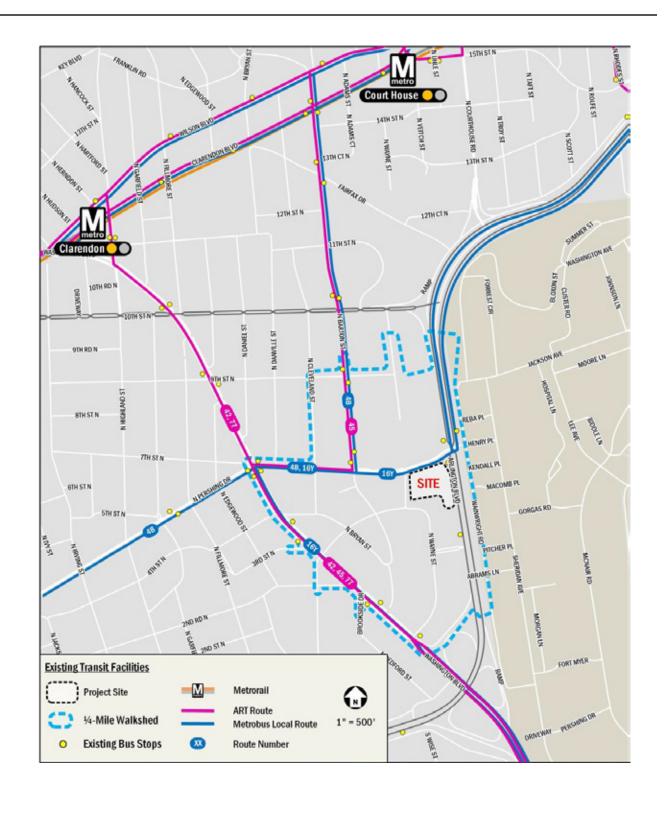
- Surrounding pedestrian network:
 - Arlington Boulevard Trail runs along eastern frontage of the site
 - Most sidewalks along major streets meet sidewalk width and accessibility standards
 - Missing sidewalks/curb ramps along residential neighborhood streets
- Enhanced pedestrian experience by improving sidewalks and surrounding streetscape and upgrades to Arlington Boulevard Trail
- Very Walkable Walkscore of 76
 - Most errands can be accomplished on foot
 - Presence of nearby parks and neighborhood serving retail



TRANSPORTATION TRANSIT FACILITIES

Transit Facilities

- 15 bus stops within ¼ mile radius
- Served by 5 bus routes including:
 - Metrobus (16Y, 4B)
 - ART (42, 45, 77)
- Clarendon (0.8 mile walking distance) & Court House (1 mile walking distance) Metro Stations
 - Orange Line
 - Silver Line

