

16.0 Agency Correspondence

This chapter includes correspondence with federal, state, and local agencies and officials throughout the development of the AA/EA.

Correspondence with Federal Agencies and Officials



September 2, 2010

U.S. Fish and Wildlife Service
Virginia Field Office
Attn: Tylan Dean
6669 Short Lane
Gloucester, Virginia 23061

Re: Columbia Pike Transit Initiative, Environmental Assessment/Alternatives Analysis

Dear Sir or Madam:

On behalf of Arlington County and Fairfax County, the Washington Metro Transit Authority (WMATA) is conducting an Environmental Assessment of proposed transit improvements along the Columbia Pike corridor. As WMATA's consultant, AECOM is requesting U.S. Fish and Wildlife Service concurrence that the project is not likely to adversely affect federally listed threatened or endangered species.

The project study area, as shown on the attached USGS quadrangles (**Figure 1**) falls within the Virginia jurisdictions of the City of Alexandria, Arlington County and Fairfax County. The study area extends approximately five miles, mostly along the highly urban corridor of Columbia Pike (State Route 244), between Bailey's Crossroads/Skyline and Pentagon City, with deviations onto other roadways at either end of the corridor. The proposed improvements will provide enhanced transit service to connect existing development and new redevelopment along the corridor, improve access to major activity centers and encourage transit ridership growth.

The Federal Transit Administration (FTA) is the lead federal agency for the project and has determined that the appropriate class of action for this project is an Environmental Assessment (EA). The project team is preparing a combined EA and Alternatives Analysis to meet federal requirements under the National Environmental Policy Act of 1969, as amended (NEPA) and to prepare an application for funding through the Federal New Starts/Small Starts program.

As part of the EA, several alternatives will be evaluated to include a No Build Alternative, Transportation Systems Management (TSM) Alternative and a Streetcar Build Alternative. Associated with both the TSM and Streetcar Build Alternatives, some additional right-of-way outside of the existing publicly owned transportation corridor may be required to accommodate relocated or new transit stops, traction power substations, and a primary operations and maintenance facility. The current alignment, potential stop locations, and sites being evaluated for a primary operations and maintenance facility are identified in **Figure 2**, attached to this letter.

AECOM reviewed the U.S. Fish and Wildlife Service, Virginia Field Office's website and identified the following federally-listed species:

Scientific Name	Common Name	Status	Jurisdiction(s)
<i>Aeschynomene virginica</i>	Sensitive joint-vetch	LT	Fairfax County
<i>Isotria medeoloides</i>	Small whorled pogonia	LT	Fairfax County
<i>Haliaeetus leucocephalus</i>	Bald Eagle	BGEPA	Fairfax County
<i>Elliptio lanceolata</i>	Yellow lance	Species of Concern	Fairfax County
<i>Sphalloplana holsingeri</i>	Holsinger's groundwater planarian	Species of Concern	Fairfax County
<i>Sphalloplana subtilis</i>	Bigger's groundwater planarian	Species of Concern	Fairfax County
<i>Stygobromus kenki</i>	Rock Creek groundwater amphipod	Species of Concern	Fairfax County
<i>Stygobromus phreaticus</i>	Northern Virginia well amphipod	Species of Concern	Fairfax County, City of Alexandria
<i>Stygobromus sp. 15</i>	A groundwater amphipod	Species of Concern	Fairfax County, Arlington County
<i>Pycnanthemum torrei</i>	Torrey's mountain-mint	Species of Concern	Fairfax County, Arlington County, City of Alexandria

Source: USFWS, Virginia Field Office website, August 2010

LT – Listed Threatened

BGEPA – Bald and Golden Eagle Protection Act

Based on a review of the species fact sheet provided for the Sensitive joint-vetch, this species occurs in fresh to slightly brackish tidal river systems, typically at the outer fringe of marshes or shores. Within the study area, Columbia Pike crosses Four Mile Run within Arlington County, west of its convergence with Four Mile Run Creek in Alexandria. Four Mile Run within Arlington is not tidally influenced and therefore it is assumed that the appropriate habitat to support the Sensitive joint-vetch does not occur within the study corridor. Furthermore, the proposed transit investment would stay mostly within existing transportation rights-of-way that are currently paved or covered by concrete; the project would cross Four Mile Run via an existing bridge, and does not propose changes to this structure, other than enhanced structural deck support, if so warranted by engineering analysis.

The Small whorled pogonia fact sheet indicates that this species is found in third-growth upland forests with an open understory and a closed canopy where the topography is typically moderately sloping or almost level. This species requires acidic sandy loam soils with low nutrient content. A portion of the study area within the City of Alexandria is a potential site for an operations and maintenance facility on the Northern Virginia Community College Campus (NOVA). As currently proposed, the site is located on a level recreational field, adjacent to an upland forested area. The project understands that this area partially consists of spoil removed from other parts of the campus during various construction activities. A review of soils information from the USDA Natural Resources Conservation Service shows that the primary soil types include the Kingstowne sandy clay loam, urban land, and the Sassafras-Marumsco complex. These soils are not defined as acidic sandy loams. Therefore it appears that the site conditions do not support the habitat required for the Small whorled pogonia.

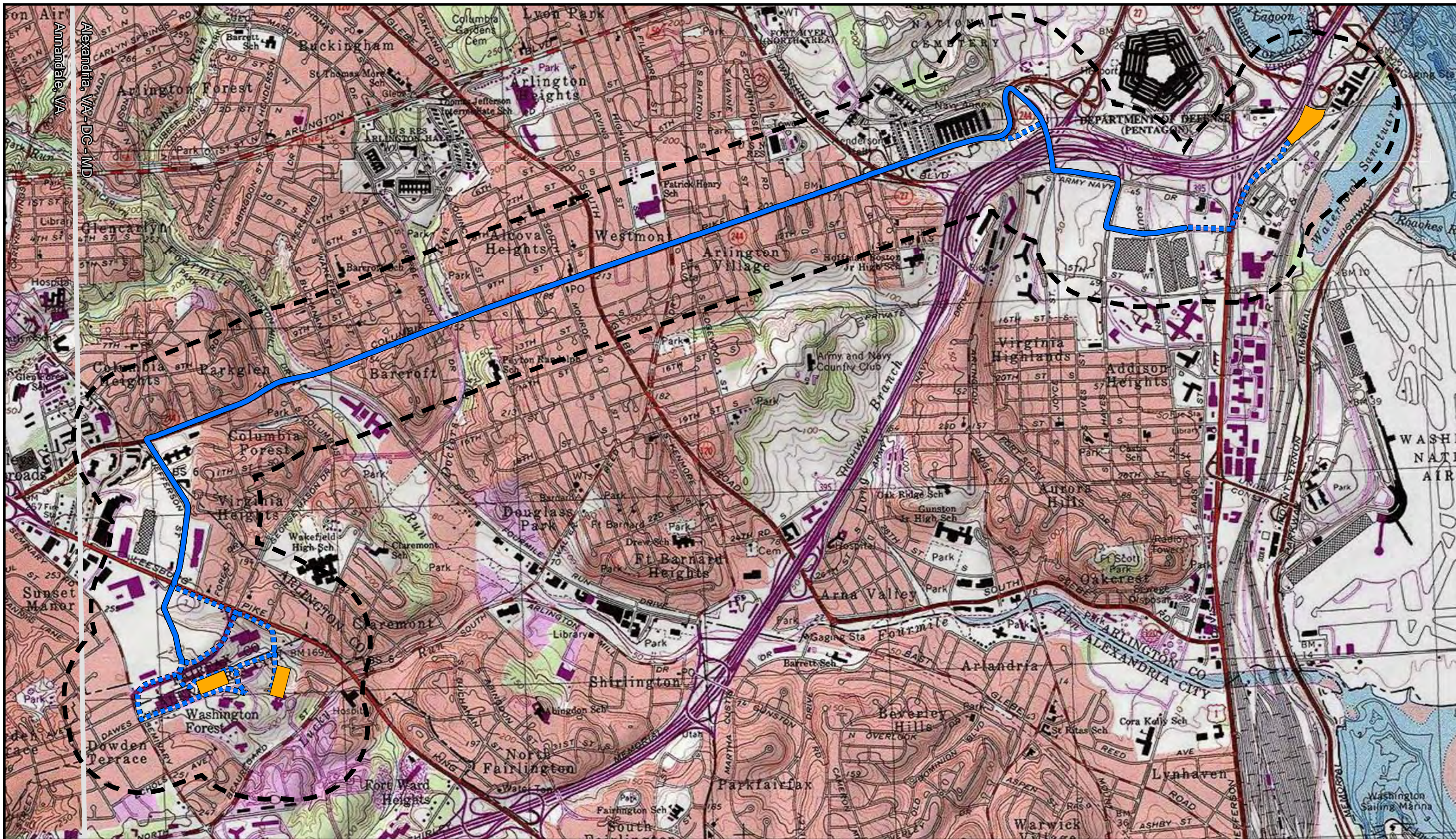
The project team respectfully requests concurrence, based on the information presented above, that the Columbia Pike Transit Initiative Project would not likely adversely affect federally listed threatened or endangered species of concern. A similar letter is being submitted to both the Virginia Department of Game and Inland Fisheries and the Virginia Department of Conservation and Recreation. Should you have any questions or comments, please contact me at 703-682-5008 or Susan.Anderson@aecom.com for more information.

Sincerely,



Susan T. Anderson, AICP
Pike Transit Initiative, NEPA Task Leader

Enclosures: Figure 1: Project Study Area USGS Quadrangles
Figure 2: Project Alignment Map



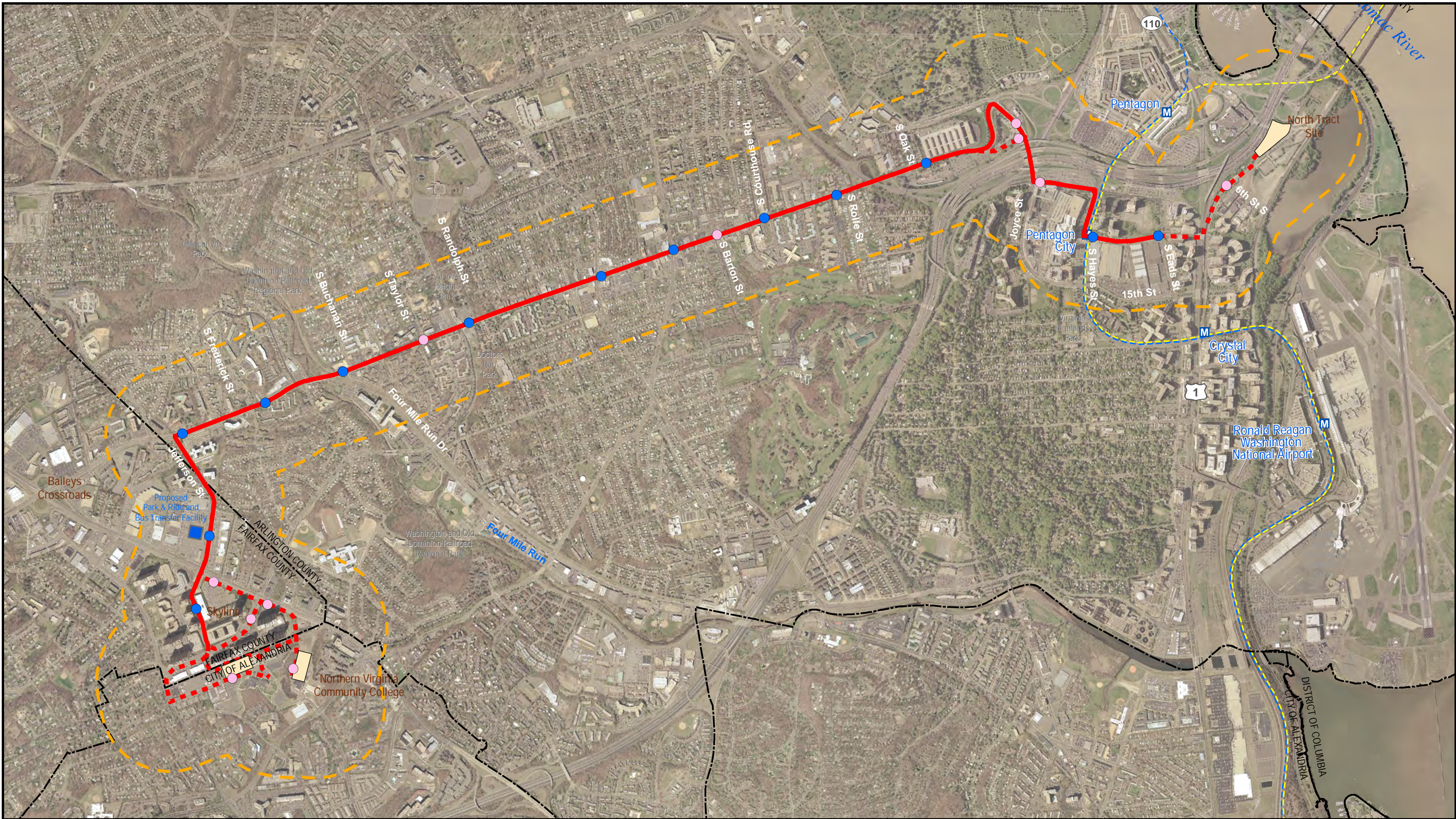
LEGEND

- 1/4 Mile Study Area Buffer
- Transit Alignment
- Alternative Alignment Option
- Maintenance Facility Option
- USGS_Quad_Sheets

Source: USGS Annandale, VA and Alexandria, VA - DC - MD Quadrangles, 2009

Figure 1

Proposed Alignment and Study Area



LEGEND

- Station Location
- Optional Station
- Transit Alignment
- - - Alternate Alignment Option
- Maintenance Facility Option
- M Metrorail Station
- - - 1/4 Mile Study Area Buffer
- - - Metrorail Blue Line
- - - Metrorail Yellow Line

Figure 2



0 0.25 0.5 Miles



Proposed Alignment and Stations



August 16, 2010

Mr. Ralph Newton
Director
Defense Facilities Directorate
1155 Defense, Pentagon
Arlington, VA 20301-1155

Re: Planned Realignment of Columbia Pike

Dear Mr. Newton:

On behalf of Arlington and Fairfax Counties, and in cooperation with the Federal Transit Administration (FTA), the Washington Metropolitan Area Transit Authority (WMATA) is preparing an Environmental Assessment (EA) in accordance with the National Environmental Policy Act (NEPA) of 1969, as amended, for proposed transit improvements in Arlington and Fairfax Counties, VA. The proposed project, known as the Columbia Pike Transit Initiative, would extend along Columbia Pike (Route 244) from the Skyline complex in Fairfax County to Pentagon City in Arlington County. The NEPA build alternative is a streetcar line.

The Columbia Pike Transit Initiative will be evaluating two design options (see attached map) in its NEPA documentation of the Columbia Pike corridor near Joyce Street. One design option follows the existing Columbia Pike roadway alignment between the Navy Annex and South Joyce Street; the other design option follows a realigned Columbia Pike as described in the 2005 Updated Pentagon Reservation Master Plan. According to this plan, it is our understanding that Columbia Pike would be realigned as part of the Arlington National Cemetery expansion. We assume that the realigned road would have a cross-section consistent with the 5-lane cross-section planned for the majority of Columbia Pike in Arlington County.

It is our understanding that the proposed roadway realignment was included in the National Capital Planning Commission's (NCPC) FY 2008-2013 Federal Capital Improvements Program (FCIP) in 2007, although it is not included in the draft FY 2011-2016 FCIP. Therefore, it is not clear whether construction will begin within the proposed timeline for construction of the Columbia Pike Transit Initiative between 2014 and 2016. The Pike Transit project team requests clarification on the status of the proposed realignment.

If you would like to discuss this further, please contact me via phone at (202) 962-1027 or via email at jdittmeier@wmata.com.

Sincerely,

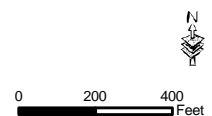
John M. Dittmeier
Project Manager

cc: AECOM – M. Wencil



LEGEND

- Transit Alignment
- Alternative Alignment Option
- M Metrorail Station
- Metrorail Blue Line
- Metrorail Yellow Line
- Station Stop
- Optional Station Stop



Navy Annex Alignment Alternatives



July 7, 2010

Mr. Francis A. Douglas, Director
Directorate of Environmental Management
U.S. Department of the Army
Joint Base Myer-Henderson Hall
204 Lee Avenue
Fort Myer, Virginia 22211-1199

Dear Mr. Douglas:

This letter responds to your June 8, 2010 letter for scoping of the proposed Dominion Virginia Power 230KV underground line and substation and of the proposed, co-located Arlington County fiber optics communication cables.

On behalf of the Columbia Pike Transit Initiative, I present the following information:

1. The Transit Initiative proposes a five-mile streetcar line from Skyline in Fairfax County to Pentagon City in Arlington County via Columbia Pike. The project is further described at the web site, www.piketransit.com.
2. The current project sponsors of the Transit Initiative are Arlington County and Fairfax County. Metro is solely a technical manager of the current study on behalf of the two Counties.
3. The current study of the Transit Initiative itself is the NEPA phase. The Federal Transit Administration has issued a Class of Action of Environmental Assessment.
4. As you may know, the Transit Initiative is coordinating with the U.S. Department of Defense on the re-alignment of Columbia Pike at Joyce Street near Henderson Hall and the U.S. Air Force Memorial.
5. The streetcar line, if selected as the Locally Preferred Alternative, will use lanes of the Pike, reconstruct the pavement of the Pike for its trackway and relocate certain utilities from the trackway.
6. The streetcar project will have a traction power system, comprised of feeders of Dominion Virginia power, its own substations, and underground 750V cables to the overhead catenary system.
7. The proposed Dominion Virginia Power project might serve the traction power system of the streetcar project. The contact within Dominion Virginia Power for the streetcar project is Mr. Patrick Haworth, Patrick.Haworth@dom.com, 703-375-5968.

Mr. Francis A. Douglas, Director
Page 2

Therefore, upon selection of a final alignment of the 230KV line that may be within or crossing Columbia Pike, the Transit Initiative requests ongoing coordination with Dominion Virginia Power in planning, design and construction.

If you have further requests for information, please contact me at 202-962-1027 or jdittmeier@wmata.com.

Sincerely,

John M. Dittmeier
Project Manager
Metro Office of Long Range Planning

cc: Dominion Virginia Power/ E. Harper
U.S. Army EMD/ K. Lalire
Dewberry/ K. Larkin
Metro/ R. Musgrave
Arlington County/ S. Del Giudice
AECOM/ J. Mumford



Kristie.Lalire@conus.army.mil

KLarkin@dewberry.com



UNITED STATES DEPARTMENT OF COMMERCE
National Oceanic and Atmospheric Administration
NATIONAL MARINE FISHERIES SERVICE
NORTHEAST REGION
55 Great Republic Drive
Gloucester, MA 01930-2276

OCT 20 2009

Robin McElhenny-Smith
Deputy Project Manager
Washington Metropolitan Area Transit Authority
600 5th Street, NW
Room 5B-26
Washington, DC 20001

Re: Columbia Pike Transit Initiative

Dear Ms. McElhenny-Smith,

This is in response to your letter dated September 18, 2009 regarding the preparation of environmental documents for proposed transit improvements in Arlington and Fairfax Counties, Virginia. The proposed project, known as the Columbia Pike Transit Initiative, would extend along Columbia Pike (Route 244) from the Skyline Complex in Fairfax County to Pentagon City in Arlington County.

The Protected Resources Division of NOAA's National Marine Fisheries Service (NMFS) oversees programs related to species listed under the Endangered Species Act (ESA) of 1973, as amended. A population of federally endangered shortnose sturgeon (*Acipenser brevirostrum*) occurs in the Potomac River. Based on the maps of the project site, it does not appear that there would be any work in the Potomac River which may affect this species or its habitat. If this is the case, then there would be no further coordination with NMFS regarding effects to listed species. However, if work will occur in the Potomac River, further coordination may be necessary. As you may know, Section 7(a)(2) of the ESA states that each Federal agency shall, in consultation with the Secretary, insure that any action they authorize, fund, or carry out is not likely to jeopardize the continued existence of a listed species or result in the destruction or adverse modification of designated critical habitat. Any discretionary federal action that may affect a listed species must undergo Section 7 consultation. If the proposed action is authorized, funded or carried out by a federal agency and that federal agency determines that the action may affect shortnose sturgeon, a consultation pursuant to Section 7 of the ESA would be necessary. Should you have any questions regarding the Section 7 process, please contact Julie Crocker of my staff at (978)0282-8480 or by e-mail (Julie.Crocker@Noaa.gov).

NMFS' Habitat Conservation Division (HCD) oversees programs related to Essential Fish Habitat designated pursuant to the Magnuson-Stevenson Fisheries Conservation Act as well as



programs related to trust resources protected under the Fish and Wildlife Conservation Act. Once project plans are developed, it is recommended that you coordinate with HCD staff to determine if any coordination on effects to NOAA trust resources is necessary. Please contact David O'Brien at (804)684-7828 (or e-mail: David.L.O'Brien@Noaa.gov) for additional information regarding NOAA trust resources in Virginia.

Thank you for the opportunity to provide comments on the proposed action. Should you have any questions about this correspondence please contact the staff noted above.

Sincerely,



Mary A. Colligan
Assistant Regional Administrator
for Protected Resources

Cc: O'Brien, F/NER4 (VA)

File Code: Sec 7 tech assist 2009 – Columbia Pike Transit Initiative
PCTS: T/NER/2009/05717



U.S. Department
of Transportation
**Federal Transit
Administration**

REGION III
Delaware, District of
Columbia, Maryland,
Pennsylvania, Virginia,
West Virginia

1760 Market Street
Suite 500
Philadelphia, PA 19103-4124
215-656-7100
215-656-7260 (fax)

Mr. Stephen Del Giudice
Transit Bureau Chief
Arlington County

OCT 8 2009

Mr. Leonard Wolfenstein
Chief, Transportation Planning Section
Fairfax County Department of Transportation

Pike Transit Initiative
c/o WMATA
600 Fifth Street, NW
Room 5B – 26
Washington, DC 20001

Re: Columbia Pike Transit Initiative – NEPA Class of Action Determination

Dear Messrs. Del Giudice and Wolfenstein:

The Federal Transit Administration (FTA) has received your request (October 5, 2009 letter) to identify the probable NEPA Class of Action for the Columbia Pike Transit Initiative. This project proposes transit improvements extending five miles along Columbia Pike between Bailey's Crossroads/Skyline area in Fairfax County and Pentagon City in Arlington County, Virginia.