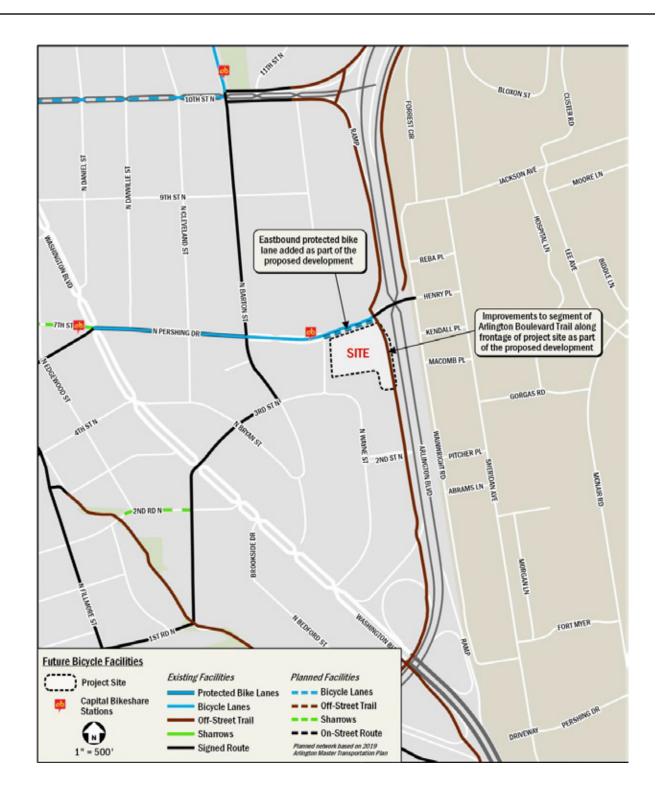


Bicycle Facilities

TRANSPORTATION

BICYCLE FACILITIES

- Existing bike lanes along N Pershing Drive
- Arlington Boulevard Trail
- Capital Bikeshare station located on N Pershing Drive 200 feet west of project site
- Proposed improvements (with development):
 - Install protected bike lanes along eastbound N Pershing Drive between N Wayne Street and Arlington Boulevard
 - New segment of Arlington Boulevard Trail along eastern frontage of the site





Bicycle Parking

TRANSPORTATION

BICYCLE PARKING

• Long-term bicycle parking proposed in ground-level bike room

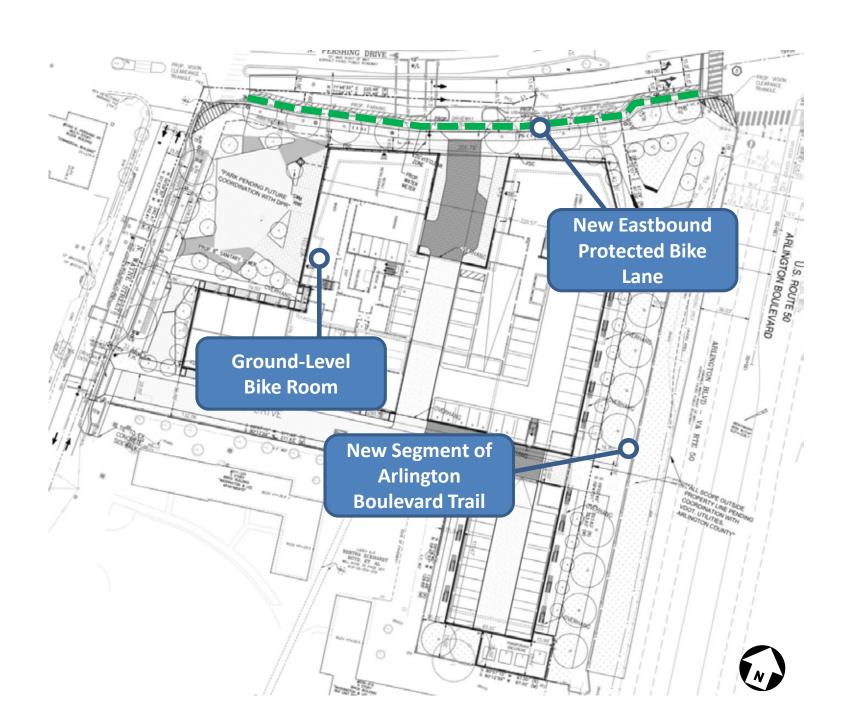
Residential: 101 long-term spaces

Retail: 1 long-term spaces

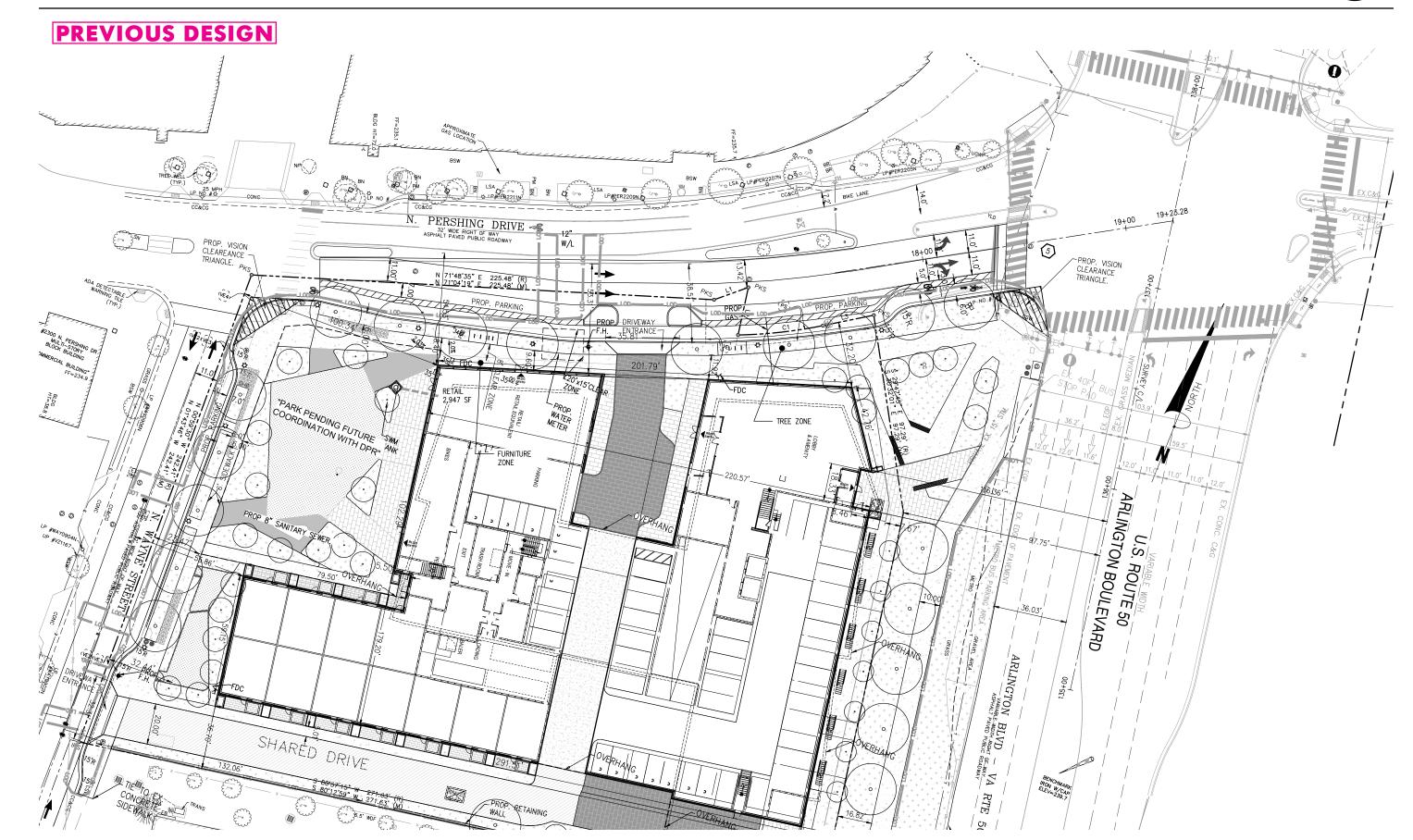
• Short-term bicycle parking proposed along site perimeter