Based upon a review of the project description and of past studies (including the Impact Screening for the local Alternatives Analysis), and a field view of the corridor, the FTA has identified the probable Class of Action for this project as an Environmental Assessment (EA).

In accordance with the Environmental Impact and Related Procedures of 23 CFR 771.115 (c) (Classes of actions – Class III (EAs)), an Environmental Assessment is defined as “[a]ctions in which the significance of the environmental impact is not clearly established.”

Timothy Lidiak of my staff is available to answer questions or provide further assistance. You may reach him at (215) 656-7084.

Sincerely,


for **Regina A. Thompson**
Regional Administrator

cc: John Dittmeier, WMATA
Jim Ashe, WMATA
Brian Glenn, FTA Washington, DC Metropolitan Office
Melissa Barlow, FTA Washington, DC Metropolitan Office



The following two letters were provided to federal, state, and local agencies (September 18, 2009) and elected officials (October 30, 2009) to announce the preparation of NEPA Documentation for the Columbia Pike Transit Initiative.

An additional two letters were provided to federal, state, and local elected and non-elected officials on November 13, 2009 to relay project information and invite officials to an agency coordination meeting for the Columbia Pike Transit Initiative.

A list of agencies and officials follows.

First Name	Last Name	Position	Organization
FEDERAL			
Brian	Glenn	Director, FTA Metropolitan Office	Federal Transit Administration, Washington Metro Office
Melissa	Barlow	Community Planner	Federal Transit Administration, Washington Metro Office
Letitia A.	Thompson	Regional Administrator	Federal Transit Administration, Region III
John C.	Metzler, Jr.	Superintendent	Arlington National Cemetery
Bob	Whitacre	Building Management Specialist	Defense Facilities Directorate, Pentagon
Roberto	Fonseca-Martinez	Division Administrator	Federal Highway Administration, Virginia Division
Tim	Lewis	Area Engineer	Federal Highway Administration, Virginia Division
Catharine	McManus	Environment Contact	FEMA Region III
Marcel	Acosta	Executive Director	National Capital Planning Commission
Carlton E.	Hart, AICP	Community Planner	National Capital Planning Commission
Patrick	Gregerson	Chief of Planning	National Park Service
Jacki	Katzmire	Regional Environmental Coordinator	National Park Service, Northeast Regional Office
Peter	Colosi	Asst. Regional Administrator- Habitat Conservation	NOAA, National Marine Fisheries Service, Northeast Regional Office
Mary	Colligan	Asst. Regional Administrator- Protected Resoruces	NOAA, National Marine Fisheries Service, Northeast Regional Office
Beverly	Cenname	Senior Policy Advisor	Transportation Security Administration
Bruce F.	Williams	Chief, NOVA Regulatory Section	U.S. Army Corps of Engineers, Norfolk District
Bob	Hume	Chief of Regulatory Branch	U.S. Army Corps of Engineers, Norfolk District
Thomas	Luebke	Secretary	U.S. Commission of Fine Arts
John A.	Bricker	State Conservationist	U.S. Department of Agriculture, Natural Resources Conservation Service
Dalton	Paxman	Action Regional Director, Region 3	U.S. Department of Health and Human Services
Mary Ann	Wilson	Virginia State Coordinator	U.S. Department of Housing & Urban Development
John E.	Hall	Field Office Director	U.S. Department of Housing and Urban Development, District of Columbia Field Office
Willie	Taylor	Director, Environmental Compliance	U.S. Department of Interior
Juan	Palmar	State Director, Eastern States	U.S. Department of the Interior, Bureau of Land Management
Barbara	Rudnick	NEPA Team Leader	U.S. Environmental Protection Agency, Region 3
Cindy	Schulz	Supervisor, Virginia Field Office	U.S. Fish and Wildlife Service, Ecological Services
Marjorie	Snyder	Assistant Regional Director, Northeast Region	U.S. Fish and Wildlife Service, Ecological Services
Hon. Gerald E.	Connolly	11th District	U.S. House of Representatives
Hon. James P.	Moran	8th District	U.S. House of Representatives
Hon. Jim	Webb	Senator	U.S. Senate
Hon. Mark R.	Warner	Senator	U.S. Senate



First Name	Last Name	Position	Organization
STATE			
Pierce	Homer	Secretary of Transportation	Commonwealth of Virginia
Sharon	Pandak	At-Large Urban	Commonwealth Transportation Board
J. Douglas	Koelemay	Northern Virginia District	Commonwealth Transportation Board
Keith	Tignor	State Apiarist/Endangered Species Coordinator	Virginia Department of Agriculture and Consumer Services
Robbie	Rhur	Environmental Impact Review Coordinator	Virginia Department of Conservation and Recreation
René	Hypes	Project Review Coordinator, Natural Heritage Program	Virginia Department of Conservation and Recreation
Thomas A.	Faha	Regional Director, Northern Regional Office	Virginia Department of Environmental Quality
Alma	Banks	Administrative Manager, Air Division	Virginia Department of Environmental Quality
Nichelle	McDaniel	SARA Title III/Toxics Release Inventory	Virginia Department of Environmental Quality
Trisha	Beasley	Office of Wetlands & Water Protection, Northern Regional Office	Virginia Department of Environmental Quality
Laura	McKay	Virginia CZM Program Manager	Virginia Department of Environmental Quality, Division of Environmental Enhancement
Shirl	Dressler	Fish and Wildlife Information Services	Virginia Department of Game and Inland Fisheries
Charles M.	Badger	Director	Virginia Department of Rail and Public Transportation
Corey	Hill	Chief of Public Transportation	Virginia Department of Rail and Public Transportation
David	Awbrey	DRPT-Urban Transit Program Administrator	Virginia Department of Rail and Public Transportation
Michael	Harris	DRPT-Project Manager	Virginia Department of Rail and Public Transportation
Valerie	Pardo	Multimodal Liaison	Virginia Department of Transportation
Morteza	Salehi	District Administrator, Northern Virginia District	Virginia Department of Transportation
Katherine	Tracy	Asst. Secretary to the CTB	Virginia Department of Transportation
Paul	Kraucunas	Land Development Section Manager	Virginia Department of Transportation, Land Development
Steve	Bates	Senior Transportation Engineer	Virginia Department of Transportation, Location and Design
Marc E.	Holma	Office of Review and Compliance	Virginia State Historic Preservation Office (SHPO)
Wayne	Turnage	Chief of Staff, Governor	Commonwealth of Virginia
Del. Albert C.	Eisenberg	47th District	Virginia House of Delegates
Del. Vivian E.	Watts	39th District	Virginia House of Delegates
Hon. Patricia S.	Ticer	30th District	Virginia State Senate
Hon. Mary Margaret	Whipple	31th District	Virginia State Senate



First Name	Last Name	Position	Organization
LOCAL AND REGIONAL			
Richard K.	Taube	Executive Director	Northern Virginia Transportation Commission
Adam	McGavock	Director of Transportation Projects	Northern Virginia Transportation Commission
Ronald	Kirby	Director, Department of Transportation Planning	Metropolitan Washington Council of Governments
Dr. Ronald T.	Buchanan	Acting Provost	Northern Virginia Community College
T. Dana	Kauffman	Director, Government Affairs and Community Relations	Northern Virginia Community College
Martin E.	Nohe	Chairman	Northern Virginia Transportation Authority
Mark	Gibb	Executive Director	Northern Virginia Regional Commission
Aimee	Vosper	Director, Environmental & Planning Services	Northern Virginia Regional Commission
Ron	Carlee	County Manager	Arlington County
Dennis	Leach	Director, Division of Transportation	Arlington County
Dinesh	Tiwari	Department of Parks, Recreation and Cultural Resources	Arlington County
Susan	Bell	Director, Department of Community Planning, Housing and Development	Arlington County
Patrick K.	Murphy	Superintendent	Arlington Public Schools
M. Douglas	Scott	Chief of Police	Arlington County Police Department
Terry	Holzheimer	Director	Arlington Economic Development
Anthony	Griffin	County Executive	Fairfax County
John W.	Dargle Jr.	Director	Fairfax County Park Authority
Fred	Selden	Planning Division Director	Fairfax County Dept. of Planning and Zoning
John Wesley	White	Director	Fairfax County Dept. of Public Works
Gerald	Gordon	Council President	Fairfax County Economic Development Authority
Susan	Herbert	Planner	Fairfax County Fire & Rescue Dept.
David M.	Rohrer	Chief of Police	Fairfax County Police Department
Jack D.	Dale	Superintendent	Fairfax County Public Schools
Kathy	Ichter	Director, Department of Transportation	Fairfax County
James	Hartmann	City Manager	City of Alexandria
Richard	Baier	Director, Transportation & Environmental Services	City of Alexandria
Hon. Barbara	Favola	Chairman	Arlington County Board of Supervisors
Hon. Jay	Fisette	Vice-Chairman	Arlington County Board of Supervisors
Hon. Mary	Hughes Hynes	Member	Arlington County Board of Supervisors
Hon. J. Walter	Tejada	Member	Arlington County Board of Supervisors
Hon. Christopher	Zimmerman	Member	Arlington County Board of Supervisors
Supervisor Catherine M.	Hudgins	Hunter Mill District	Fairfax County Board of Supervisors
Supervisor Michael R.	Frey	Sully District	Fairfax County Board of Supervisors
Supervisor John C.	Cook	Braddock District	Fairfax County Board of Supervisors
Supervisor Gerald W.	Hyland	Mount Vernon District	Fairfax County Board of Supervisors
Chairman Sharon	Bulova	Chairman, At-Large	Fairfax County Board of Supervisors
Supervisor Penelope A.	Gross	Mason District, Vice-Chairman	Fairfax County Board of Supervisors
Supervisor John W.	Foust	Dranesville District	Fairfax County Board of Supervisors
Supervisor Jeffrey C.	McKay	Lee District	Fairfax County Board of Supervisors
Supervisor Pat	Herrity	Springfield District	Fairfax County Board of Supervisors
Supervisor Linda	Smyth	Providence District	Fairfax County Board of Supervisors
Robert J.	Smith	Chairman	WMATA Board of Directors



November 13, 2009

Brian Glenn
Director, FTA Metropolitan Office
Federal Transit Administration, Washington Metro Office
1990 K Street NW, Suite 510
Washington DC 20006

Re: Columbia Pike Transit Initiative—Project Information and Agency Coordination Meeting

Dear Brian Glenn:

Your participation is requested in a project information and agency coordination meeting on December 1, 2009 to present and discuss the Columbia Pike Transit Initiative. The lead federal agency for this effort is the Federal Transit Administration (FTA) which has identified the probable NEPA class of action as an Environmental Assessment (EA). This invitation is a follow-up to an earlier project initiation letter which you received in late September.

At the December agency meeting the project team, consisting of Arlington County, Fairfax County, and WMATA, will provide a brief overview presentation of the project and request comment, feedback, and questions from attendees. Prior to the meeting, we encourage you to review the materials sent with the previous correspondence, study the project at its web site, www.piketransit.com, and consider areas of potential interest for discussion under your jurisdiction.

Meeting Details:

Tuesday, December 1 at 2:00 PM
Washington Metropolitan Area Transit Authority
600 5th Street NW, Board Meeting Room
Washington, DC 20001

If you have any questions about the December 1st meeting or would like to comment before then, please feel free to contact us at (202) 962-1114 or rmcelhennysmith@wmata.com. We thank you in advance for your participation and look forward to working with you on this important study.

Sincerely,

Robin McElhenny-Smith
WMATA Deputy Project Manager



November 13, 2009

Hon. Gerald E. Connolly
11th District
U.S. House of Representatives
4115 Annandale Road, Suite 103
Annandale, VA 22003

Re: Columbia Pike Transit Initiative—Project Information and Agency Coordination Meeting

Dear Hon. Gerald E. Connolly:

Your participation is requested in a project information and agency coordination meeting on December 1, 2009 to present and discuss the Columbia Pike Transit Initiative. The lead federal agency for this effort is the Federal Transit Administration (FTA) which has identified the probable NEPA class of action as an Environmental Assessment (EA). This invitation is a follow-up to an earlier project initiation letter which you received in late October.

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Meeting Details:

Tuesday, December 1 at 2:00 PM
Washington Metropolitan Area Transit Authority
600 5th Street NW, Board Meeting Room
Washington, DC 20001

If you have any questions about the December 1st meeting or would like to comment before then, please feel free to contact us at (202) 962-1114 or rmcelhennysmith@wmata.com. We thank you in advance for your participation and look forward to working with you on this important study.

Sincerely,

Robin McElhenny-Smith
WMATA Deputy Project Manager



October 30, 2009

Hon. Gerald E. Connolly
11th District
U.S. House of Representatives
4115 Annandale Road, Suite 103
Annandale, VA 22003

Re: Columbia Pike Transit Initiative—NEPA Documentation

Dear Hon. Gerald E. Connolly:

On behalf of Arlington and Fairfax Counties, and in cooperation with the Federal Transit Administration (FTA), the Washington Metropolitan Area Transit Authority (WMATA), is preparing an environmental document in accordance with the National Environmental Policy Act (NEPA) of 1969, as amended, for proposed transit improvements in Arlington and Fairfax Counties, VA. The proposed project, known as the Columbia Pike Transit Initiative, would extend along Columbia Pike (Route 244) from the Skyline complex in Fairfax County to Pentagon City in Arlington County, as described and depicted in the enclosures. This NEPA documentation continues the local alternatives analysis of 2005 and prepares the project for possible Federal funding.

The proposed transit improvements will support local travel along Columbia Pike and facilitate connections to the regional transit system. Columbia Pike is a busy thoroughfare in Northern Virginia that is experiencing rapid commercial and residential growth due to its proximity to Washington, D.C. In recent years several high-density, mixed-use development projects have been initiated along the corridor, increasing the already heavy demand for existing transit services. Furthermore, this segment of Columbia Pike links regional attractions including the Pentagon, Pentagon City, Bailey's Crossroads, and the Skyline complex.

The project team, which is comprised of Arlington County, Fairfax County and WMATA, requests that you review the enclosed documents, study the project at its web site, www.piketranisit.com, identify significant environmental issues for analysis and suggest reasonable alternatives for evaluation. The team greatly appreciates your input on this study and will be issuing an invitation to a November 2009 project information and agency coordination meeting.

Please note that Arlington County is advancing a parallel planning and NEPA documentation effort to address multimodal improvements of Columbia Pike in cooperation with the Federal Highway Administration (FHWA) and the Virginia Department of Transportation. While the



Columbia Pike Multimodal Project and the Columbia Pike Transit Initiative Project share much of the same study area, they are separate projects. The two projects will work cooperatively to share relevant information.

Should you need further information or have any questions, please contact us at (202) 962-1114, jdittmeier@wmata.com, or rmcelhennysmith@wmata.com. Please send your written input to:

Robin McElhenny-Smith, Deputy Project Manager
Washington Metropolitan Area Transit Authority
600 5th Street, NW
Room 5B-26
Washington, DC 20001

Sincerely,

A handwritten signature in blue ink that reads "John M. Dittmeier".

John Dittmeier
WMATA Project Manager, Columbia Pike Transit Initiative

A handwritten signature in blue ink that reads "Robin McElhenny-Smith".

Robin McElhenny-Smith
WMATA Deputy Project Manager, Columbia Pike Transit Initiative

Enclosures



Project Description and Fact Sheet

The current phase of the Columbia Pike Transit Initiative includes environmental documentation and preliminary engineering for proposed transit improvements extending five miles along Columbia Pike between Bailey's Crossroads/Skyline area in Fairfax County and Pentagon City in Arlington County, Virginia. This documentation continues the local alternatives analysis of 2005 and prepares the project for possible Federal funding.

The Columbia Pike Transit Initiative is one element of a decade-long effort by Arlington and Fairfax Counties to accommodate growing demand for transit service along this quickly redeveloping urban corridor. Citizens, planners, and community leaders have expressed desire for a modern, higher capacity transit system that supports expected levels of ridership and reinforces the "Main Street" environment envisioned for Columbia Pike.

Demographic Characteristics

- Population: 67,000 residents after growth of 10,000 between 1990 and 2000.
85,000 residents by 2030.
- Employment: 73,000 jobs within the study area.
100,000 jobs by 2030.

Transit Characteristics

- 15,000 weekday corridor ridership (WMATA and Arlington Transit (ART) bus services).
- Significant ridership increase with expanded PikeRide bus service.
- Transit and walk/bike trips are 25 to 30 percent of all corridor trips.
- Current PikeRide: branded service, some limited stop service, and signal priority on some routes.
- Future PikeRide: expanded signal priority and passenger information, plus "Super Stops".

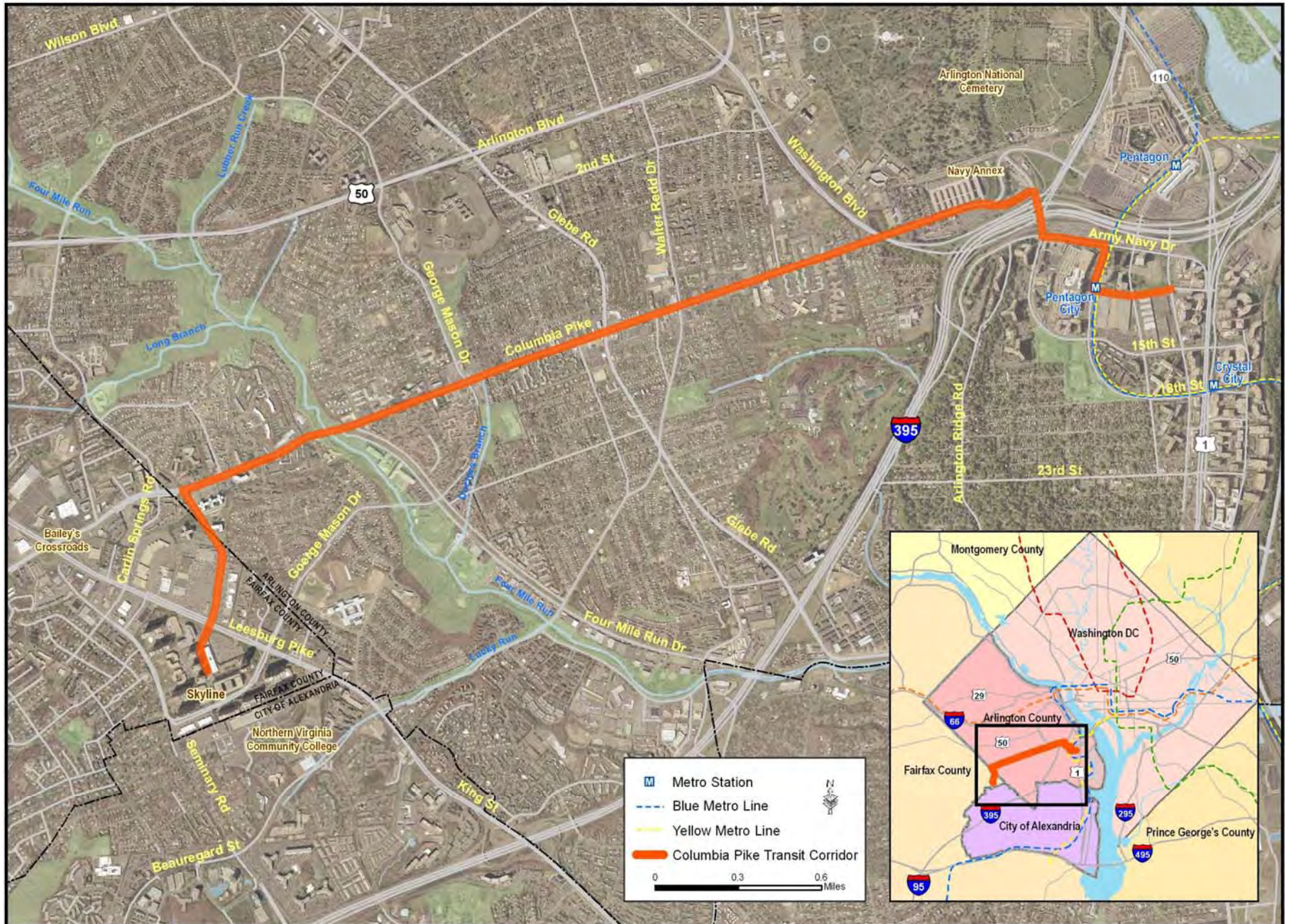
Proposed Improvements

The environmental documentation will evaluate the alternatives of no build, enhanced bus, and streetcar. As proposed, the transit improvements are expected to have the following features:

- Transit would generally operate in shared traffic lanes within existing streets.
- Stations/stops with improved shelters, passenger amenities, and real-time information.
- Fare pre-payment and integration with WMATA's SmartTrip system.
- Operations: six-minute all-day service supplemented by transit bus during peak hours.
- One primary vehicle storage and maintenance facility at the western end of corridor.

Conditions along the corridor are very urban with a mix of commercial and residential land uses. Most of the corridor has been disturbed over the years to make way for the various developments that exist. Very little natural environment exists with the exception of designated recreation areas, landscaped areas, and Four Mile Run and Doctor's Branch. The Columbia Pike Corridor is shown in the attached map. Please visit the project web site, www.piketransit.com, for more information.

Columbia Pike Transit Initiative COLUMBIA PIKE CORRIDOR





September 18, 2009

«First_Name» «Last_Name»

«Position»

«Organization»

«Address_1»

«Address_2» «Zip»

Re: Columbia Pike Transit Initiative—NEPA Documentation

Dear «First_Name» «Last_Name»:

On behalf of Arlington and Fairfax Counties, and in cooperation with the Federal Transit Administration (FTA), the Washington Metropolitan Area Transit Authority (WMATA), is preparing an environmental document in accordance with the National Environmental Policy Act (NEPA) of 1969, as amended, for proposed transit improvements in Arlington and Fairfax Counties, VA. The proposed project, known as the Columbia Pike Transit Initiative, would extend along Columbia Pike (Route 244) from the Skyline complex in Fairfax County to Pentagon City in Arlington County, as described and depicted in the enclosures. This NEPA documentation continues the local alternatives analysis of 2005 and prepares the project for possible Federal funding.

The proposed transit improvements will support local travel along Columbia Pike and facilitate connections to the regional transit system. Columbia Pike is a busy thoroughfare in Northern Virginia that is experiencing rapid commercial and residential growth due to its proximity to Washington, D.C. In recent years several high-density, mixed-use development projects have been initiated along the corridor, increasing the already heavy demand for existing transit services. Furthermore, this segment of Columbia Pike links regional attractions including the Pentagon, Pentagon City, Bailey's Crossroads, and the Skyline complex.

The project team, which is comprised of Arlington County, Fairfax County and WMATA, requests that you review the enclosed documents, study the project at its web site, www.piketranisit.com, identify significant environmental issues for analysis and suggest reasonable alternatives for evaluation. The team greatly appreciates your input on this study and will be issuing an invitation to a November 2009 project information and agency coordination meeting.

Please note that Arlington County is advancing a parallel planning and NEPA documentation effort to address multimodal improvements of Columbia Pike in cooperation with the Federal Highway Administration (FHWA) and the Virginia Department of Transportation. While the



Columbia Pike Multimodal Project and the Columbia Pike Transit Initiative Project share much of the same study area, they are separate projects. The two projects will work cooperatively to share relevant information.

Should you need further information or have any questions, please contact us at (202) 962-1114, jdittmeier@wmata.com, or rmcelhennysmith@wmata.com. Please send your written input to:

Robin McElhenny-Smith, Deputy Project Manager
Washington Metropolitan Area Transit Authority
600 5th Street, NW
Room 5B-26
Washington, DC 20001

Sincerely,

A handwritten signature in blue ink that reads "John M. Dittmeier".

John Dittmeier
WMATA Project Manager, Columbia Pike Transit Initiative

A handwritten signature in blue ink that reads "Robin McElhenny-Smith".

Robin McElhenny-Smith
WMATA Deputy Project Manager, Columbia Pike Transit Initiative

Enclosures



Project Description and Fact Sheet

The current phase of the Columbia Pike Transit Initiative includes environmental documentation and preliminary engineering for proposed transit improvements extending five miles along Columbia Pike between Bailey's Crossroads/Skyline area in Fairfax County and Pentagon City in Arlington County, Virginia. This documentation continues the local alternatives analysis of 2005 and prepares the project for possible Federal funding.

The Columbia Pike Transit Initiative is one element of a decade-long effort by Arlington and Fairfax Counties to accommodate growing demand for transit service along this quickly redeveloping urban corridor. Citizens, planners, and community leaders have expressed desire for a modern, higher capacity transit system that supports expected levels of ridership and reinforces the "Main Street" environment envisioned for Columbia Pike.

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85,000 residents by 2030.
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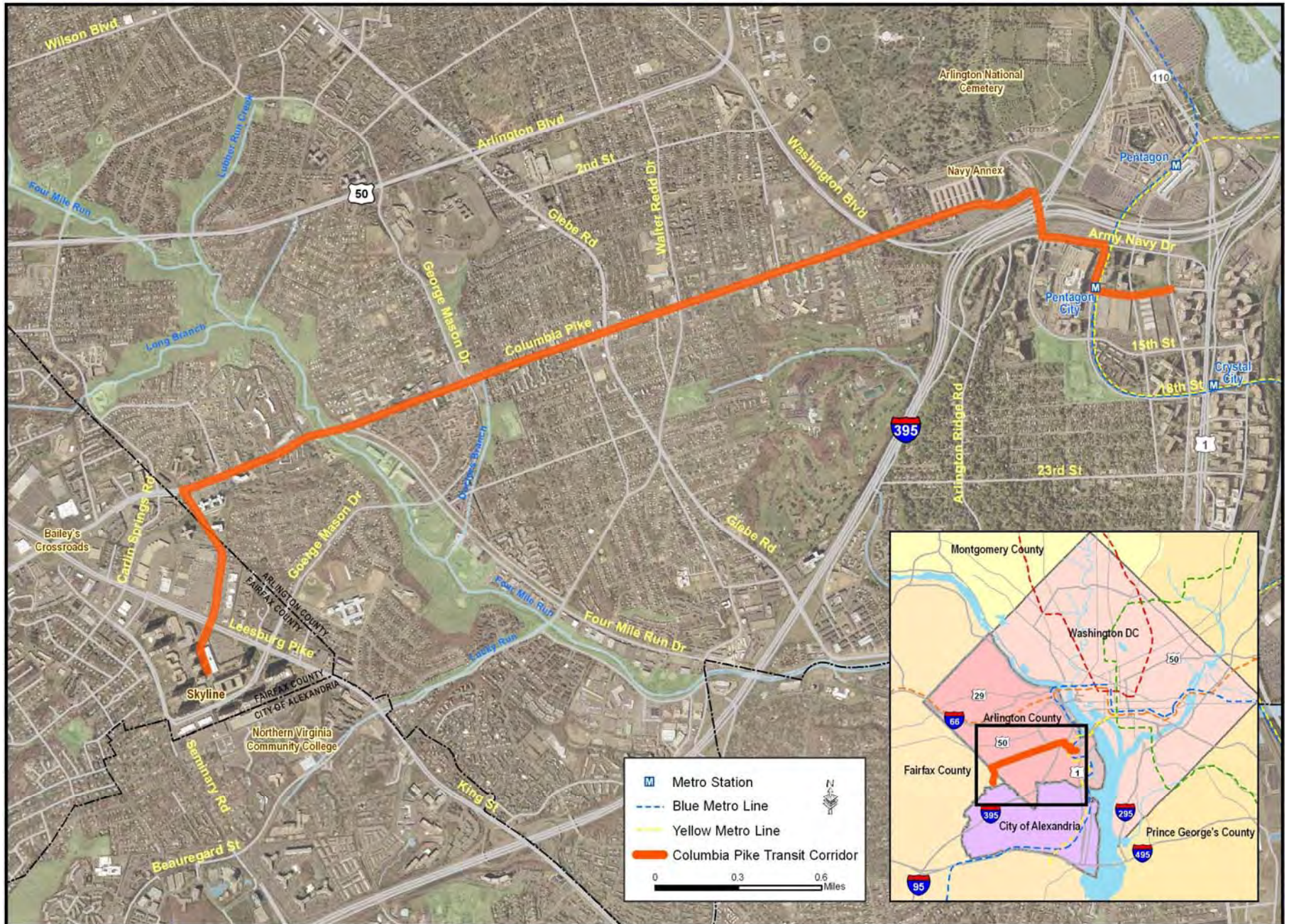
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Columbia Pike Transit Initiative COLUMBIA PIKE CORRIDOR



Correspondence with State Agencies and Officials



COMMONWEALTH of VIRGINIA

Department of Historic Resources

Douglas W. Domenech
Secretary of Natural Resources

2801 Kensington Avenue, Richmond, Virginia 23221

Kathleen S. Kilpatrick
Director

Tel: (804) 367-2323
Fax: (804) 367-2391
TDD: (804) 367-2386
www.dhr.virginia.gov

December 12, 2011

Melissa Barlow, Community Planner
Federal Transit Administration, DC Metro
1990 K Street NW, Suite 510
Washington, D.C. 20006

Re: Columbia Pike Transit Initiative – Revised Area of Potential Effects and Scope of Work
Arlington and Fairfax Counties
DHR File No. 2009-1506

Dear Ms. Barlow,

On November 14, 2011, the Virginia Department of Historic Resources (DHR) received additional information regarding the above-referenced project for our review and comment pursuant to Section 106 of the National Historic Preservation Act of 1966, as amended. We understand that the proposed project may be receiving federal funding through the Federal Transit Administration's (FTA) New Starts/Small Starts Program, and an Environmental Assessment is being prepared.

DHR understands that the Washington Metropolitan Transit Authority (WMATA), in coordination with Arlington and Fairfax Counties, proposes a series of transit improvements along a 4.94-mile corridor of Columbia Pike from the Bailey's Crossroads/Skyline Area in Fairfax County to Pentagon City in Arlington County. The scope of work and area of potential effects have been revised since DHR last commented in our letter dated January 31, 2011.

One streetcar alternative, two Transportation Systems Management (TSM) alternatives, and a "no build" alternative, inclusive of the Arlington County Super Stop Program and the Columbia Pike Multi-Modal Project, are currently being evaluated. Each of the alternatives generally share the same alignment and deviations are concentrated at the western and eastern ends. In addition to the project corridor, accompanying facilities are being proposed for the streetcar alternatives to include seven (7) stations, one operations and maintenance facility, four (4) traction powered substations, one transit center, one construction staging and equipment area, and tracks and catenary Lines servicing the streetcars. Additionally structural improvements to the Columbia Pike Bridge over Four Mile Run will be required.

Based upon a review of the information provided, we concur with the revised Area of Potential Effects (APE) for architectural and archaeological resources at this stage of development, with the understanding that it will be refined as the project develops, especially for archaeological resources.

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Petersburg, VA 23803
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Fax: (804) 862-6196

Capital Region Office
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Tel: (804) 367-2323
Fax: (804) 367-2391

Tidewater Region Office
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Newport News, VA 23608
Tel: (757) 886-2807
Fax: (757) 886-2808

Western Region Office
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Fax: (540) 387-5446

Northern Region Office
5357 Main Street
PO Box 519
Stephens City, VA 22655
Tel: (540) 868-7031
Fax: (540) 868-7033

December 12, 2011
Ms. Melissa Barlow
Page 2

We also concur with the identified sensitive areas and proposed scope of work for archaeological survey and monitoring. If the project scope changes and those changes require expanding the archaeological APE to areas not currently included in the APE, then the archaeological sensitivity and need for survey of the additional areas must be assessed. We also concur with the scope of work for architectural resources.

Should you have any additional questions, please contact me at (804) 482-6084, or via email at andrea.kampinen@dhr.virginia.gov.

Sincerely,



Andrea Kampinen, Architectural Historian
Office of Review and Compliance

Cc:

Katie Grasty, FTA
Daniel Koenig, FTA
John M. Dittmeier, WMATA
Alan Tabachnick, AECOM
Susan Anderson, AECOM
Jason Mumford, AECOM
Stephen Del Giudice, Arlington County
William Roberts, Arlington County
Leonard Wolfenstein, Fairfax County

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U.S. Department
of Transportation
**Federal Transit
Administration**

REGION III
Delaware, District of
Columbia, Maryland,
Pennsylvania, Virginia,
West Virginia

1760 Market Street
Suite 500
Philadelphia, PA 19103-4124
215-656-7100
215-656-7260 (fax)

NOV 10 2011

Ms. Andrea Kampinen, Architectural Historian
Office of Review and Compliance
Virginia Department of Historic Resources
2801 Kensington Avenue
Richmond, Virginia 23221

Subject: Section 106 Initiation Columbia Pike Transit Initiative

Dear Ms. Kampinen:

Arlington and Fairfax County, Virginia, in cooperation with the Federal Transit Administration (FTA) are proposing an undertaking to address an identified transportation need within the Columbia Pike Corridor, located in both jurisdictions. This project proposes a series of transit improvements along a five-mile corridor of Columbia Pike from the Bailey's Crossroads/Skyline Area in Fairfax County to Pentagon City in Arlington County, potentially inclusive of enhanced bus service or streetcar service operating in mixed traffic. The project is seeking federal funding through the FTA New Starts/Small Starts Program, which requires projects to conduct a federally approved Alternatives Analysis (AA) and prepare the appropriate National Environmental Policy Act (NEPA) documentation.

As part of this undertaking, FTA along with Arlington and Fairfax Counties intend to make every effort to identify historic properties and archaeological resources that could be affected by the proposed undertaking and fully assess potential effects. As a Federal undertaking, the project is subject to Section 106 of the National Historic Preservation Act of 1966, as amended, and the associated implementing regulations, 36 CFR Part 800.

On October 11, 2011 FTA and the project team met with VDHR staff to discuss proposed changes to the APE based on refinements to the proposed action. Based on these refinements, FTA would like to re-initiate consultation to address the cultural and historic resource issues, pursuant to the regulations implementing Section 106 of the National Historic Preservation Act (36 CFR Part 800). FTA is pleased to submit the attached Revised Area of Potential Effects (APE) and Scope of Work document for the Columbia Pike Transit Initiative project located in Arlington and Fairfax Counties, Virginia (DHR File No. 2010-1572).

FTA requests that you review the enclosed document and provide any relevant feedback regarding the revised APE and Scope of Work. Please feel free to contact Daniel Koenig of my staff at 202-219-3528 or daniel.koenig@dot.gov with any questions or concerns regarding the enclosed information. We appreciate your participation and look forward to your comments.

Sincerely,

A handwritten signature in cursive script, appearing to read "Brigid Hynes-Cherin", followed by a horizontal line.

Brigid Hynes-Cherin
Acting Region III Administrator

Enclosures - Proposed Area of Potential Effects and Scope of Work Document

cc: Melissa Barlow (FTA)
Jay Fox (FTA)
John Dittmier (WMATA)

Revised Area of Potential Effects and Scope of Work (11/16/2011)
Columbia Pike Transit Initiative
Arlington and Fairfax Counties
DHR File No. 2010-1572

Introduction

The Federal Transit Administration (FTA), in cooperation with Washington Metropolitan Area Transit Authority (WMATA) and Arlington and Fairfax Counties, is preparing a combined Alternatives Analysis/Environmental Assessment (AA/EA) in accordance with FTA guidance for the Section 5309 Small Starts Criteria, the National Environmental Policy Act (NEPA) of 1969, as amended, and Section 106 of the National Historic Preservation Act (36 CFR Part 800), as amended, for the Columbia Pike Transit Initiative. The current phase of the Columbia Pike Transit Initiative includes alternatives analysis, environmental documentation and conceptual engineering to support proposed transit improvements extending 4.94 miles along Columbia Pike between Bailey's Crossroads/Skyline area in Fairfax County and Pentagon City in Arlington County, Virginia.

The purpose of this document is to provide revised Areas of Potential Effects (APE) for both historic architectural and archaeological resources (**Figure 1**) and provide supporting documentation for the development of the APEs as part of the Section 106 Review process with VDHR for the Columbia Pike Transit Initiative project. A revised Scope of Work is included as well as an overview of proposed alternatives (**Appendix A**).

Previous Correspondence with VDHR

The Columbia Pike Transit Initiative project team has had the following correspondence with VDHR to date:

- **November 9, 2010:** Project team met with VDHR to go over the project details at that time.
- **January 19, 2011:** FTA submitted a project initiation package for the project, which included an APE and scope of work for review.
- **January 31, 2011:** VDHR concurred with the recommended APE and scope of work for the project presented in the January 19, 2011 package
- February 8, 2011: Project field tour with VDHR.

Revised Project Alternatives Description

The proposed project remains the same: to provide improved transit along Columbia Pike, generally between Skyline and Pentagon City. However, over the last year, the project has undergone more detailed analysis, which has resulted in the need to modify the proposed alternatives and prompted the need for this revised APE and scope of work. At the time of initial contact, the project presented a No Build Alternative; two enhanced bus alternatives, or Transportation Systems Management (TSM) alternatives; and a Streetcar Build Alternative with several design options. The Streetcar Build Alternative design options were married to potential sites for a storage and maintenance facility with variations in the proposed alignment to access the sites. The project alternative definitions have been modified as follows:

- **No Build Alternative:** The No Build Alternative has been modified to include construction of all transit stops along Columbia Pike within the project study area through Arlington County's Super Stop program. Additionally, the Arlington County Columbia Pike Multimodal Project has advanced beyond the transit initiative and will

now be included fully into the No Build Alternative. All other features of the No Build Alternative remain the same.

- **TSM 1 and 2 Alternatives:** No Change, other than what is described for the No Build Alternative.
- **Streetcar Build Alternative:** The project team has narrowed the potential storage and maintenance facilities to one location in Pentagon City; thus previous design options that were to evaluate a storage and maintenance facility at the Northern Virginia Community College (NOVA) and in Crystal City are no longer considered.

All the alternatives generally share the same alignment and deviations are concentrated at the west and east ends. Generally, the shared alignment extends north from Skyline in Fairfax County to Baileys Crossroads along Jefferson Street and then continues east-northeast along Columbia Pike in Arlington County to the Navy Annex Building. It then turns southeast and continues to the east along Army Navy Drive, South Hayes Street and 12th Street South to South Eads Street in Pentagon City. In addition to the project corridor, accompanying facilities are being proposed and include approximately 7 stations, one operations and maintenance facility, four traction power substations (TPSS), one transit center, one construction staging and equipment storage area, as well as tracks and catenary lines servicing the streetcars. Additionally structural improvements to the Columbia Pike Bridge over Four Mile Run will be required as part of the project. Because the TSM1 and “no build” alternatives include no infrastructure development, an analysis of the potential impacts of these alternatives is omitted from the current discussion.

As stated above, station locations previously proposed along Columbia Pike as part of the Columbia Pike Initiative project are now being built as part of Arlington County’s Columbia Pike Super Stops Project. Four of the proposed stations have already been found to meet the criteria for a Programmatic Categorical Exclusion (PCE) based on an agreement approved by the Federal Highway Administration (FHWA) on December 29, 2004. All remaining stops to be built by the Super Stops program will also be evaluated through that program and it is expected that they will all qualify for the same PCE.

Additionally, Columbia Pike between the Fairfax County Line and the easternmost exchange with South Washington Boulevard (VA-27) was previously surveyed as part of the Columbia Pike Multimodal Streets Improvement Project (VDHR File No. 2010-2030). The APE for that project measured approximately 3.8 miles long and encompassed approximately 36.85 acres. Despite substantial twentieth-century development, several areas were assessed to have moderate to high potential for intact prehistoric or historic archaeological materials. Since proposed project activities were limited to minor grading for street resurfacing and sidewalk replacement as well as limited deep excavation for utility relocation, proposed project activities were considered to have little potential for impacting “significant *in situ* archaeological resources” (EAC/A 2011: 75); no additional archaeological testing was recommended unless final project plans required expansion of more than five feet beyond existing sidewalks.

The Columbia Pike Multimodal Streets Improvement Project also conducted a reconnaissance survey of historic architectural resources within the project area. The consultant found 48 previously recorded historic architectural resources and identified an additional 55 historic architectural resources over 50 years of age within the project APE. Based on the limited activities associated with the project, the consultant recommended a finding of no adverse effect to historic architectural resources within the project APE.

On August 26, 2011, VDHR concurred with the “no adverse effect” findings for both archaeological and historic architectural resources within the APE for the Columbia Pike Multimodal Street Improvements project (Holma 2011).

Revised Area of Potential Effects for Archaeology

Because only conceptual engineering is available at the current time, the proposed APE for archaeology has been developed to be sufficient to encompass any areas of potential direct impact that might be required for the current undertaking. Such areas include but are not limited to permanent and temporary right-of-ways and easements, construction staging areas and materials storage areas, as well as the locations of all proposed improvement options (e.g. maintenance facilities, TPSS locations, signage, transit stations, etc.) (**Figure 2** and **Figure 3**) As engineering plans mature the extent of the APE for archaeology will be modified to eliminate those areas where no subsurface disturbance is anticipated.

The proposed APE for archaeology currently consists of a 15-meter buffer on both sides of the centerline of proposed trackage as well as the footprint of construction for all proposed traction power substation (TPSS) locations and proposed improvements. A single previously documented archaeological resource, the Alexandria Canal (44AX0028) has been recorded with the VDHR within the current APE.

The majority of the proposed project corridor has been subjected to significant 20th century commercial, residential and industrial development and attendant ground disturbance. However, sixteen areas of moderate to high archaeological sensitivity (**Table 1, Figures 2 and 3**) have been identified to date based on several criteria:

- Areas that appear to be testable open ground not currently occupied by standing structures, sidewalks, paved parking areas, etc.;
- These areas are proximate to historically documented structures identified through historic map research;
- And/or these areas are proximate to physiographic features and landforms typically associated with prehistoric site locations (e.g. stream confluences, upland terraces, etc.).

Table 1: Areas of Archaeological Sensitivity

Area #	Location	Notes
1	Grassy areas on the south side of Jefferson Pike between South Jefferson Street and South George Mason Drive	Undeveloped at-grade grassy area previously occupied by several middle 20 th century buildings (USGS 1945; NETR Online 2010). The presence of a retaining wall at the western end of this area indicates some potential for significant disturbance at that location; however some at-grade surfaces exist between the retaining wall and Jefferson Pike.
2	Northeast corner of the intersection of South Jefferson Street and Leesburg Pike.	Undeveloped grassy area at the approximate location of a middle 20 th century building/residence (USGS 1945; NETR Online 2010).
3	On Columbia Pike at the Four Mile Run Bridge between South Arlington Mill Drive and South Four Mile Run Drive.	Map research indicates there may be historic 19 th century archaeological deposits at this location associated with Arlington Station, the Washington and Ohio Railroad and the "Sarah Dvdhon" residence (Hopkins 1879). Intermittent paving and excessive slopes (>15%) may reduce the testable area at this location considerably.
4	The Four Mile Run floodplain below the Columbia Pike Bridge	Deeply buried deposits may exist within the Four Mile Run floodplain.
5	South side of Columbia Pike between its intersections with South Thomas and South Taylor Streets.	Grassy yard areas at the approximate location of the 19 th century "Wm Mack" residence (Hopkins 1879).
6	North side of Columbia Pike between South Monroe Street on the west and South Lincoln Street on the east.	Grassy yard area adjacent to the Arlington Presbyterian Church. Research indicates that the church was chartered in 1908, the original building was destroyed by fire in 1924 and rebuilding began in 1930 (APC). Intact early 20 th century deposits associated with the original church building may be present at this location.
7	South side of Columbia Pike between South Barton Street and South Adams Street.	Historic mapping (USGS 1945) indicates several structures at this location suggesting that intact early-middle 20 th century archaeological deposits may be present.
8	North side of Columbia Pike between South Wayne Street on the west and South Veitch Street on the east.	Grassy yard area around Trinity Episcopal Church; research indicates that the church was built at this location in 1903 (TEC); intact early 20 th century deposits associated with the church may be present at this location.
9	North side of Columbia Pike west of South Scott Street.	Yard area surrounding St. John's Baptist Church, former site of the Arlington Episcopal Church (c. 1878).