– Residential : 8 short-term spaces

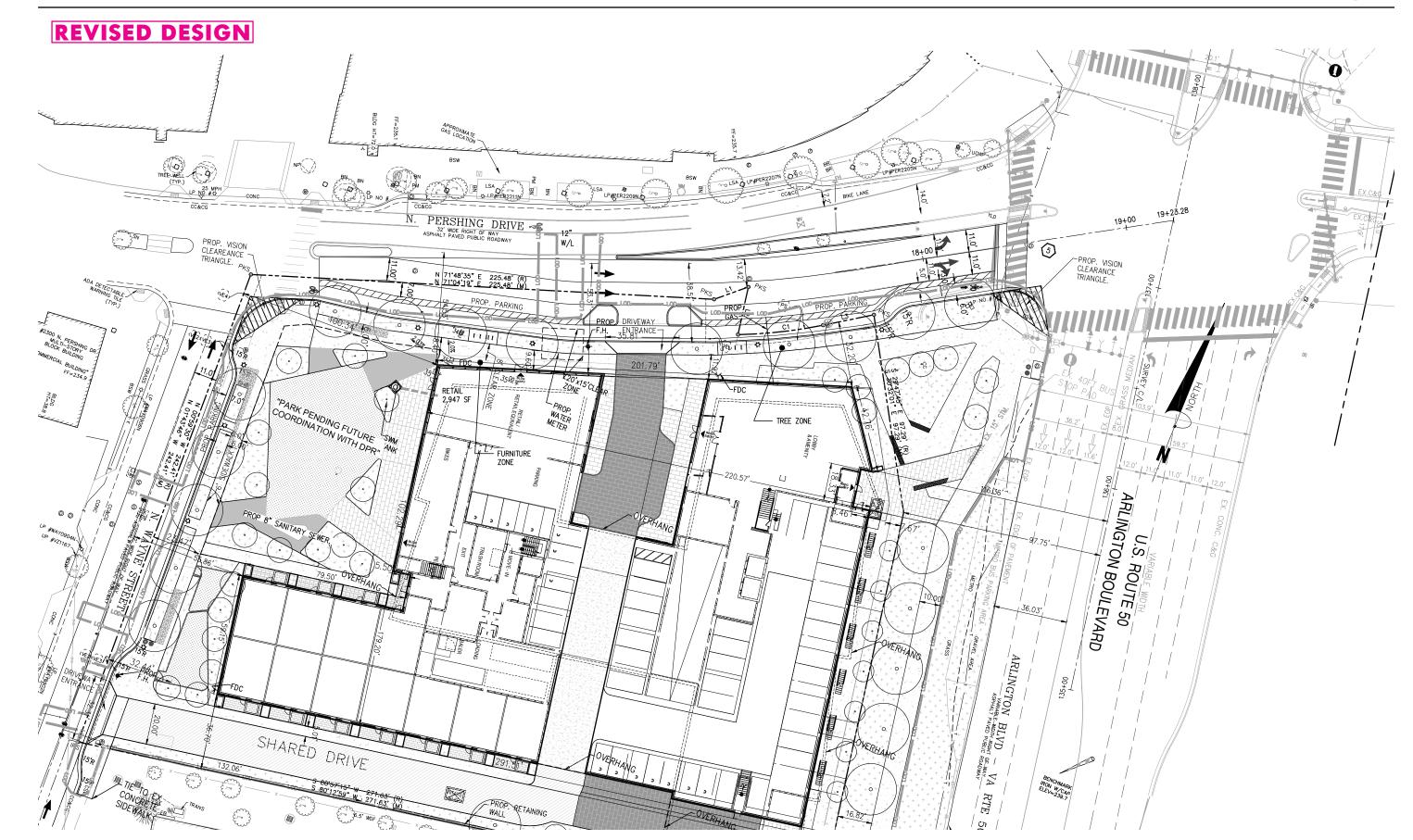
Retail: 8 short-term spaces













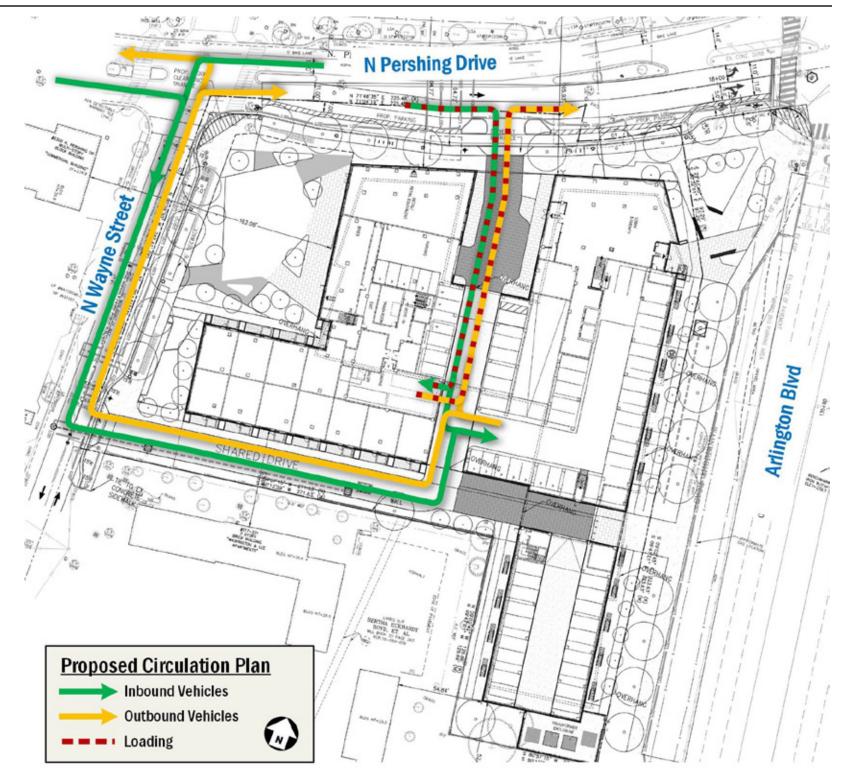
Site Circulation & Parking

- Two (2) proposed vehicular access points:
 - N Pershing Drive (Right-In/Right-Out Only)
 - N Wayne Street (Left-In/Right-Out Only)
- Internal street connects driveways to parking and loading facilities
- Shared Drive/Mews along southern frontage of residential building will include a curbless design, non-traditional paving and streetscape elements
- Proposed parking

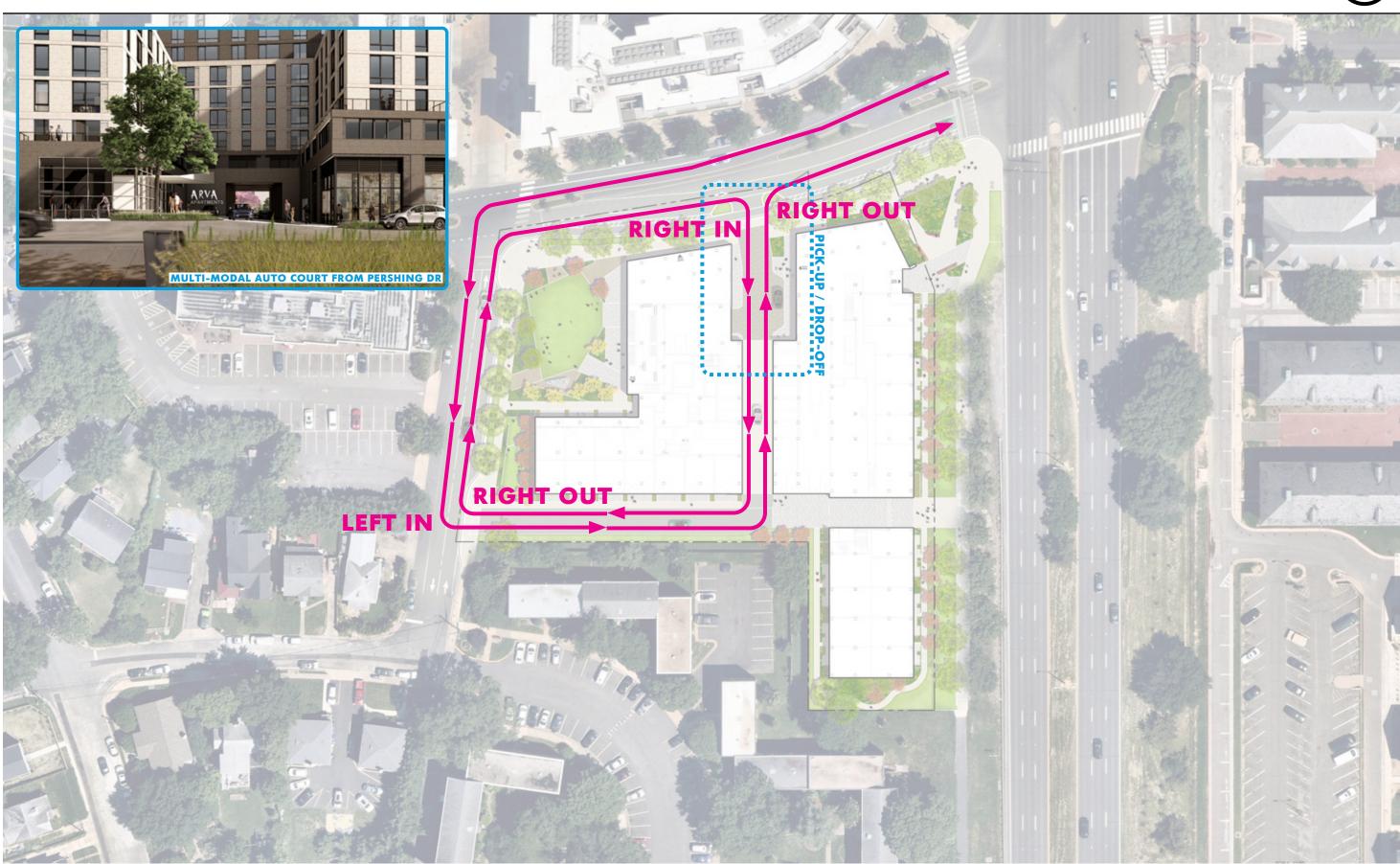
TRANSPORTATION

SITE CIRCULATION & PARKING

- Residential: 0.8 spaces per unit (201 spaces)
- Retail: 1 space per 580 square feet (6 spaces)
- Electric vehicle parking
 - 8 EV charging spaces on Day 1
 - 23 additional EV ready spaces