Area #	Location	Notes
10	South side of Columbia Pike west of South Orme Street.	Approximate former location of the late 19 th century “Rich. ^d Johnson” residence (Hopkins 1879).
11	North side of Columbia Pike west of South Orme Street.	Yard area south of the Sheraton National Hotel; former site of Arlington Chapel.
12	South side of Columbia Pike east of South Oak Street.	Approximate former site of the late 19 th century “J.R. Johnson” residence and “N.S. Wright” residence and store (Hopkins 1879).
13	West side of Columbia Pike east of the Air Force Memorial.	Approximate former location of the Columbia Pike “Toll Gate” and late 19 th century “H.S. Johnson” residence (Hopkins 1879).
14	East side of Columbia Pike east of the Air Force Memorial.	Approximate former location of the Columbia Pike “Toll Gate”, the late 19 th century “H.S. Johnson” store and “B.S. Sh.” (Blacksmith shop?) (Hopkins 1879).
15	West side of South Eads Street west of 12 th Street South.	Approximate former location of the Chesapeake and Ohio Canal/Alexandria Canal (44AX0028) (c.1833 – c. 1886) (Hopkins 1979; USGS 1885; CoA).
16	East side of South Eads Street north of 12 th Street South.	Approximate former location of the Potomac Brick Works (Hopkins 1879).

Additionally, the current right-of-way of Columbia Pike has the potential to contain archaeological deposits associated with the original construction of the turnpike (c.1810) and its subsequent operation.

Revised Area of Potential Effects for Historic Architectural Resources

The APE for historic architectural resources is the geographic area or areas within which an undertaking may directly or indirectly cause alterations in the character or use of historic properties, if any such properties exist (**Figures 1-3**). Based on a preliminary site visit and a review of the project mapping, the proposed APE includes:

- All buildings fronting the proposed alignment;
- Any buildings expected to be visible from the proposed alignment (e.g. buildings fronting on intersecting streets from which the proposed alignment would likely be visible);
- Any buildings/properties that have the potential to be physically or visually affected by associated tracks, catenary lines, traction power substations, the transit center, the operations and maintenance facility, or the construction staging and equipment storage area.

Any substantive changes to the location or design of the proposed alignment or associated buildings and features would require a reassessment of the proposed APE.

Preliminary Findings

Based upon preliminary background studies, a preliminary site visit, and consultation with the Columbia Pike Multimodal Streets Improvements Project team, the APE contains approximately 116 historic architectural resources that are 50 years of age or older. Resources include:

- Two (2) National Register-listed historic districts (000-0024 and 000-8823); one (1) National Register-listed object (000-0022); and one (1) multiple property themed district (MPDF) (000-8825);
- Four (4) National Register-eligible historic districts (000-0042, 000-7818, 053-0276, and 000-9419), and one (1) National Register-eligible building (000-3371);
- Twelve (12) resources determined individually ineligible for listing in the National Register by VDHR;
- One (1) resource previously recorded in the DSS, but not evaluated for eligibility (029-5470); and
- Approximately 13 previously unidentified and undocumented resources.

In addition, the Columbia Pike Multimodal Streets Improvement Project study included the following recommendations for resources in the APE:

- Five (5) individual resources (000-4539, 000-2266, 000-4530, 000-4524 and 000-4503) and one district (000-9418) were recommended eligible for listing in the National Register;
- Thirty-three (33) resources were recommended not eligible for listing in the National Register (see table);
- Forty (40) resources were not recommended individually eligible, but recommended as contributing resources to an existing or potential historic district; and
- Four (4) resources did not appear to meet the 50 year cut-off and will have to be re-evaluated at the appropriate time.

For a more detailed description of the historic architectural resources in the APE for the Columbia Pike Initiative Project, see Table 2 below. Those resources that have been previously identified have already been assigned DSS numbers. Previously unidentified resources are noted as such and are lacking DSS numbers. The table is organized from west to east in the project corridor.

Table 2: Historic Architectural Resources in the APE

VDHR DSS #	Name	Address	City	Date	Description	NR Status/Previous Recommendation	Multimodal (MM) 2011 Recommendation*
	Holy Cross Romanian Church**	5150 Leesburg Pike	Falls Church	c. 1900	Frame church	Previously unidentified	Out of MM APE
	Golden Gate Apartments**	3529 Leesburg Pike	Falls Church	c. 1960	Apartment complex	Previously unidentified	Out of MM APE
000-0022	Boundary Markers of the original District of Columbia MPD (DC) – Boundary Marker SW6	S. Jefferson St.	Falls Church	c. 1790; Moved 1965	stone boundary marker	NR Listed: 1-28-1991 VLR Listed: 8-21-1990	Out of MM APE
	Grandview Apartments**	3404 Carlin Springs Road	Falls Church	c. 1960	apartment building	Previously unidentified	Out of MM APE
000-4542		5555 Columbia Pike	Arlington	1950	commercial/offices	Previously unidentified	Not individually eligible
000-1115	Greenbriar Gardens	871-877 S. Greenbriar St.	Arlington	1949	garden apartments	Surveyed, not evaluated (1996); Contributes to the Garden Apartments in Arlington Virginia MPDF (000-8825) (2003)	Not individually eligible; May contribute to the Arlington County Garden Apartments MPDF (000-8825)
000-4541	Food Mart	5511 Columbia Pike	Arlington	c. 1955	commercial/gas station	Previously unidentified	Not individually eligible
000-4540	Palazzo Apartments	5353 Columbia Pike	Arlington	post-1960	commercial/gas station	Previously unidentified	Not individually eligible
000-4539	Arlington Auto	5200 Columbia Pike	Arlington	c. 1955	commercial/gas station	Previously unidentified	Potentially individually eligible
000-1117	Magnolia Gardens	880 S. Frederick St.	Arlington	1948	garden apartments	Surveyed, not evaluated (1996); Contributes to the Garden Apartments in Arlington Virginia MPDF (000-8825) (2003)	Not individually eligible; Potentially eligible through the Arlington County Garden Apartments MPDF (000-8825)

VDHR DSS #	Name	Address	City	Date	Description	NR Status/Previous Recommendation	Multimodal (MM) 2011 Recommendation*
000-4538	Frederick Courts	5104 Columbia Pike	Arlington	1947-1948	Colonial Revival garden apartment complex	Contributes to the Garden Apartments in Arlington Virginia MPDF (000-8825) (2003)	Potentially eligible through the Arlington County Garden Apartments MPDF (000-8825)
000-4537	Infinity Apartments	4990 Columbia Pike; 955 S. Columbus St.	Arlington	c. 1950	apartment building	Previously unidentified	Potentially eligible through the Arlington County Garden Apartments MPDF (000-8825)
000-4536	Columbia Pike Plaza	5001-5037 Columbia Pike	Arlington	1959	commercial shopping center	Previously unidentified	Not individually eligible
000-7818-0001	Barcroft Shopping Center	4901 Columbia Pike	Arlington	1950	commercial strip	Surveyed, Not evaluated (1996)	Not individually eligible
053-0276	Washington & Old Dominion Railroad Regional Park (W&OD Trail)	Crosses Columbia Pike at Four Mile Run	Arlington		Former railroad converted to recreational trail	NR Eligible: 2-4-1999	Not evaluated by MM
000-2266	Columbia Pike Bridge (Bridge #1008)	Route 244/Columbia Pike Bridge	Arlington	1941; altered 1958	concrete bridge	Not Eligible(1995)	Potentially individually eligible
000-7818	Barcroft Historic District		Arlington	c. 1892	Garden apartment complex	NR Eligible: 7-12-2001	Not evaluated by MM

VDHR DSS #	Name	Address	City	Date	Description	NR Status/Previous Recommendation	Multimodal (MM) 2011 Recommendation*
000-9419-0024, 000-9419-0025, 000-9419-0026, 000-9419-0027, 000-9419-0047, 000-9419-0048, 000-9419-0050, 000-9419-0053, 000-9419-0054, 000-9419-0055	Barcroft Apartments Historic District	4200-4600 Columbia Pike; 1130 S. George Mason Dr.	Arlington	post-1939	garden apartments complex	NR Eligible: 12-13-2007	Still eligible district (000-9419); buildings still contributing
000-4535	Peruvian Chicken	4707 Columbia Pike	Arlington	post-1960	Google commercial	Previously unidentified	Not currently individually eligible – reevaluate
000-9697	Atlatcatl	4701 Columbia Pike	Arlington	1919	commercial restaurant - altered	Not Eligible (2006)	Not individually eligible
000-9419-0058		4700-4714 Columbia Pike	Arlington	post-1939	brick commercial strip	Contributes to the Barcroft Apartments Historic District (000-9419) (2007)	Still contributing to eligible district (000-9419)
000-4534	Tire Store	4615 Columbia Pike	Arlington	c. 1950	commercial/gas station	Previously unidentified	Not individually eligible
000-9696	Latino Market	4611 Columbia Pike	Arlington	1955	commercial	Not Eligible (2005)	Not individually eligible
000-9695	Auto Plus	4601 Columbia Pike	Arlington	1951	gas station	Not Eligible (2005)	Not individually eligible
000-4533	Tanglewood Apartments	4241 S. Taylor St.	Arlington	post-1960	apartment complex	Previously unidentified	Not individually eligible
000-4532	Food Star	950 S. George Mason Dr.	Arlington	1959	commercial shopping center	Previously unidentified	Not individually eligible

VDHR DSS #	Name	Address	City	Date	Description	NR Status/Previous Recommendation	Multimodal (MM) 2011 Recommendation*
000-4531	Wachovia Bank	951 S. George Mason Dr.	Arlington	post-1961	International style commercial	Previously unidentified	Not currently individually eligible – reevaluate
000-4530	Shell/Liberty	4211 Columbia Pike	Arlington	c. 1955	commercial/gas station	Previously unidentified	Potentially individually eligible
000-4529		4103-4111 Columbia Pike	Arlington	c. 1950	commercial strip	Previously unidentified	Not individually eligible
		912 S. Randolph St**	Arlington	c. 1950	Colonial Revival house	Previously unidentified	Out of MM APE
000-4528	Quebec Apartments	4010-4032 Columbia Pike	Arlington	c. 1950	garden apartment complex	Previously unidentified	Potentially eligible through the Arlington County Garden Apartments MPDF (000-8825))
		908 S. Randolph St.**	Arlington	c. 1950	bungalow house	Previously unidentified	Out of MM APE
000-4527	Westmont Gardens	3860 Columbia Pike	Arlington	c. 1970	garden apartment complex	Previously unidentified	Not eligible
000-4526	Oakland Apartment	3710 Columbia Pike	Arlington	1954-1956	garden apartment complex		Potentially eligible through the Arlington County Garden Apartments MPDF (000-8825)
		3712 9th Street South**	Arlington	c. 1935	Tudor Revival house	Previously unidentified	Out of MM APE
		3704 9th Street South**	Arlington	c. 1945	Bungalow house	Previously unidentified	Out of MM APE
		3700 9th Street South**	Arlington	c. 1935	Colonial Revival house	Previously unidentified	Out of MM APE
		3620 9th Street South**	Arlington	c. 1945	Bungalow house	Previously unidentified	Out of MM APE
000-4525	Rancho Migueleno	3709-3711 Columbia Pike	Arlington	c. 1965	commercial with ceramic tiled façade	Previously unidentified	Not currently individually eligible – reevaluate
000-4524	Good Guys	3700 Columbia Pike	Arlington	c. 1955	commercial/gas station	Previously unidentified	Potentially individually eligible

VDHR DSS #	Name	Address	City	Date	Description	NR Status/Previous Recommendation	Multimodal (MM) 2011 Recommendation*
000-4522		3621 Columbia Pike	Arlington	c. 1950	commercial building	Previously unidentified	Not individually eligible
000-4521	The Broiler	3601 Columbia Pike	Arlington	c. 1930	commercial/auto shop	Previously unidentified	Potentially individually eligible
000-4523	El Pollo	3612 Columbia Pike	Arlington	c. 1950	commercial building	Previously unidentified	Not individually eligible
000-2123		919 S Monroe Street	Arlington	1940	Moderne concrete house	Surveyed, not evaluated (1997)	Not individually eligible; Recommend review of Alcova Heights
000-2117		918 S Monroe Street	Arlington	1921	Craftsman house	Surveyed, not evaluated (1997)	Not individually eligible; Recommend review of Alcova Heights
000-4520		3533 Columbia Pike	Arlington	post-1960	commercial building	Previously unidentified	Not individually eligible
000-2219	Arlington Presbyterian Church	3507 Columbia Pike	Arlington	1930	stone church	Surveyed, not evaluated (1997)	Not individually eligible; recommend review of Alcova Heights
000-4519	Rosenthal Auto Dealership	3400 Columbia Pike	Arlington	c. 1955	commercial/automobile showroom	Previously unidentified	Not individually eligible
000-2207	Westmont Shopping Center	3233-3263 Columbia Pike	Arlington	1940	altered commercial building; brick covered with dry-vit	Surveyed, not evaluated (1997)	Not individually eligible
000-4518		3217-3219 Columbia Pike	Arlington	post-1960	commercial building	Previously unidentified	Not individually eligible
000-4517		3215 Columbia Pike	Arlington	c. 1950	commercial	Previously unidentified	Not individually eligible
000-4516		3213 Columbia Pike	Arlington	c. 1950	commercial	Previously unidentified	Not individually eligible
000-4515	Good Fortune/Hair Cutter	3209-3211 Columbia Pike	Arlington	c. 1940	commercial duplex	Previously unidentified	Not individually eligible

VDHR DSS #	Name	Address	City	Date	Description	NR Status/Previous Recommendation	Multimodal (MM) 2011 Recommendation*
000-4514	Kabobs Inn	3205-3207 Columbia Pike	Arlington	c. 1940	commercial duplex	Previously unidentified	Not individually eligible
000-4513	Bambi's Grocery	3201-3203 Columbia Pike	Arlington	c. 1940	commercial complex	Previously unidentified	Not individually eligible
000-4512		3111 Columbia Pike	Arlington	c. 1960	commercial/res taurant	Previously unidentified	Not individually eligible
000-4213	Sun Trust Bank	3108 Columbia Pike	Arlington	1961	commercial and office	Surveyed, not evaluated (1997)	Not currently eligible – reevaluate
000-4511	Verizon	3102 Columbia Pike	Arlington	c. 1950	commercial	Previously unidentified	Not individually eligible
000-4510	Mancini de Paris	3045 Columbia Pike	Arlington	post-1960	commercial/offices	Previously unidentified	Not individually eligible
000-4509	Days Inn	3016 Columbia Pike	Arlington	post-1960	commercial/motel	Previously unidentified	Not individually eligible
000-2205, 000-9418	The Charles Building	3008-3014 Columbia Pike	Arlington	1937	commercial building	Surveyed, not evaluated (1997); Columbia Pike/ South Walter Reed Drive Com. HD (000-9418) recommended not eligible (2009)	Not individually eligible; Still contributes to the Walter Reed Commercial Historic District (000-9418)
000-2218	7-11	3003-3007 Columbia Pike	Arlington	1938	altered commercial building	Surveyed, not evaluated (1997)	Not individually eligible
000-9418-001		2921-2927 Columbia Pike	Arlington	c. 1955	commercial strip	Surveyed, not evaluated (1997); Columbia Pike/ South Walter Reed Drive Com. HD (000-9418) recommended not eligible (2009)	Not individually eligible; may be considered contributing to the potential Walter Reed Commercial Historic District (000-9418)

VDHR DSS #	Name	Address	City	Date	Description	NR Status/Previous Recommendation	Multimodal (MM) 2011 Recommendation*
000-2203, 000-9418-0013		2920 Columbia Pike	Arlington	1920	Commercial, façade only	Surveyed, not evaluated (1997); Columbia Pike/ South Walter Reed Drive Com. HD (000-9418) recommended not eligible (2009)	Not individually eligible; no longer contributing to the Walter Reed Commercial Historic District (000-9418)
000-2216, 000-9418-0002		2915-2919 Columbia Pike	Arlington	1936	commercial strip	Surveyed, not evaluated (1997); Columbia Pike/ South Walter Reed Drive Com. HD (000-9418) recommended not eligible (2009)	Not individually eligible; still contributing to the Walter Reed Commercial Historic District (000-9418)
000-2214, 000-9418-0003	Arlington Cinema	2901-2911 Columbia Pike	Arlington	1939	theatre and commercial buildings	Surveyed, not evaluated (1997); Columbia Pike/ South Walter Reed Drive Com. HD (000-9418) recommended not eligible (2009)	Individually eligible; still contributing to the Walter Reed Commercial Historic District (000-9418)
000-2201, 000-9418-0012	Old Dominion Bank	2900 Columbia Pike	Arlington	1900	Colonial Revival corner bank	Surveyed, not evaluated (1997); Columbia Pike/ South Walter Reed Drive Com. HD (000-9418) recommended not eligible (2009)	Not individually eligible; still contributing to the Walter Reed Commercial Historic District (000-9418)
000-2199, 000-9418-0004		2801-2811 Columbia Pike	Arlington	c. 1936	Moderne commercial building	Surveyed, not evaluated (1997); Columbia Pike/ South Walter Reed Drive Com. HD (000-9418) recommended not eligible (2009)	Individually eligible; should be contributing to Penrose HD (000-8823); still contributing to the Walter Reed Commercial Historic District (000-9418)

VDHR DSS #	Name	Address	City	Date	Description	NR Status/Previous Recommendation	Multimodal (MM) 2011 Recommendation*
000-2197	Arlington Post Office (Ski Chalet)	2704 Columbia Pike	Arlington	c. 1925	commercial building	Surveyed, not evaluated (1997)	Not individually eligible; still contributing to the Walter Reed Commercial Historic District (000-9418)
000-9418	Columbia Pike/ South Walter Reed Drive Commercial Historic District	Intersection of Columbia Pike and S. Walter Reed Dr.	Arlington	1900-1954	commercial district	Columbia Pike/ South Walter Reed Drive Com. HD (000-9418) recommended not eligible (2009)	Considered eligible district (000-9418); recommend expansion of boundary
000-9418-016		2630-2634 Columbia Pike	Arlington	c. 1955	commercial strip	Surveyed, not evaluated (1997); Columbia Pike/ South Walter Reed Drive Com. HD (000-9418) recommended not eligible (2009)	Not individually eligible; still contributing to the Walter Reed Commercial Historic District (000-9418)
000-2212, 000-9418-0007		2626-2628 Columbia Pike	Arlington	c. 1955	commercial strip	Surveyed, not evaluated (1997); Columbia Pike/ South Walter Reed Drive Com. HD (000-9418) recommended not eligible (2009)	Not individually eligible; still contributing to the Walter Reed Commercial Historic District (000-9418)
000-2210, 000-9418-0006	Arlington Animal Hospital	2624 Columbia Pike	Arlington	c. 1936	Colonial Revival commercial building	Surveyed, not evaluated (1997); Columbia Pike/ South Walter Reed Drive Com. HD (000-9418) recommended not eligible (2009)	Not individually eligible; still contributing to the Walter Reed Commercial Historic District (000-9418)
000-4508		1012 S. Cleveland St.	Arlington	post-1939	Colonia Revival house	Previously unidentified	Not individually eligible; may contribute to Arlington Village HD (000-0024)

VDHR DSS #	Name	Address	City	Date	Description	NR Status/Previous Recommendation	Multimodal (MM) 2011 Recommendation*
000-2195, 000-8823- 0508	Fillmore Shopping Center	2601-2705 Columbia Pike	Arlington	1948	commercial strip	Contributes to the Penrose Historic District (000-8823) (2004)	Contributes to Penrose HD (000-8823); still contributing to the Walter Reed Commercial Historic District (000-9418)
000-0024	Arlington Village Historic District		Arlington			NR Listed: 4-11-2003 VLR Listed: 12-4-2002	
000-2132; 000-0024- 0005	Arlington Village Center	2500-2530 Columbia Pike	Arlington	1939	commercial strip	Contributes to the Arlington Village Historic District (000-0024) (2003)	Contributes to NR listed Arlington Village HD (000-0024); May contribute to the Walter Reed Commercial Historic District (000-9418)
000-4507		2406-2408 Columbia Pike	Arlington	c. 1945	commercial strip	Previously unidentified	Not individually eligible; potential commercial district
000-4506		2338-2344 Columbia Pike	Arlington	c. 1945	commercial strip	Previously unidentified	Not individually eligible; potential commercial district
000-4505		2330 Columbia Pike	Arlington	c. 1945	commercial building	Previously unidentified	Not individually eligible; potential commercial district
000-4504	Citgo Gas Station	2324 Columbia Pike	Arlington	c. 1955	gas station	Previously unidentified	Not individually eligible; Non-contributing to potential commercial district
000-4503	Bob & Edith's Diner	2310 Columbia Pike	Arlington	1959	commercial/res taurant	Previously unidentified	Potentially individually eligible; potential commercial district
000-8823	Pennrose Historic District		Arlington	c. 1948	residential	NR Listed: 11-15-2004 VLR Listed: 12-3-2003	Not evaluated by MM

VDHR DSS #	Name	Address	City	Date	Description	NR Status/Previous Recommendation	Multimodal (MM) 2011 Recommendation*
000-8823-0480	Trinity Episcopal Church	2217 Columbia Pike	Arlington	1940s	church	Contributes to the Penrose Historic District (000-8823) (2004)	Still contributing to the Penrose Historic District (000-8823)
000-8823-0479	Samaritan House	2207 Columbia Pike	Arlington	1940s	bungalow house	Contributes to the Penrose Historic District (000-8823) (2004)	Still contributing to the Penrose Historic District (000-8823)
000-8823-0478		912 S Veitch St.	Arlington	1940s	Colonia Revival house	Contributes to the Penrose Historic District (000-8823) (2004)	Still contributing to the Penrose Historic District (000-8823)
000-4502	Key Apartments	2112 Columbia Pike	Arlington	c. 1960	apartment building	Previously unidentified	Not individually eligible
000-4501	Dorchester Tower	2001 Columbia Pike	Arlington	pos-1960	apartment building	Previously unidentified	Not individually eligible
000-0514	St. John's Baptist Church	1905 Columbia Pike	Arlington		church	Not Eligible (2005)	Not Eligible
000-4500	Dorchester Apartments	1900 Columbia Pike (office)	Arlington	c. 1950	garden apartment complex	Previously unidentified	Potentially eligible through Arlington Co. Garden Apts. MPDF (000-8825)
000-4499	Wellington	1850 Columbia Pike	Arlington	post-1960	apartment building	Previously unidentified	Not individually eligible
000-0512		1833 Columbia Pike	Arlington	c. 1920	Craftsman house	Not Eligible (1997)	Not Eligible
000-4498	Lancaster Apartments	1830 Columbia Pike	Arlington	c. 1960	apartment building	Previously unidentified	Not individually eligible
000-1668		1829 Columbia Pike	Arlington	c. 1925	Craftsman house	Surveyed, not evaluated (1997)	Not individually eligible
000-0511		1827 Columbia Pike	Arlington	c. 1915	gable front frame house	Not Eligible (1997)	Not Eligible
000-1647		1001 S. Rolfe St.	Arlington	c. 1925	Craftsman house	Not Eligible (1996)	Not individually eligible; May contribute to potential Arlington View HD

VDHR DSS #	Name	Address	City	Date	Description	NR Status/Previous Recommendation	Multimodal (MM) 2011 Recommendation*
000-1663		1806 Columbia Pike	Arlington	c. 1925	Colonial Revival house	Not Eligible (1996)	Not Eligible; May contribute to potential Arlington View HD
000-5001		1802 Columbia Pike	Arlington	c. 1925	bungalow house	Not Eligible (1997)	Not Eligible: 1997; May contribute to potential Arlington View HD
000-1661		1726 Columbia Pike	Arlington	c. 1920	Craftsman house	Not Eligible (1996)	Not Eligible: 1997; May contribute to potential Arlington View HD
000-1666		1724 Columbia Pike	Arlington	c. 1930	Colonial Revival house	Not Eligible (1996)	Not Eligible: 1997; May contribute to potential Arlington View HD
000-2267	Washington Blvd Bridge; Bridge #1024	Washington Blvd over Columbia Pike	Arlington	1944	bridge	Not Eligible (1995)	Not Eligible
000-4495		919-921 S. Orme St.	Arlington	c. 1940	Colonial Revival duplex	Previously unidentified	Not individually eligible; May contribute to potential Foxcroft Heights HD
000-1702		921 S. Ode St.	Arlington	1935	Colonial Revival	Surveyed, not evaluated (1997)	Not individually eligible; May contribute to potential Foxcroft Heights HD
000-4492	A-One Auto Clinic	1515 Columbia Pike	Arlington	c. 1945	Commercial/gas station	Previously unidentified	Not individually eligible; May contribute to potential Foxcroft Heights HD
000-4491		1501-1509 Columbia Pike	Arlington	c. 1945	commercial/restaurant	Previously unidentified	Not individually eligible; May contribute to potential Foxcroft Heights HD
		921 S. Oak St**	Arlington	c. 1935	Colonial Revival house	Previously unidentified	Not evaluated by MM

VDHR DSS #	Name	Address	City	Date	Description	NR Status/Previous Recommendation	Multimodal (MM) 2011 Recommendation*
000-1677		920 S. Oak St.	Arlington	1935	Colonial Revival house	Surveyed, not evaluated (1997)	Not individually eligible; May contribute to potential Foxcroft Heights HD
000-4219		Columbia Pike at Oak St.	Arlington	c. 1945	commercial/res taurant	Previously unidentified	Not individually eligible; Non-contributing to potential Foxcroft Heights HD
000-0046	VA State Police Area Office/VDOT	1440 Columbia Pike	Arlington	c. 1920	Spanish Revival brick bldg	Not Eligible (1992)	Not Eligible
000-3371	Navy Annex	Columbia Pike and Southgate Rd.	Arlington	1943	government bldg	NR Eligible (2004)	NR Eligible
000-0042	Arlington National Cemetery	Southgate Rd.	Arlington	Arlington House: 1802 ; Cemetery 1860s to present	cemetery	Eligible as NHL (1986); NR Eligible: 1991	Not in MM APE
	James at River House	1111 Army Navy Dr.**	Arlington	1957	Apartment building	Previously unidentified	Not in MM APE
		1201 S. Fern St.**	Arlington	1953	Commercial/re tail	Previously unidentified	Not in MM APE
	BMW of Arlington	1200 S. Eads St.**	Arlington	1956	Commercial/au to showroom	Previously unidentified	Not in MM APE
029-5470	Washington , Arlington, and Falls Church Electric Railway Co. Trolley Line		Arlington	Early 1900s	Trolley line; may no longer be extant	Surveyed, not evaluated	Not in MM APE

VDHR DSS #	Name	Address	City	Date	Description	NR Status/Previous Recommendation	Multimodal (MM) 2011 Recommendation*
000-8825	Arlington County Garden Apartments MPDF	various locations	Arlington	Mid-20 th century	garden apartments	VLR: 12-11-2002 NR: 5-22-2003	Not evaluated by MM
<p>MM = Multimodal Improvement Project</p> <p>APE = Area of Potential Effects</p> <p>NR = National Register</p> <p>VLR = Virginia Landmarks Register</p> <p>* = Multimodal eligibility recommendation submitted to VDHR on March 25, 2011 with revisions submitted on July 27, 2011; Note: recommendations made during cultural resource investigations for the Multimodal project were concurred with by Mark Holma on August 26, 2011.</p> <p>** = Will be surveyed and evaluated by the Columbia Pike Initiative</p>							

Revised Scope of Work

In addition to the Columbia Pike Initiative Project, Arlington County is conducting a separate project called the Columbia Pike Multimodal Streets Improvements Project. The latter consists of a transportation analysis and environmental studies for preliminary design of streetscape and related road improvements for pedestrians, bicycles, transit, and vehicles. The two projects share a 3.5-mile study area along Columbia Pike. The Columbia Pike Initiative extends further east and west than the Multimodal project, stretching approximately 5 miles in total. In an effort to avoid repetitive work in the overlapping areas, Arlington and Fairfax Counties are coordinating with the cultural resource team from the Multimodal project to share data.

Revised Scope of Work for Archaeology

A program of background research, sensitivity analysis, limited subsurface testing, geomorphological testing and construction monitoring will be conducted to identify intact archaeological resources that may exist within the project corridor, evaluate their potential eligibility for inclusion in the National Register of Historic Places (NRHP) or the Virginia Landmarks Register (VLR), and to assess the impact of proposed construction on those resources.

Background Research

Background documentary research on the environment, history and prehistory of the entire project corridor will be conducted. The purpose of this research will be twofold: to determine whether there are known sites that might be affected by the project; and to evaluate the potential range of site types that might be present and their characteristics in order to assess the likelihood that the APE contains undocumented sites. Background research will include an examination of the VDHR's archaeological site files, as well as examination of historic maps in state and local libraries and historical societies. It is anticipated the background research for the project will utilize a number of sources and repositories including but not limited to:

- The Archives of the Virginia Department of Historic Resources, Richmond, VA
- The Arlington Historical Society and Museum, Arlington, VA
- The Arlington Public Library
- The Library of Congress

Sensitivity Analysis

The results of the background research will be used to generate an archaeological sensitivity model of the APE in order to identify those areas most likely to contain archaeological resources as well as the nature of those resources.

Limited Subsurface Testing

Phase I subsurface testing in the form of regularly spaced shovel test pits (STPs) will be conducted in archaeologically sensitive areas where project engineering indicates subsurface disturbance is likely and where apparently testable open ground exists, that is areas not currently occupied by standing structures, sidewalks, paved parking areas, etc. This may include areas of Columbia Pike previously surveyed as part of the Columbia Pike Multimodal Streets Improvement Project should construction activities be required beyond limited road resurfacing and widening. As project engineering becomes finalized, additional areas for Phase I testing may be identified including but not limited to transit station and TPSS locations.

Geomorphological Assessment and Deep Testing

The potential for deeply buried deposits may exist within the floodplain of Four Mile Run (see **Table 1**, Area 4, above). Should forthcoming engineering indicate direct impacts to the floodplain, a geomorphological assessment of the floodplain will be conducted by a qualified geomorphologist to determine whether deeply buried surfaces exist there. Subsequent deep testing of the floodplain if deemed necessary by VDHR will be conducted.

Construction Monitoring

In areas of archaeological sensitivity where subsurface testing is not feasible and future engineering indicates that subsurface disturbance is likely (e.g. within the existing Columbia Pike right-of-way, under paved parking areas, beneath existing sidewalks etc.) a program of construction monitoring will be implemented to identify and document archaeological resources exposed during construction.

Scope of Work for Historic Architectural Resources

A Reconnaissance Level Survey and report will be undertaken to identify and evaluate every historic architectural resource within the APE over 50 years of age that has the potential to be affected by the proposed project.

Approximately 13 historic architectural resources within the APE have not been previously identified and will be surveyed and evaluated as part of Reconnaissance Level Survey Report. An additional resource, the Washington, Arlington, and Falls Church Electric Railway Co. Trolley Line will be inspected for extant features and evaluated as necessary. AECOM will update the existing DSS listing for the Washington, Arlington, and Falls Church Electric Railway Co. Trolley Line and complete DSS survey forms for 13 newly identified architectural resources, as stated above. AECOM will also include a table listing the resources evaluated by the Columbia Pike Multimodal Streets Improvement Project. AECOM will make eligibility recommendations for each historic architectural resources in the APE and provide an assessment of effects.

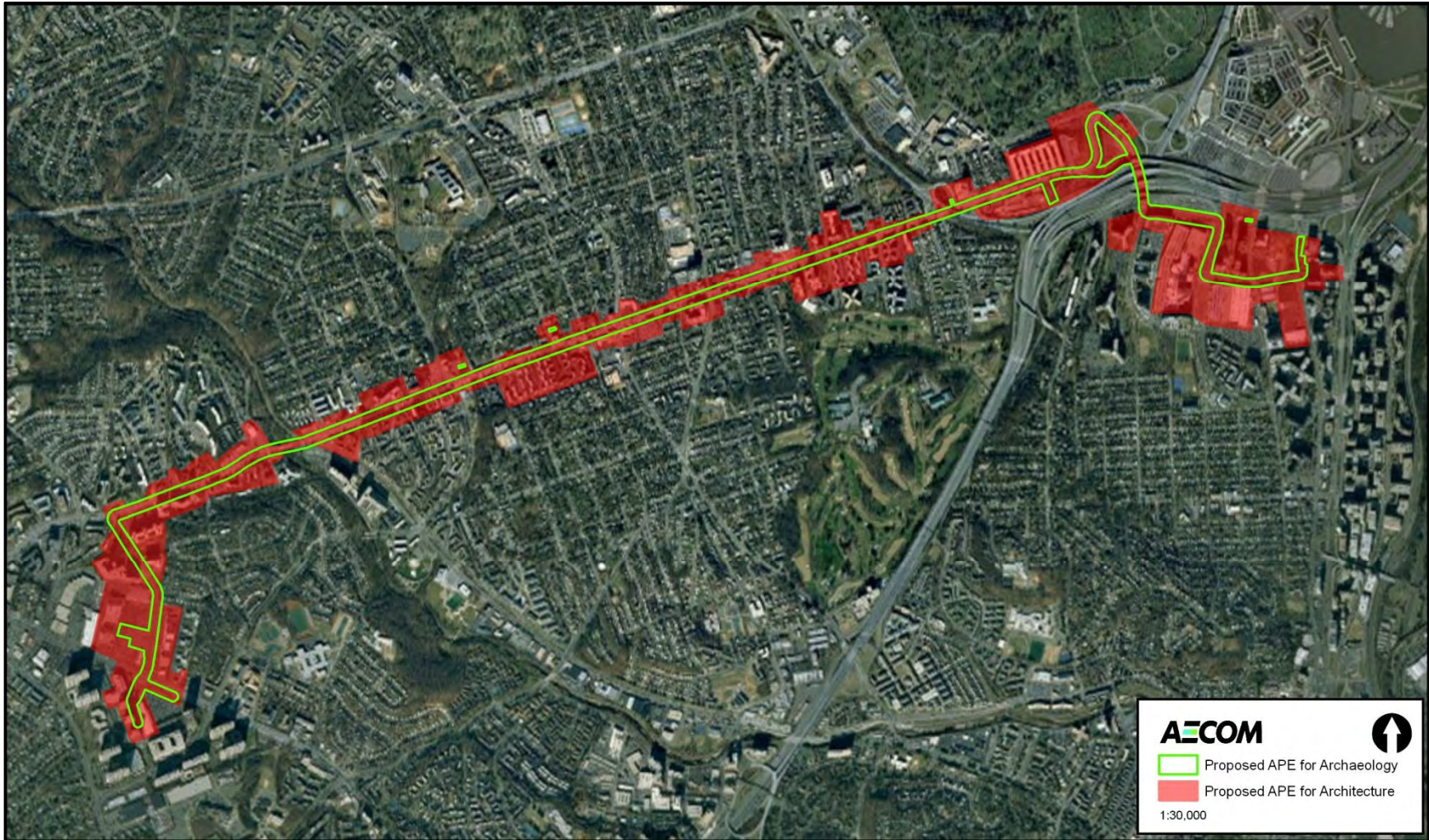


Figure 1: Project APE (ESRI 2011).

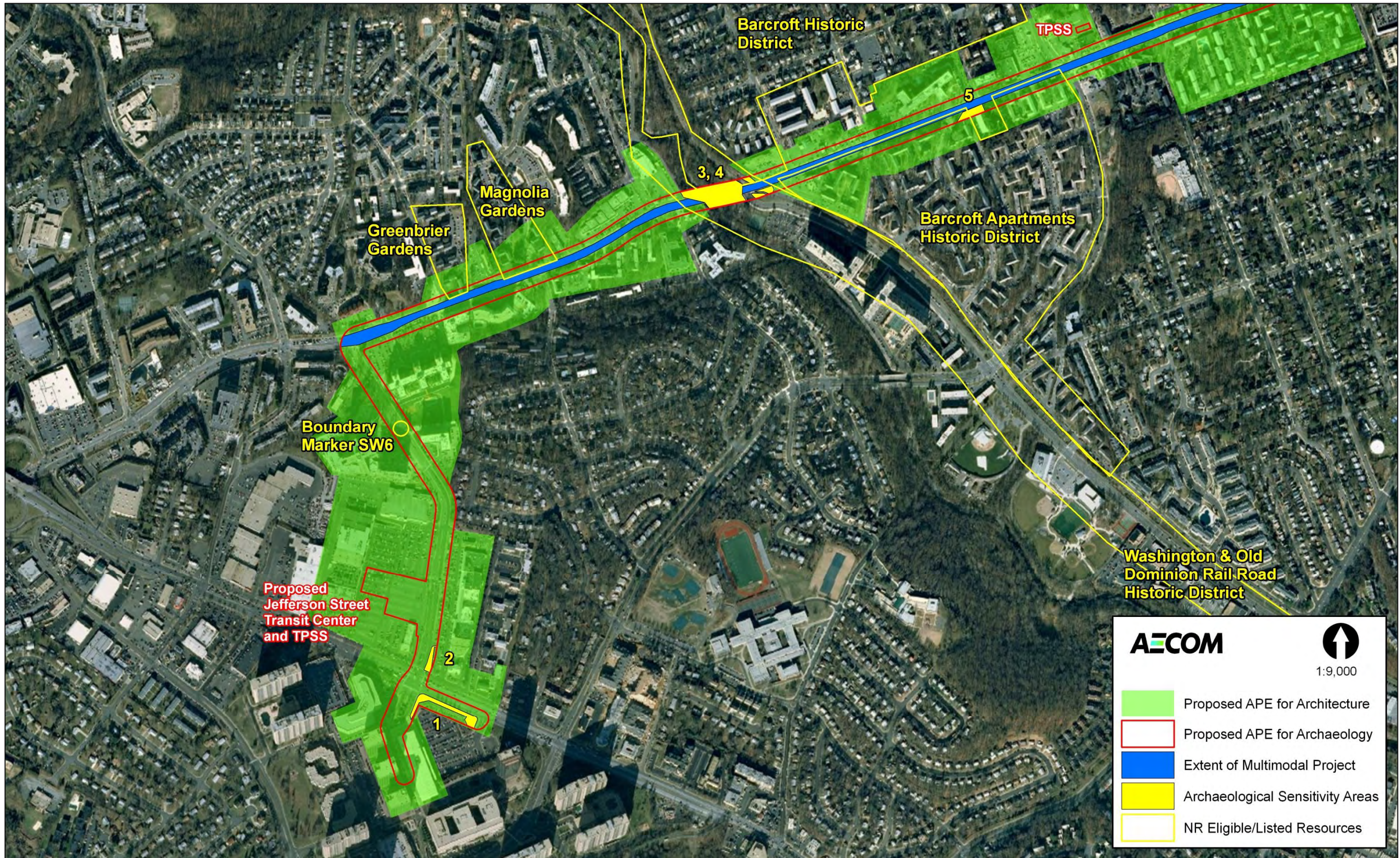


Figure 2: APE, Western Portion (ESRI 2011).

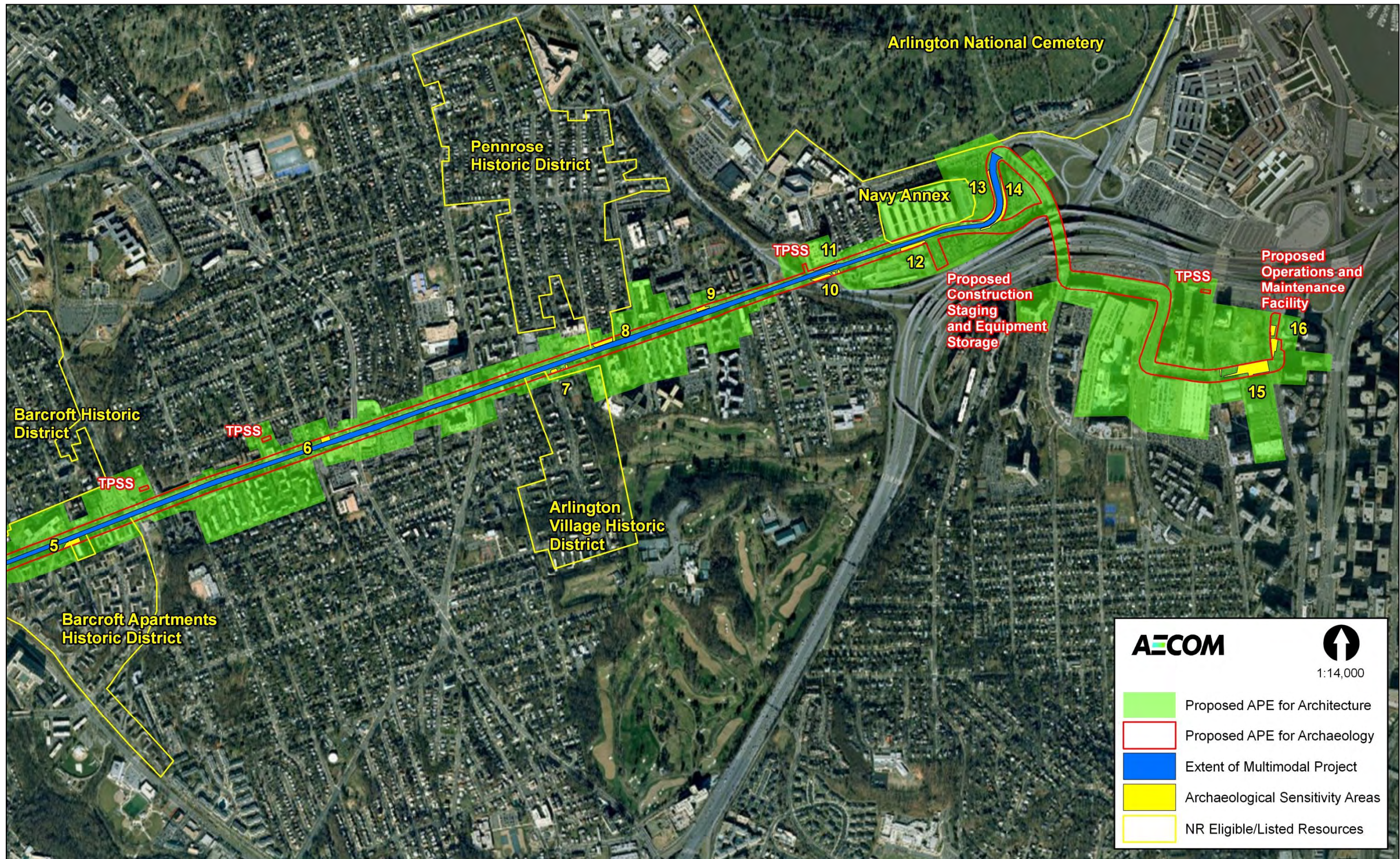


Figure 3: APE, Eastern Portion (ESRI 2011).