PICK-UP / DROP-OFF

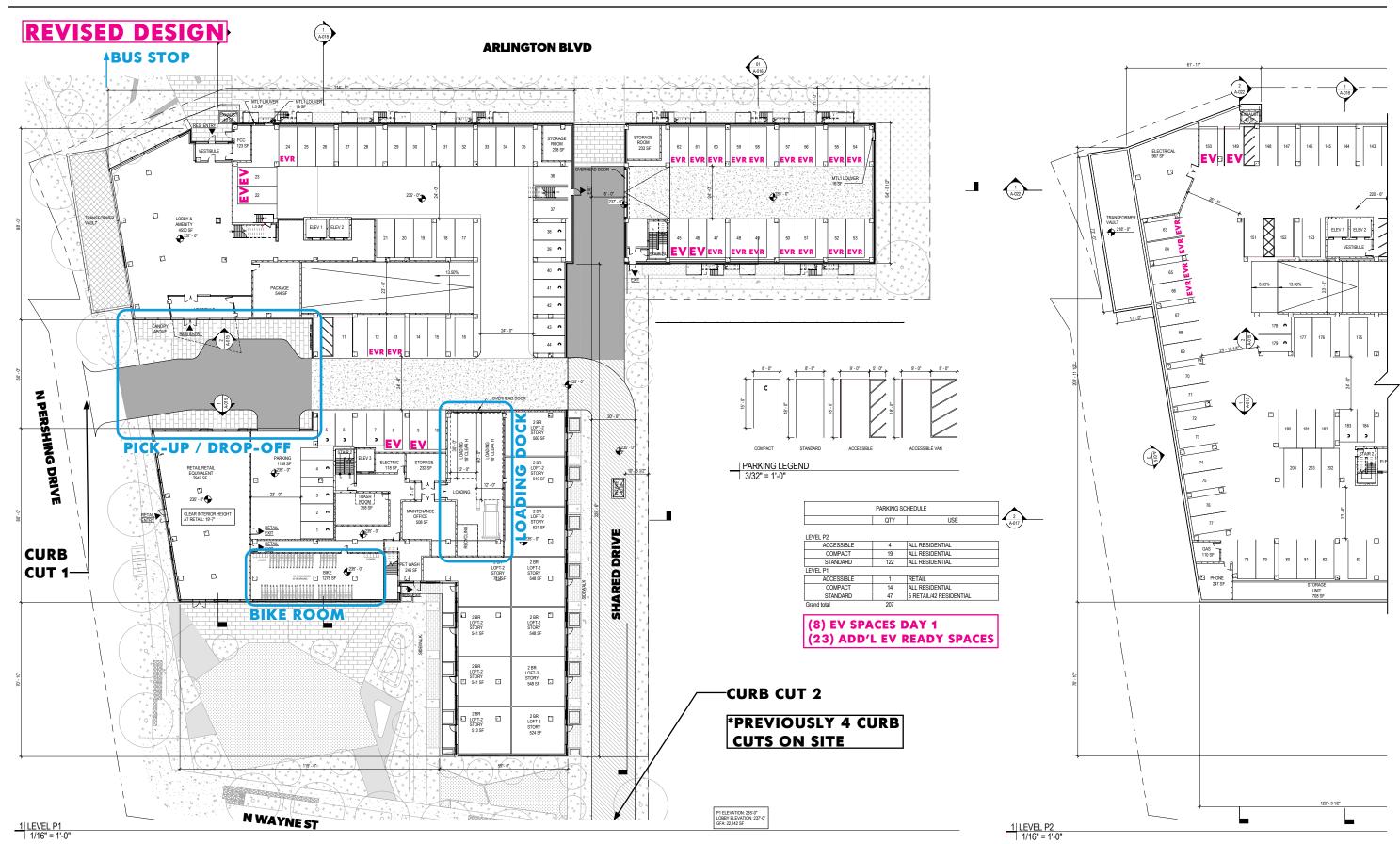


TRANSPORTATION

LEVEL P1/P2 PARKING & EV STATIONS

STUDIOS LandDesign. Bowman GOROVE SLADE

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Multimodal Transportation Assessment

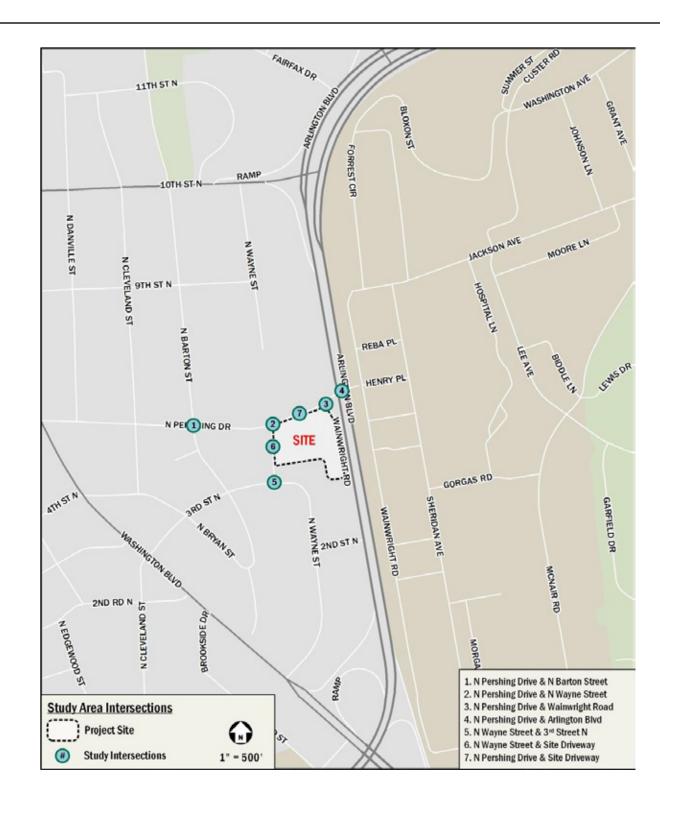
Scoped with County

TRANSPORTATION

- 7 intersections studied, including site driveways
- No mitigation triggered by capacity analysis
- Project impacts will be mitigated through the implementation of the following:
 - Enhanced site design elements

MULTIMODAL TRANSPORTATION ASSESSMENT

- Improvements identified during staff review and SPRC process
- Implementation of Transportation Management Plan



Transportation Improvements

- 1. N Pershing Drive Eastbound Protected Bike Lane
- 2. New Segment of Arlington Boulevard Trail
- 3. East-West Shared Street/Mews Connection to Trail
- 4. Improved Sidewalks Along Site Perimeter
- 5. Open Space



Transportation Management Plan

• Participation and Funding

TRANSPORTATION MANAGEMENT PLAN

ATP Membership

TRANSPORTATION

- Designated PTC
- Contribute to ACCS
- Facilities and Improvement
 - Information display in building lobbies
 - On-site bicycle facilities
 - Ground-level bicycle room
 - Parking Management Plan

- Promotions, Services, and Polices
 - Provide site-specific transit information
 - Provide one-time SmarTrip cards, bikeshare membership, or carshare membership
 - Provide sustainable commuter benefit program for employees
 - Provide links to transportation resources on website
- Performance Monitoring
 - Annual report to County
 - Transportation and parking performance monitoring