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**Appendix A:
Alternatives Overview**

**COLUMBIA PIKE TRANSIT INITIATIVE:
UPDATED ALTERNATIVES OVERVIEW: DEFINITIONS AND MAPS**

September 2011

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Table 1: Stop Locations by Alternative

Stop ID	General Location	No Build (local bus stop/Super Stop)	TSM 1 and 2	Streetcar Build (incl. platform location)
Local	Pentagon City (varies by route)	local	✓	background bus service
A	12th St/Eads St	local	✓	center
B	Pentagon City Metro/S. Hayes St	local	✓	center
C	Army Navy Drive/S. Joyce St	local	✓	curbside
D (optional)	S. Joyce St	local	✓	center
Local	Southgate Rd	local	-	-
E	Heritage Center (Oak St)	Super Stop (planned)	✓	curbside
Local	S. Orme St	local	-	-
Local	Rolfe St	local	-	-
F	Scott St	Super Stop (planned)	✓	curbside
G	S. Courthouse Rd	Super Stop (planned)	✓	curbside
H	S. Barton St	Super Stop (WB under construction; EB planned)	✓	curbside
I	Walter Reed Drive	Super Stop (EB under construction; WB planned)	✓	curbside
Local	S. Highland St	local	-	-
J	S. Glebe Rd	Super Stop (planned)	✓	curbside
K	S. Monroe St	Super Stop (planned)	-	curbside
Local	S. Oakland St	local (alternate TSM Super Stop location in place of Monroe St)	✓	-
Local	S. Quincy St	local	-	-

Table 1: Stop Locations by Alternative (Cont.)

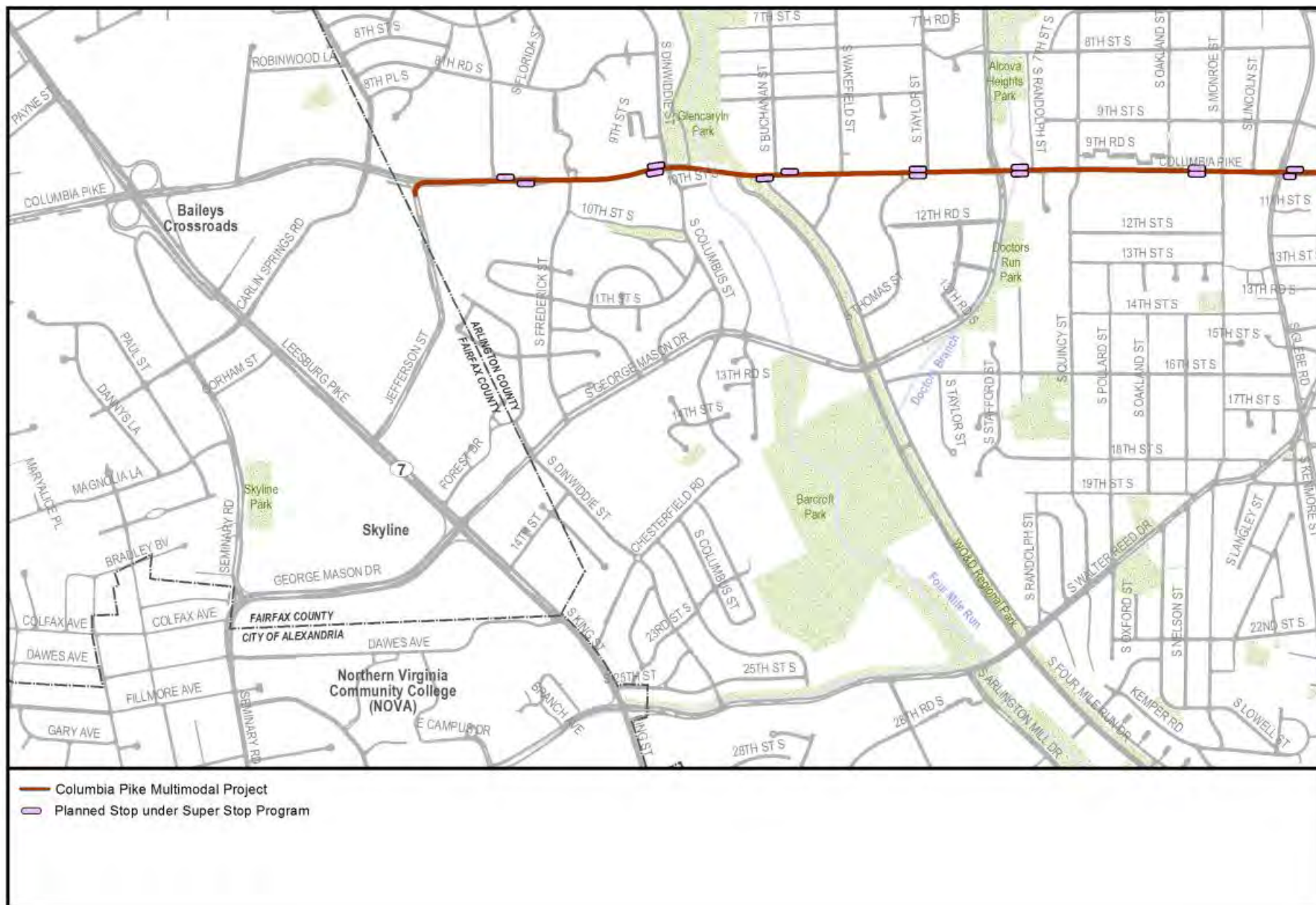
Stop ID	General Location	No Build (local bus stop/Super Stop)	TSM 1 and 2	Streetcar Build (incl. platform location)
L	George Mason Drive	Super Stop (planned)	✓	curbside
M	S. Thomas St	Super Stop (planned)	✓	curbside
N	S. Buchanan/Four Mile Run Dr	Super Stop (planned)	✓	curbside
O	Arlington Mill (Dinwiddie St)	Super Stop (under construction)	✓	curbside
Local	Frederick St	local	-	-
P	Greenbrier St	Super Stop (planned)	✓	curbside
Local	Wildwood Park/Windsor Towers	local	-	-
Local	Jefferson St near Columbia Pike (westbound only)	local	-	-
Q	Jefferson St - Goodwin House	local	✓	center
R	Jefferson St near Leesburg Pike	local	✓ (TSM 1 curbside) (TSM 2 inside transit center)	center (adjacent to transit center; terminus stop of Jefferson St Design Option)
S	Skyline Complex	local (various locations)	✓ (TSM 1 at George Mason Dr) (TSM 2 in Skyline Complex)	Central Plaza Design Option: curbside (in Skyline Complex) Route 7 Design Option: center (along Skyline Route 7 frontage)

No Build

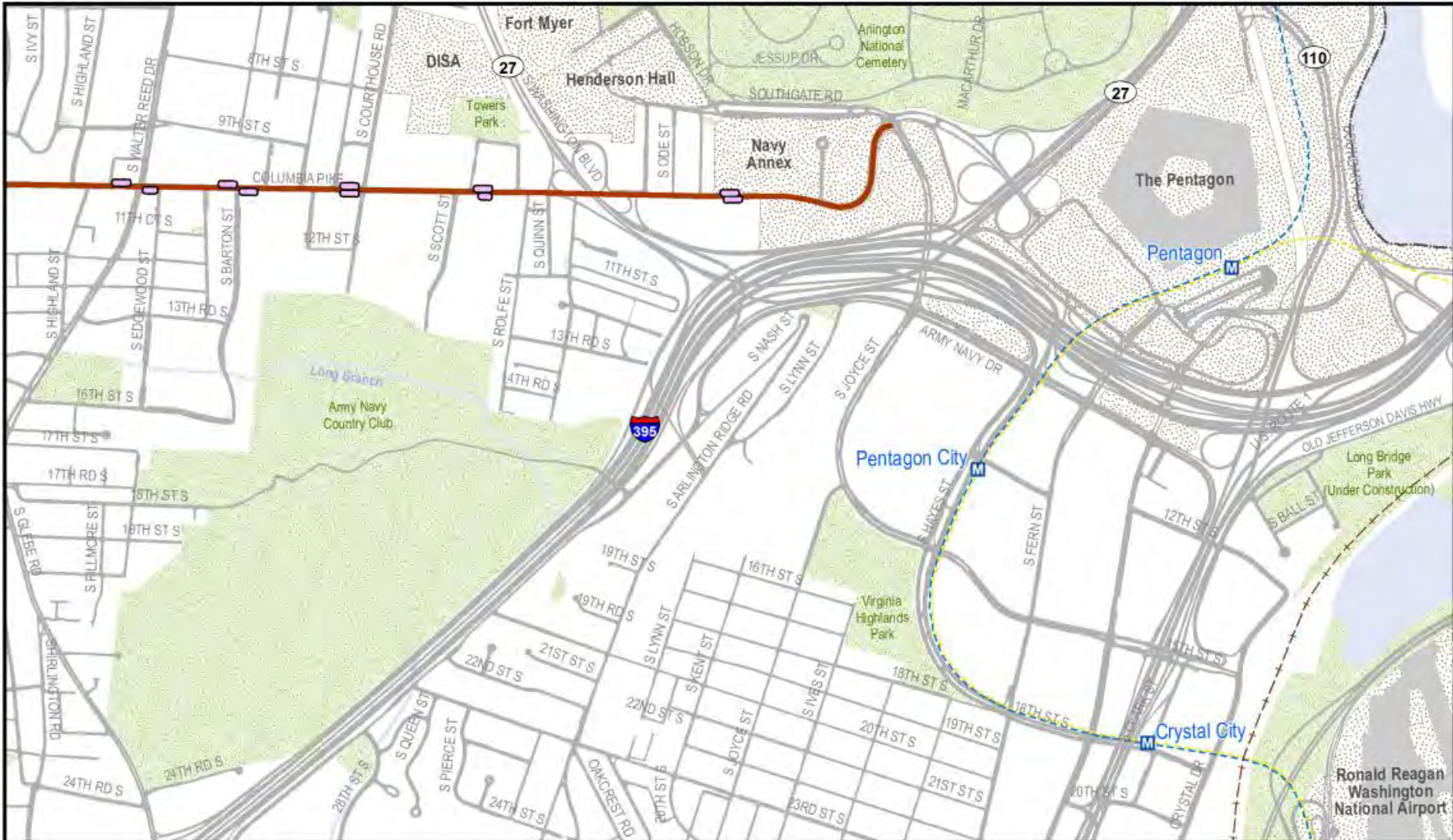
Table 2: Current and Planned Projects along Columbia Pike (Projects Included in No Build)

Project	Lead Agency/ Organization	Project Definition/ Key Elements	Timeline
Baileys Crossroads Streetscape Improvements	Fairfax County	Landscaping, lighting, and sidewalk construction (based on roadway cross-section developed in the Baileys Crossroads Planning Study)	Fairfax Co. CIP, ongoing
Columbia Pike Super Stops Construction	Arlington County	Construction of larger bus shelters with electronic and printed information, wireless access, enhanced lighting and new security features, improved landscape treatments and sidewalks, accommodation of off-board fare payment vending machines. Includes implementation along Columbia Pike from South Jefferson Street to South Joyce Street.	Arlington Co. CIP 2009 Based on last PMT meeting this is 80% funded (\$70M) in the current TIP, will be fully funded for Fiscal Year 2013; phased construction
Columbia Pike Multimodal Street Improvements	Arlington County	Street improvements to Columbia Pike with the goal of providing a standardized street cross-section (two travel lanes in each direction with a center median or left-turn lane), on-street parking, bicycle accommodations, wider sidewalks, enhanced pedestrian crossings, landscaped median areas, and street trees where practicable. Implementation along Columbia Pike between South Jefferson Street and South Joyce Street.	Based on last PMT meeting this is 80% funded (\$70M) in the current TIP, will be fully funded for Fiscal Year 2013; phased construction
Bus Information Technology Deployment and Signal Prioritization	Arlington County	Installing Bus Transit Signal Priority technology at Columbia Pike signalized intersections	TIP, ongoing. Technology already installed at 10 intersections. Installation at other intersections not yet scheduled
Transit ITS	Arlington County	Establish a communications network for transit vehicles, traffic signals and control centers to improve performance and reliability as well as safety	TIP, Arlington Co. CIP, ongoing
Columbia Pike Streetscape Improvements	Arlington County	Landscaping, lighting, and sidewalk construction [LIST FOCUS AREAS]	TIP, ongoing
Shared Use Path Construction	Arlington County	Construct shared use path to connect Washington Boulevard path to Air Force Memorial and existing path to Pentagon along Columbia Pike	CLRP 2013
Washington Boulevard (VA 27) Bridge and Interchange	Virginia Department of Transportation (VDOT)	Interchange modifications and bridge replacement	TIP 2012
Pentagon City Multimodal Improvements	Arlington County	Construction of improvements to multimodal circulation in the Pentagon City area. Includes bus circulation, pedestrian and passenger amenities, traffic turning movements, signal improvements, ITS technologies, and streetscape improvements	TIP, Arlington Co. CIP 2013

No Build Multimodal Project Alignment and Super Stop Locations (1/2)



No Build Multimodal Project Alignment and Super Stop Locations (2/2)



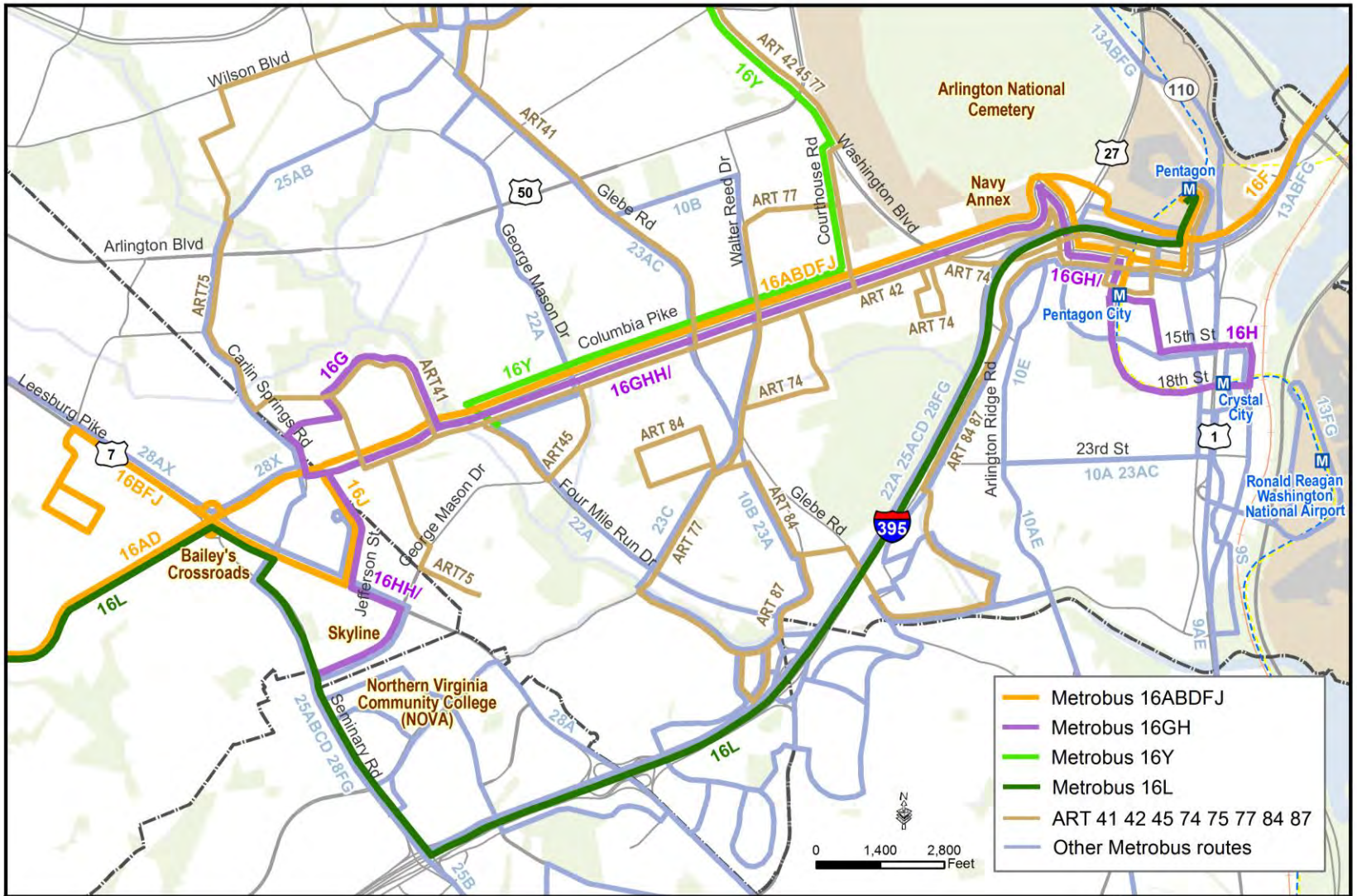
Source: Arlington County, Fairfax County, and City of Alexandria data

**No Build Multimodal
Project Alignment and
Super Stop Locations**



September 16, 2011

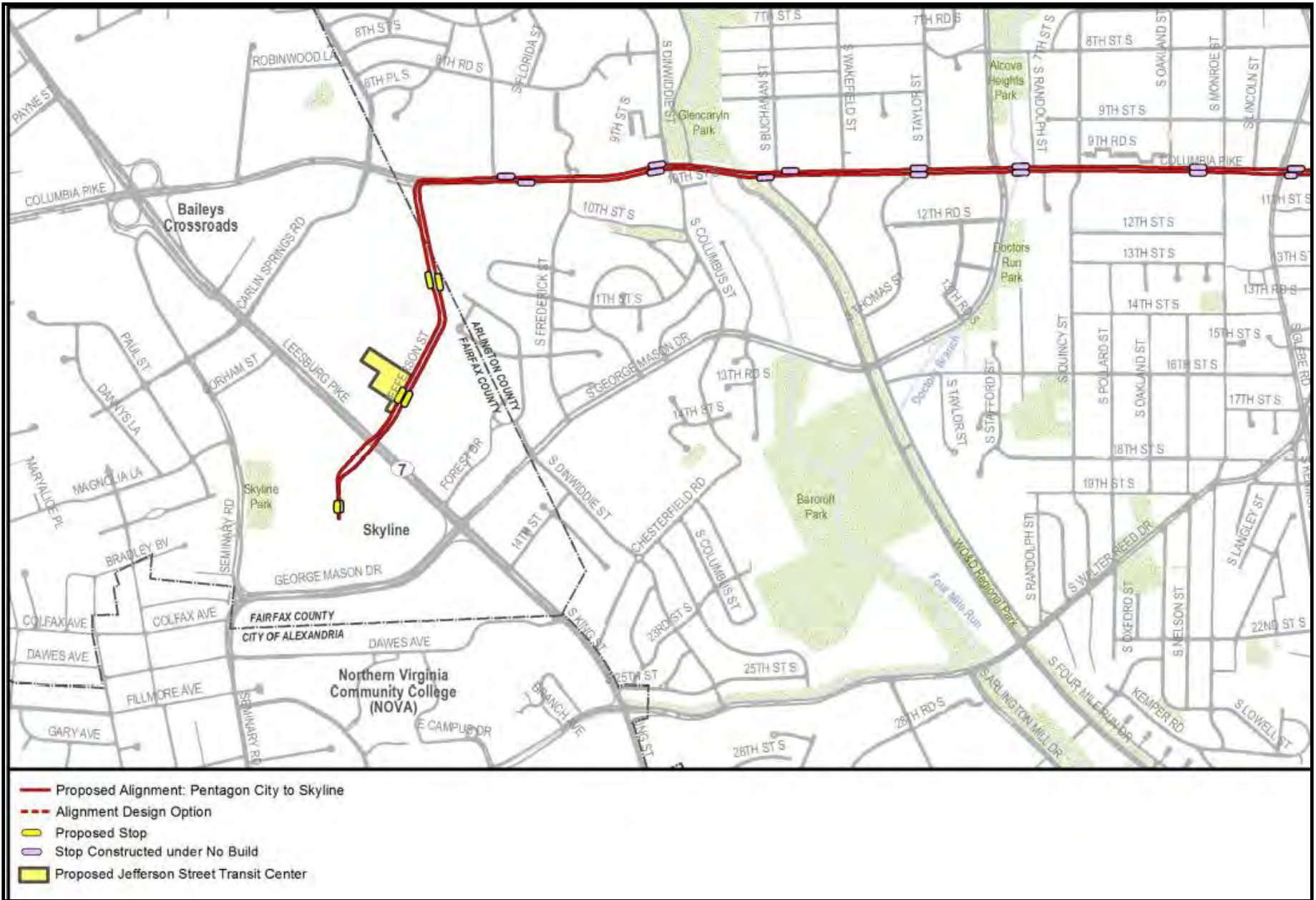
No-Build Route Network



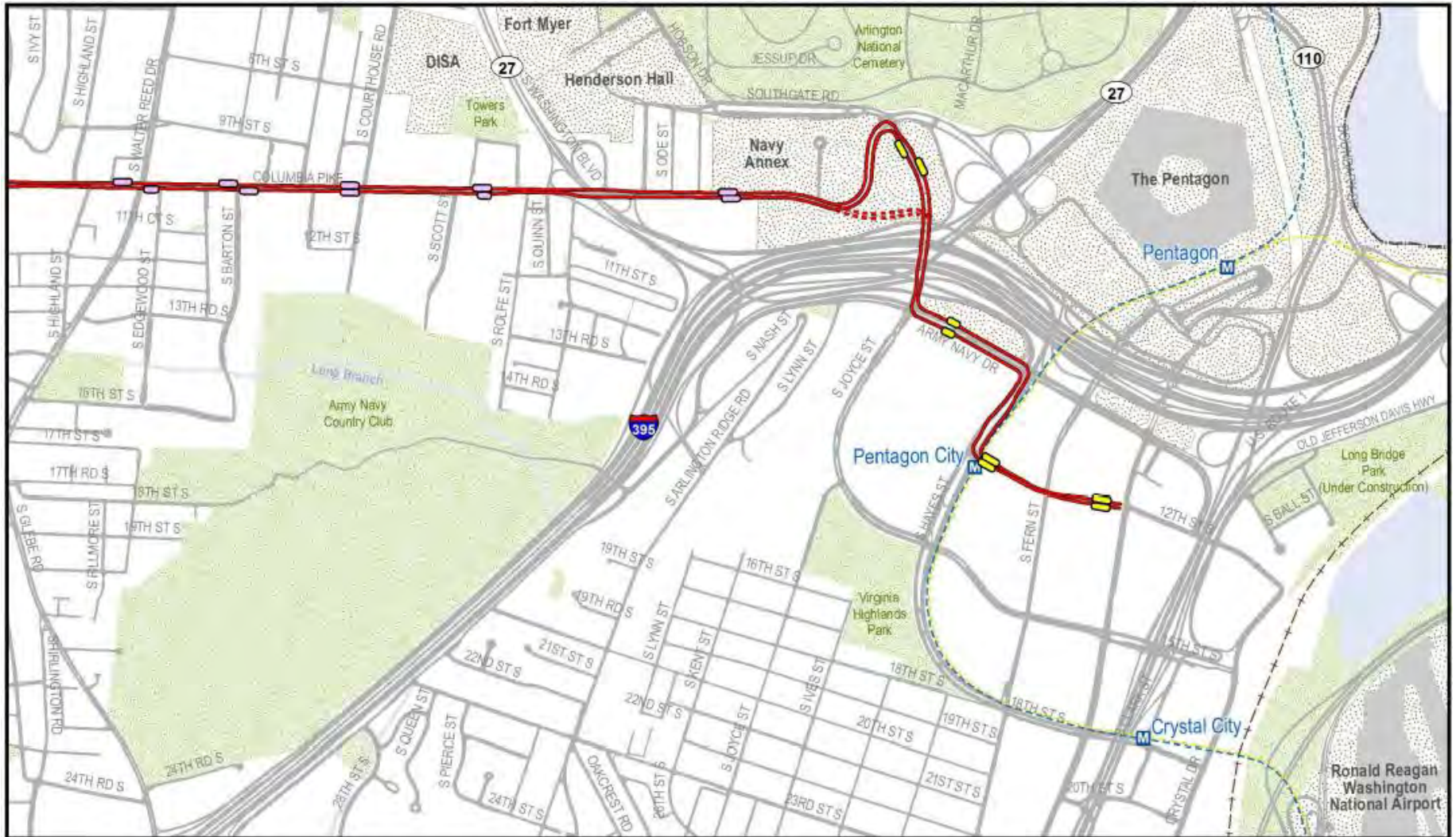
No-Build Alternative



TSM Alternative Alignment and Super Stop Locations (1/2)



TSM Alternative Alignment and Super Stop Locations (2/2)



0 600 1,200 Feet

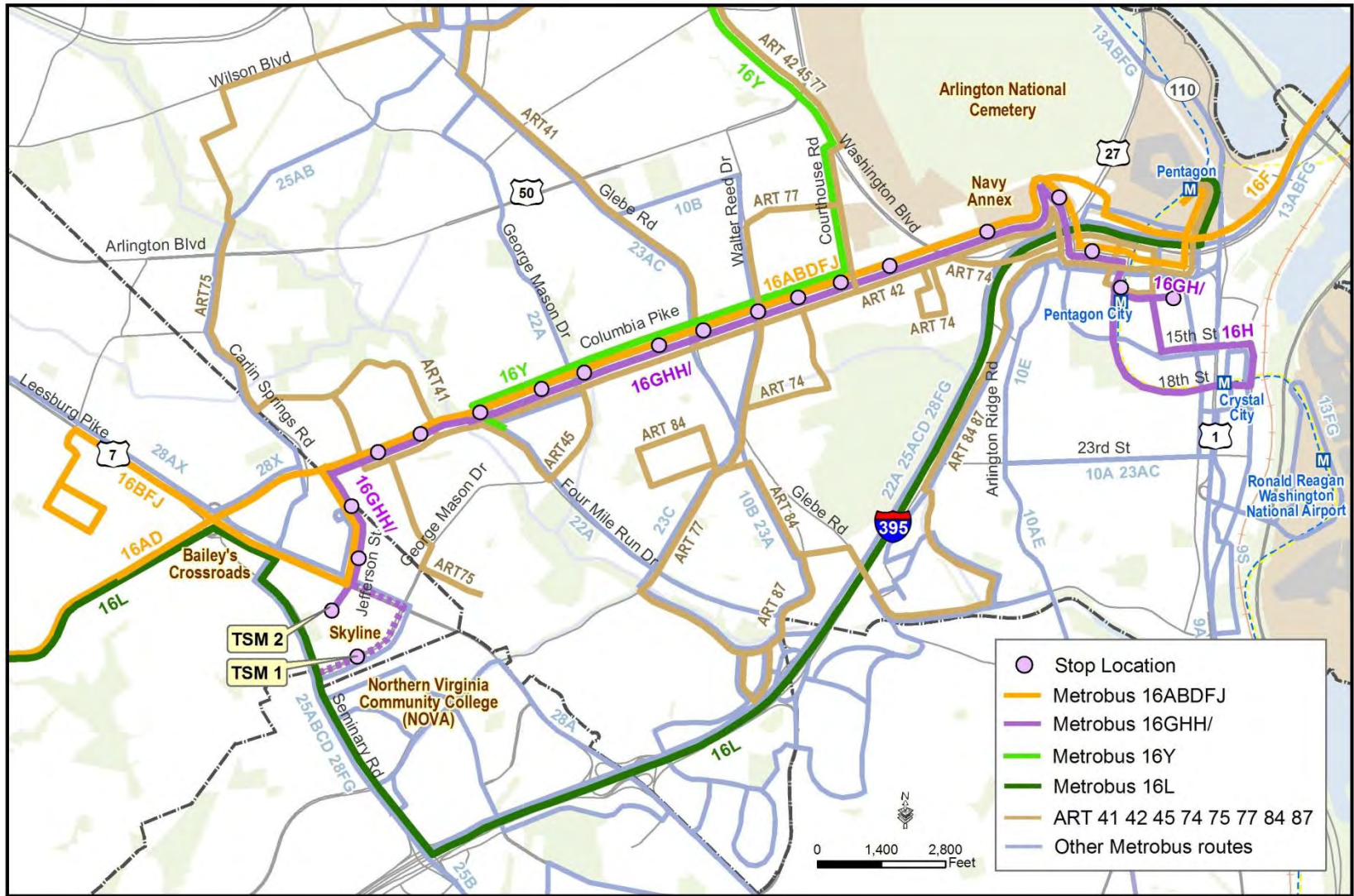


Proposed Transportation Systems Management 2 (TSM2) Alternative Alignment and Stop Locations



Source: Arlington County, Fairfax County, and City of Alexandria data

TSM Route Network and Stops



TSM Alternatives



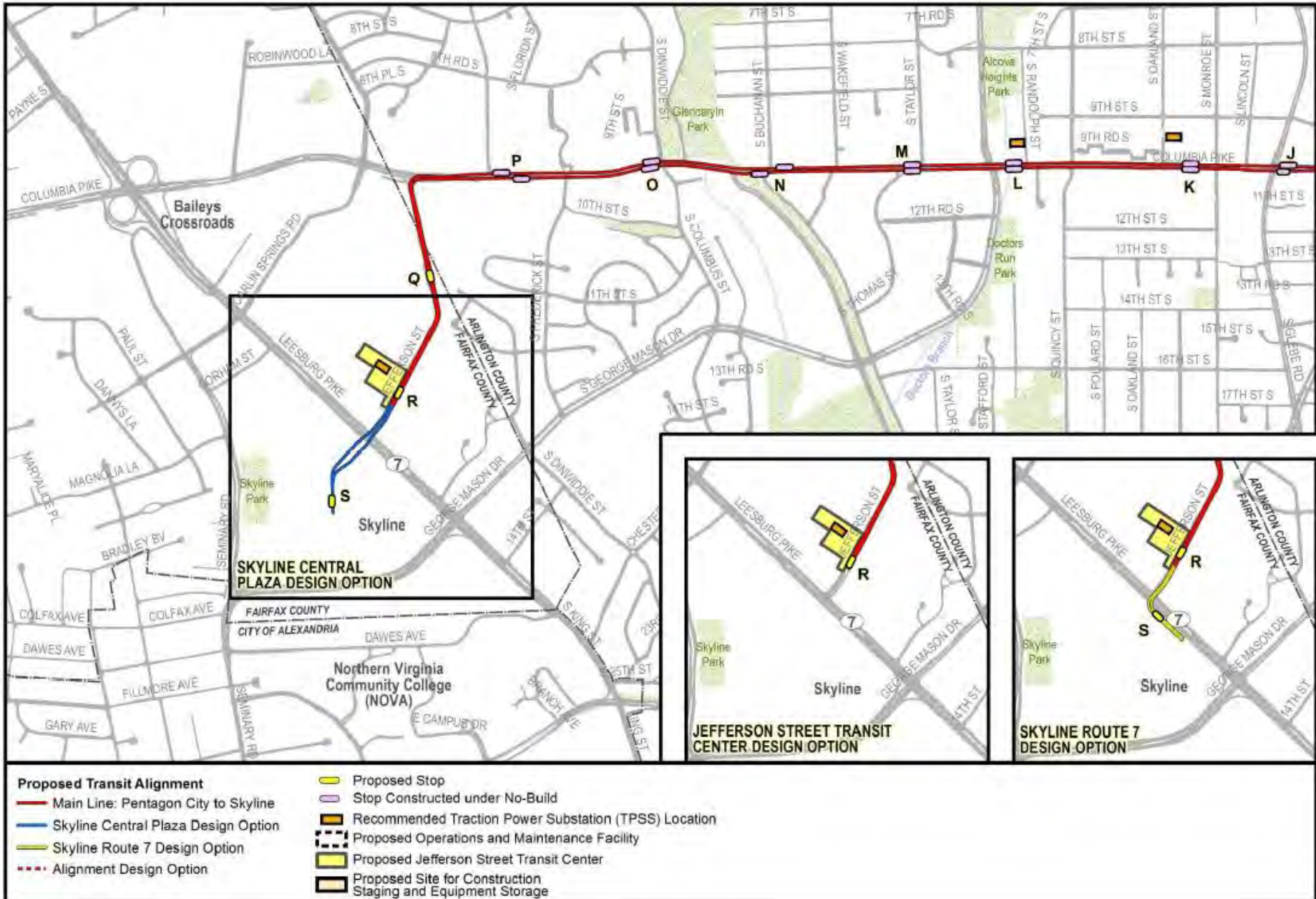
Streetcar Build Alternative

Table 3: Build Alternative Physical Characteristics

Alignment	
Eastern Terminus	Pentagon City (12 th Street/South Eads Street)
Design Options for Western Terminus	<ul style="list-style-type: none"> Design Option A: Skyline - Jefferson Street Transit Center Design Option B: Skyline Central Plaza Design Option C: Skyline at Route 7
Design Options for Roadway Alignment	<ul style="list-style-type: none"> Current Columbia Pike alignment between the Navy Annex and Joyce Street Realigned Columbia Pike between the Navy Annex and Joyce Street
Areas of Median Running	<p>Median running segments:</p> <ul style="list-style-type: none"> Jefferson Street between Leesburg Pike and Columbia Pike (0.51 miles); exclusive guideway along a portion of Jefferson Street for Design Option A (approximately 400 feet) South Joyce Street between Army Navy Drive and Columbia Pike (0.30 miles) South Hayes Street/Army Navy Drive to 12th Street/South Eads Street (0.42 miles); exclusive guideway along a portion of 12th Street between Fern and Eads (approximately 300 feet)
Alignment Length	<ul style="list-style-type: none"> 4.71 miles (Design Option A) 4.94 miles (Design Option B) 4.91 miles (Design Option C)
Non-Revenue Track Length	160 feet

Facilities	
Stops	<p>18 stop locations in each direction (Design Option A); 19 stop locations in each direction (Design Options B and C)</p> <p>See Table 3 for list of stops</p>
Operations and Maintenance Facility	Pentagon City (along Eads Street between 12 th Street and Army Navy Drive)
Fleet Size (# of revenue cars)	12 cars
Facility Storage Capacity	12 cars
Transit Center	Jefferson Street (Baileys Crossroads): at-grade off-street bus bays and approximately 200 park-and-ride spaces.
Traction Power Substations	<ul style="list-style-type: none"> Army Navy Drive/I-395 ramp Washington Boulevard interchange Oakland Street Buchanan Street Jefferson Street Transit Center

Streetcar Build Alternative Alignment and Super Stop Locations (1/2)



Initial Streetcar Build Alternative Design Options Considered



Proposed Streetcar Alignment	○ Proposed Stop
— Pentagon City to Skyline	○ Alternate Stop
— Skyline to NOVA (via George Mason Dr)	■ Operations and Maintenance Facility
— Skyline to NOVA (via Rt. 7)	■ Jefferson Street Transit Center

0 2,000 4,000 Feet

Initial Streetcar Build Alternative Design Options

Douglas W. Domenech
Secretary of Natural Resources



David A. Johnson
Director

COMMONWEALTH of VIRGINIA
DEPARTMENT OF CONSERVATION AND RECREATION

Division of Natural Heritage
217 Governor Street
Richmond, Virginia 23219-2010
(804) 786-7951

October 31, 2011

Susan Anderson
WMATA
600 5th Street, NW, Room 6F-16
Washington, DC 20001

Re: Columbia Pike Transit Initiative Streetcar Build Alternative

Dear Ms. Anderson:

The Department of Conservation and Recreation's Division of Natural Heritage (DCR) has searched its Biotics Data System for occurrences of natural heritage resources from the area outlined on the submitted map. Natural heritage resources are defined as the habitat of rare, threatened, or endangered plant and animal species, unique or exemplary natural communities, and significant geologic formations.

According to the information currently in our files, the Barcroft Seeps Conservation Site is downstream of the project area. Conservation sites are tools for representing key areas of the landscape that warrant further review for possible conservation action because of the natural heritage resources and habitat they support. Conservation sites are polygons built around one or more rare plant, animal, or natural community designed to include the element and, where possible, its associated habitat, and buffer or other adjacent land thought necessary for the element's conservation. Conservation sites are given a biodiversity significance ranking based on the rarity, quality, and number of element occurrences they contain; on a scale of 1-5, 1 being most significant. Barcroft Seeps Conservation Site has been given a biodiversity significance ranking of B2, which represents a site of very high significance. The natural heritage resources of concern at this site are:

Coastal Plain / Piedmont Seepage Bog	G1/SNR/NL/NL
Coastal Plain / Piedmont Acidic Seepage Swamp	G3?/SNR/NL/NL

Coastal Plain / Piedmont Seepage Bog is a saturated woodland known from a limited area at and just east of the Fall-line in Maryland and northern Virginia. It occurs on saturated toeslopes at bases of highly weathered, highly acidic, fluvial-estuarine terrace gravel deposits of Cretaceous or Tertiary age. Irregular microtopography with abundant groundwater seepage forming braided channels, *Sphagnum*-covered hummocks, and mucky depressions are characteristic. Historic accounts of this vegetation describe these areas as "bogs" with *Magnolia virginiana* and various shrubs fringing and forming clumps within a more open center dominated by herbaceous plants. Today, remaining examples exist mostly as open woodlands with very dense shrubs (averaging 80% cover) and very small, scattered herbaceous patches. *Nyssa*

sylvatica is the most dominant species, followed by *Magnolia virginiana*, *Acer rubrum*, *Liriodendron tulipifera*, and *Ilex opaca* var. *opaca*. Historically, fire may have been an important factor maintaining herbaceous patches and limiting the growth of shrubs and trees, but dynamics of this community type are not fully understood. This community has always had a limited distribution in the Mid-Atlantic fall-line zone and has probably always been rare. Today, less than 10 sites remain in very small patches degraded by fire exclusion, woody succession, and various anthropogenic impacts.

Coastal Plain/Piedmont Acidic Seepage Swamp, commonly known as Southern Red Maple-Blackgum Swamp Forest, is an acidic swamp forest ranging from southeastern New York and New Jersey to southeastern Virginia on the coastal plain and the extreme portion of Virginia's piedmont. This swamp is a nutrient-poor wetland forest that occurs in groundwater saturated stream bottoms and in poorly drained depressions. The soil consists of deep muck over mineral soil with pools of standing water on the surface. The acidic waters originate from groundwater seepage, with little to no overland seasonal flooding. This uncommon wetland habitat is vulnerable to alteration or destruction by beavers and various anthropogenic activities including hydrologic modifications.

To minimize adverse impacts to the aquatic ecosystem as a result of the proposed activities, DCR recommends the implementation of and strict adherence to applicable state and local erosion and sediment control/storm water management laws and regulations.

Under a Memorandum of Agreement established between the Virginia Department of Agriculture and Consumer Services (VDACS) and the Virginia Department of Conservation and Recreation (DCR), DCR represents VDACS in comments regarding potential impacts on state-listed threatened and endangered plant and insect species. The current activity will not affect any documented state-listed plants or insects.

There are no State Natural Area Preserves under DCR's jurisdiction in the project vicinity.

New and updated information is continually added to Biotics. Please contact DCR for an update on this natural heritage information if a significant amount of time passes before it is utilized.

The Virginia Department of Game and Inland Fisheries maintains a database of wildlife locations, including threatened and endangered species, trout streams, and anadromous fish waters that may contain information not documented in this letter. Their database may be accessed from <http://vafwis.org/fwis/> or contact Shirl Dressler at (804) 367-6913.

Should you have any questions or concerns, feel free to contact me at 804-692-0984. Thank you for the opportunity to comment on this project.

Sincerely,



Alli Baird, LA, ASLA
Coastal Zone Locality Liaison

Literature Cited

Fleming, G.P., K.D. Patterson, K. Taverna, and P.P. Coulling. 2011. The natural communities of Virginia: classification of ecological community groups. Second approximation. Version 2.4. Virginia Department of Conservation and Recreation, Division of Natural Heritage, Richmond, VA.

NatureServe. 2011. NatureServe Explorer: An online encyclopedia of life [web application]. Version 7.1. NatureServe, Arlington, Virginia. Available <http://www.natureserve.org/explorer>. (Accessed:October 31, 2011)



COMMONWEALTH of VIRGINIA

DEPARTMENT OF ENVIRONMENTAL QUALITY
Street address: 629 East Main Street, Richmond, Virginia 23219
Mailing address: P.O. Box 1105, Richmond, Virginia 23218
TDD (804) 698-4021
www.deq.virginia.gov

Douglas W. Domenech
Secretary of Natural Resources

David K. Paylor
Director

(804) 698-4000
1-800-592-5482

October 24, 2011

Washington Metropolitan Area Transit Authority
Attn: Ms. Susan T. Anderson, AICP
NEPA Task Leader, Pike Transit Initiative
600 5th Street, N.W.
Room 6F-16
Washington, D.C. 20001

RE: Columbia Pike Transit Initiative, Alternatives Analysis/Environmental Assessment

Dear Ms. Anderson:

This letter responds to your October 6, 2011 letter and enclosures (received October 11) providing an updated project description, in which additional superstructure and sub-structure improvements to the project bridge over Four Mile Run are proposed. You also included a copy of our October 1, 2009 letter to Ms. McElhenny-Smith concerning the scope of the National Environmental Policy Act (NEPA) document for the Columbia Pike Transit Initiative as it was then contemplated. This letter will not repeat our earlier letter except as necessary.

PROJECT DESCRIPTION

In addition to the project elements addressed in our exchange of correspondence in October 2009, the Washington Metropolitan Area Transit Authority (WMATA) also contemplates the following improvements to the bridge over Four Mile Run:

- Reconstruction of the outer lanes, sidewalk, and parapet of the bridge deck;
- Installation of new structural lightweight concrete box beams to support a new streetcar concrete deck with embedded rails; and
- Addition of concrete in-fill walls along existing bridge piers and the expansion of the footings of existing bridge piers to provide sufficient support for deck improvements.

ENVIRONMENTAL REVIEW

The roles of the Virginia Department of Environmental Quality (DEQ) are the same as those described in our October 1, 2009 letter, i.e., coordination of Virginia's review of the NEPA document and, in the event of federal funding, of the federal consistency certification required for the project by the Coastal Zone Management Act.

FEDERAL CONSISTENCY UNDER THE COASTAL ZONE MANAGEMENT ACT

Our federal consistency guidance remains the same as in our October 1, 2009 letter.

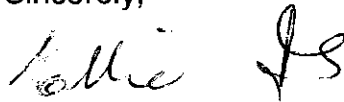
PROJECT SCOPING

Our scoping guidance remains the same as in the October 1, 2009 letter, except that agency re-organizations have resulted in office name changes since then. These changes are:

- In DEQ, the Waste Division is now called the Division of Land Protection and Restoration.
- In the Department of Conservation and Recreation, the Division of Soil and Water Conservation is now part of the Division of Stormwater Management. So is the Division of Chesapeake Bay Local Assistance.

I hope the foregoing information is helpful to you.

Sincerely,



Ellie L. Irons, Program Manager
Environmental Impact Review

cc: David Hartshorn, DEQ-NRO
Kotur S. Narasimhan, DEQ-Air
Richard J. Criqui, DEQ-DLPR
Roberta Rhur, DCR



October 6, 2011

Virginia Department of Environmental Quality
Office of Environmental Impact Review
Attn: Ellie Irons
P.O. Box 1105
Richmond, VA 23218

Re: Columbia Pike Transit Initiative, Alternatives Analysis/Environmental Assessment

Dear Sir or Madam:

On behalf of Arlington County and Fairfax County, the Washington Metropolitan Area Transit Authority (WMATA) is conducting an Alternatives Analysis/Environmental Assessment (AA/EA) of proposed transit improvements along the Columbia Pike corridor. In 2009, the project team contacted the Virginia Department of Environmental Quality (VDEQ) to initiate agency contact and obtain input regarding any potential resources within the project study area under your agency's jurisdiction. In response, VDEQ provided guidance to WMATA in 2009 regarding the need to provide a Federal Consistency Certification (FCC) and a recommendation to include it as part of the NEPA documentation. Please find VDEQ's previous correspondence with WMATA enclosed.

Since this previous correspondence, some time has passed and the definitions of the project's No Build Alternative and Streetcar Build Alternative have been revised based on more detailed analysis. As WMATA's consultant, AECOM is making further contact to provide these updates and to seek additional input from VDEQ regarding potential impacts to natural resources in the project study area.

The project study area, as shown on the enclosed USGS quadrangles (**Figure 1**) extends approximately five miles, mostly along the highly urban corridor of Columbia Pike, between Skyline and Pentagon City. The current Streetcar Build Alternative (comprised of the alignment itself, stop locations, traction power substations, an operations and maintenance facility, and transit center) are identified in the enclosed **Figure 2**. As previous correspondence with VDEQ noted, the proposed transit improvement would stay mostly within existing transportation rights-of-way that are currently paved or covered by concrete.

Previous correspondence also noted that the project would cross Four Mile Run via the existing Columbia Pike roadway bridge, but would not propose modifications to this structure, other than enhanced structural deck support. Since then, engineering analysis undertaken by the project team has identified the need for the following superstructure and substructure improvements to the bridge in order to accommodate the Streetcar Build Alternative:

1. Reconstruction of the outer lanes, sidewalk and parapet of the Four Mile Run bridge deck. (See **Figure 3** and **Figure 4** for sections of the existing and proposed bridge deck.)

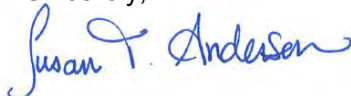
2. Installation of new structural light-weight concrete box beams to support a new streetcar concrete deck with embedded rails. (**Figure 4**)
3. Addition of concrete in-fill walls along existing bridge piers and the expansion of the footings of existing bridge piers to provide sufficient support for deck improvements. (See **Figure 5** and **Figure 6** for sections of the existing and proposed pier elevation.)

Aside from temporary impacts associated with construction staging, the improvements made to the Four Mile Run bridge deck (Items 1 and 2) would not likely impact natural resources associated with Four Mile Run. It is anticipated that construction time to complete superstructure improvements would be between approximately six to eight months.

However, the addition of concrete in-fill walls between the existing bridge piers and the expansion of the footings on the existing bridge piers (Item 3) may have the potential to have temporary as well as permanent impacts on the Four Mile Run stream. In order to construct the in-fill walls and expand the footings of the existing piers, a cofferdam would be built and used to dewater the construction area. Construction time to complete substructure improvements would be approximately four to six months. See the enclosed **Figure 7** for images showing existing conditions at Four Mile Run under the Four Mile Run bridge.

The project team respectfully requests input, based on the updates presented above, regarding the Columbia Pike Transit Initiative Project's potential impacts to natural resources in the project study area. A similar letter is being submitted to the Virginia Department of Conservation and Recreation and Virginia Department of Game and Inland Fisheries. Should you have any questions or comments, please contact me at 703-340-3023 or Susan.Anderson@aecom.com for more information.

Sincerely,



Susan T. Anderson, AICP
Pike Transit Initiative, NEPA Task Leader

Enclosures: Letter dated 10/01/09 from E. Irons, VDEQ to R. McElhenny-Smith, WMATA re: Proposed Columbia Pike Transit Initiative, request for scoping comments for the preparation of a National Environmental Policy Act document

Figure 1: Proposed Alignment and Study Area (USGS Quadrangles)

Figure 2: Proposed Alignment and Study Area

Figure 3: Existing Typical Section - Four Mile Run Bridge

Figure 4: Proposed Typical Section - Four Mile Run Bridge

Figure 5: Existing Pier Elevation

Figure 6: Proposed Pier Elevation

Figure 7: Four Mile Run Images

cc: WMATA – J. Dittmeier

Figure 1: Proposed Alignment and Study Area (USGS Quadrangles)

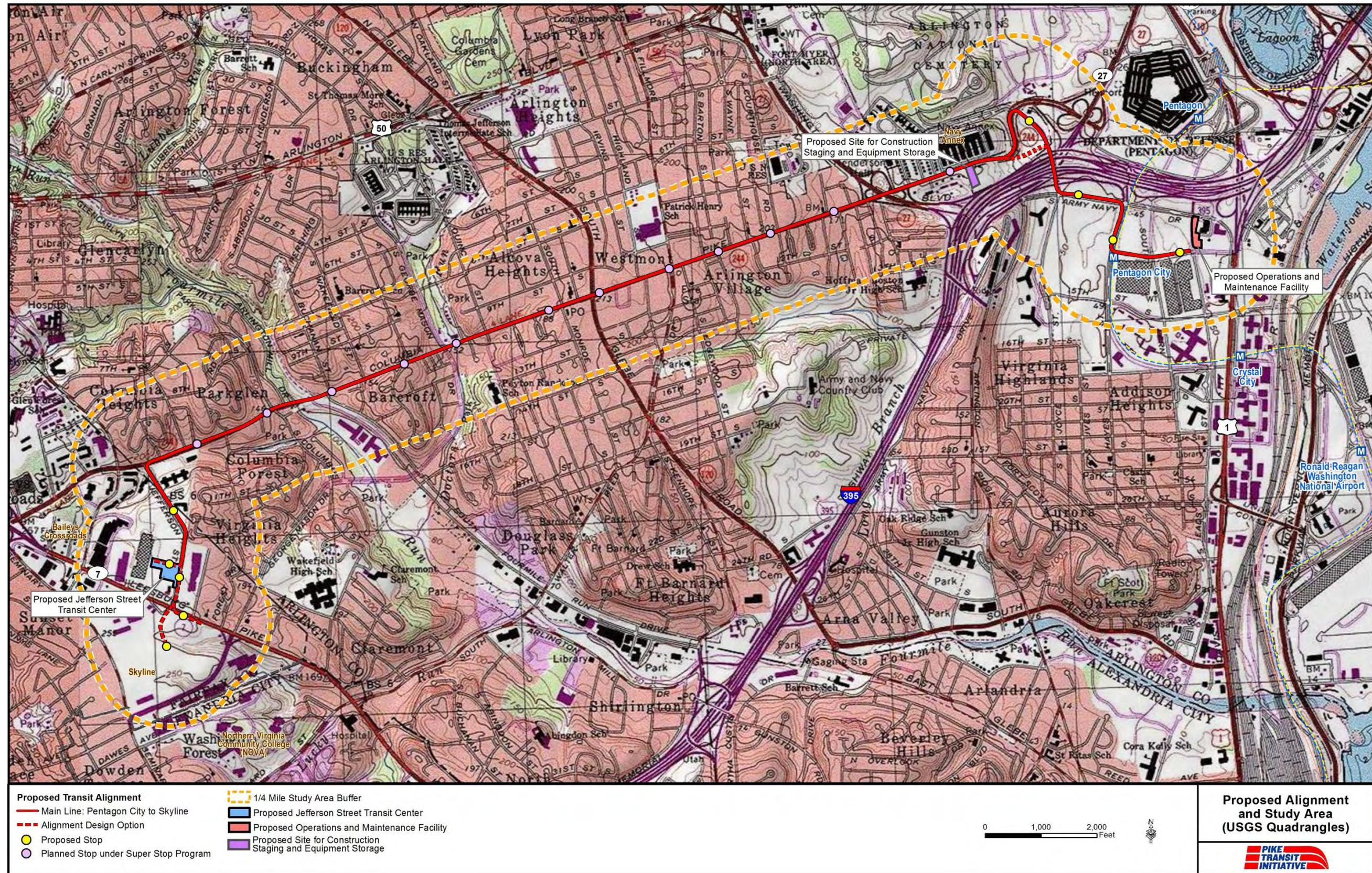


Figure 2: Proposed Alignment and Study Area

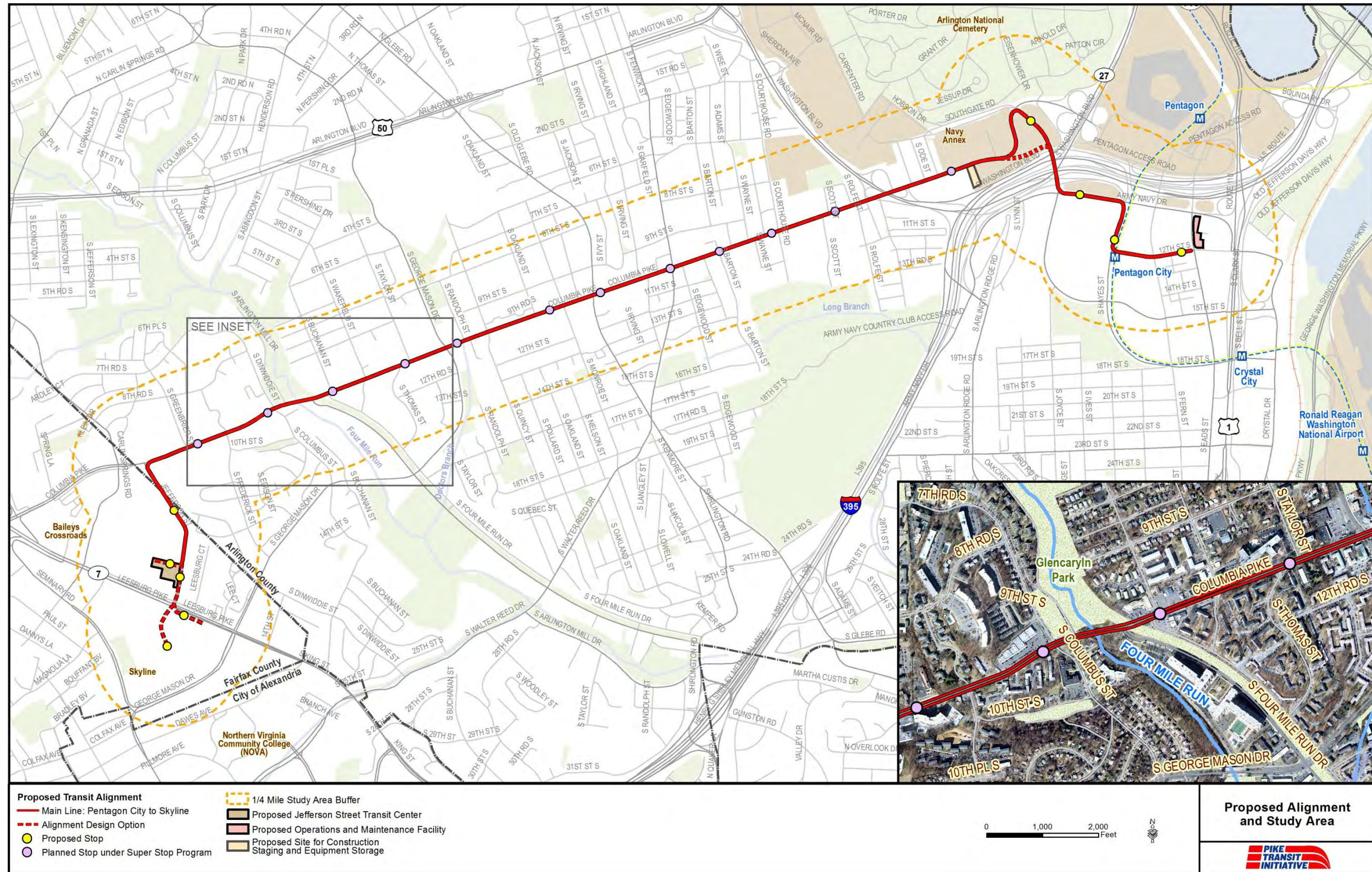
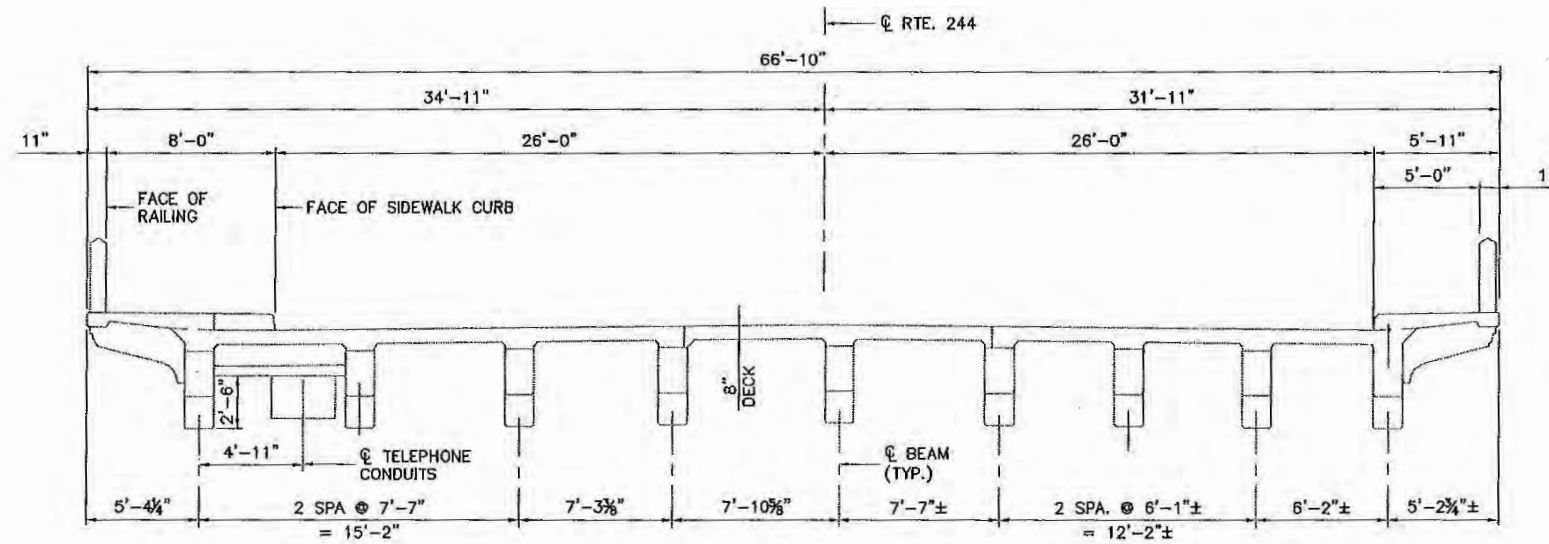


Figure 3: Existing Typical Section - Four Mile Run Bridge

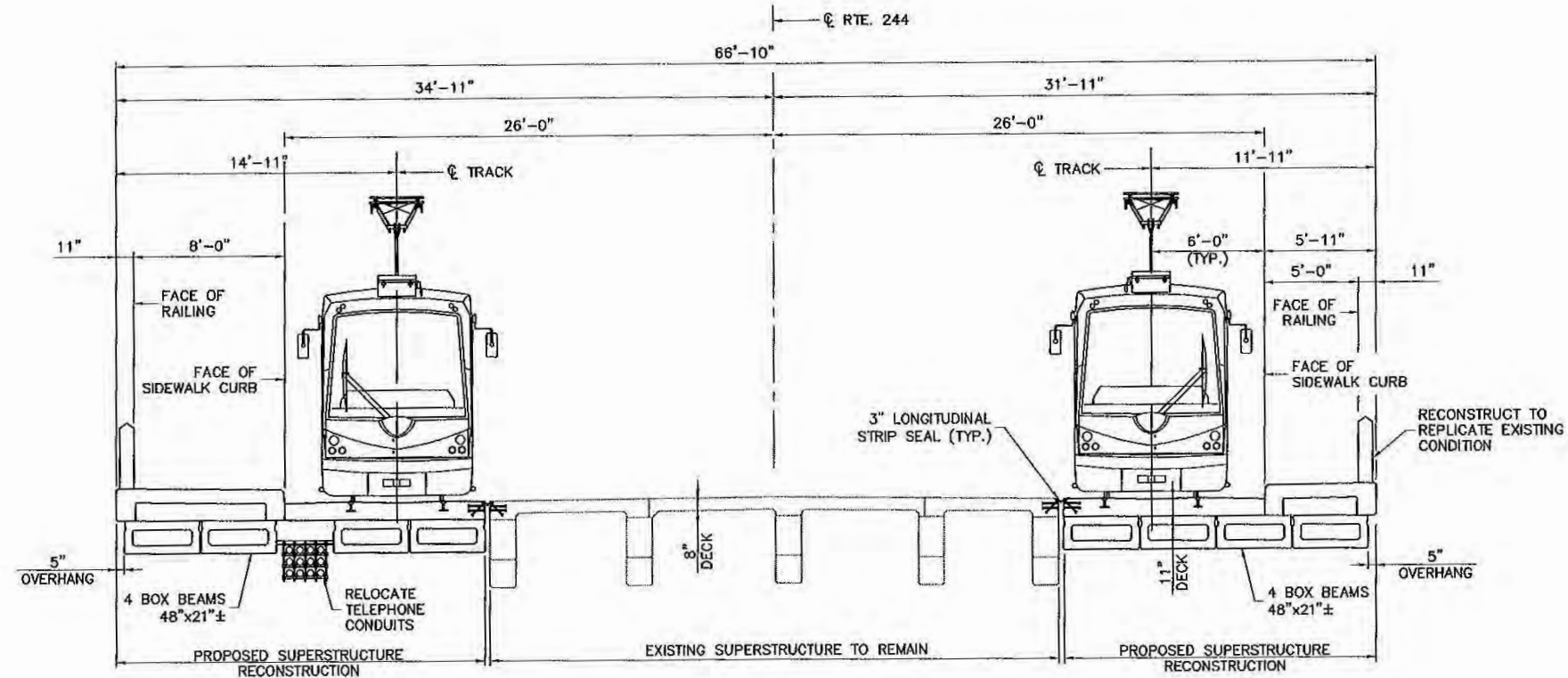


EXISTING TYPICAL SECTION
FOUR MILE RUN BRIDGE

Jul. 15 2011 11:39 am C:\60020294\Structure\Presentation\Exhibits\Columbia Pike_ExistSec.dwg By: jccintoc

	<p>Columbia Pike Streetcar Preliminary Streetcar Alignment East Option Existing Bridge Section Attachment 1</p>	Scale:
		Date: 08-XX-2010
		Dwg. No.
		Sheet No: 1 of 5

Figure 4: Proposed Typical Section - Four Mile Run Bridge

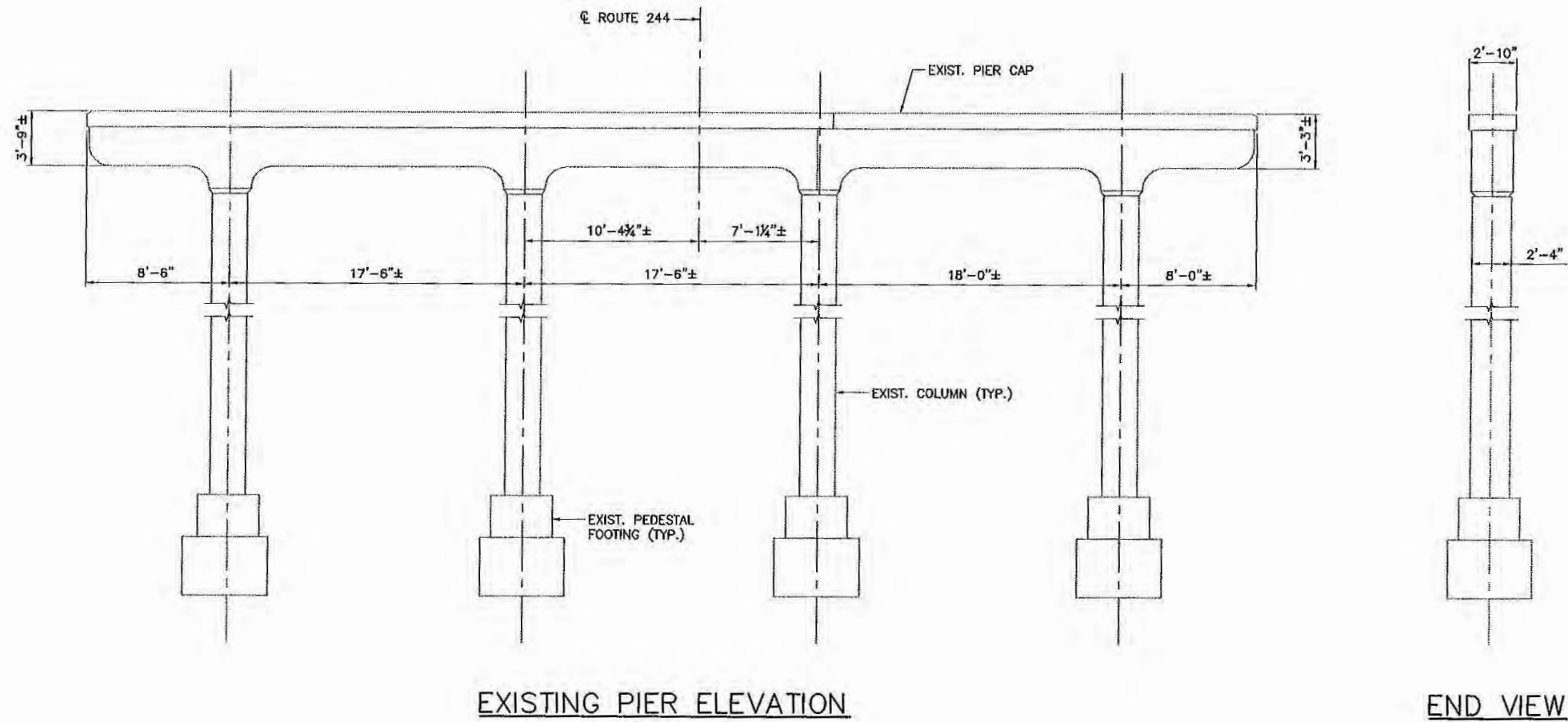


PROPOSED TYPICAL SECTION
FOUR MILE RUN BRIDGE

Jul. 15 2011 11:40 am G:\60020294\Structure\Presentation\Exhibits\Columbia Pike\proposed.dwg By: jacinoc

	<p>Columbia Pike Streetcar Preliminary Streetcar Alignment East Option Proposed Bridge Section Attachment 3</p>	<p>Scale:</p>
		<p>Date: 08-XX-2010</p>
		<p>Dwg. No.</p>
		<p>Sheet No: 3 of 5</p>

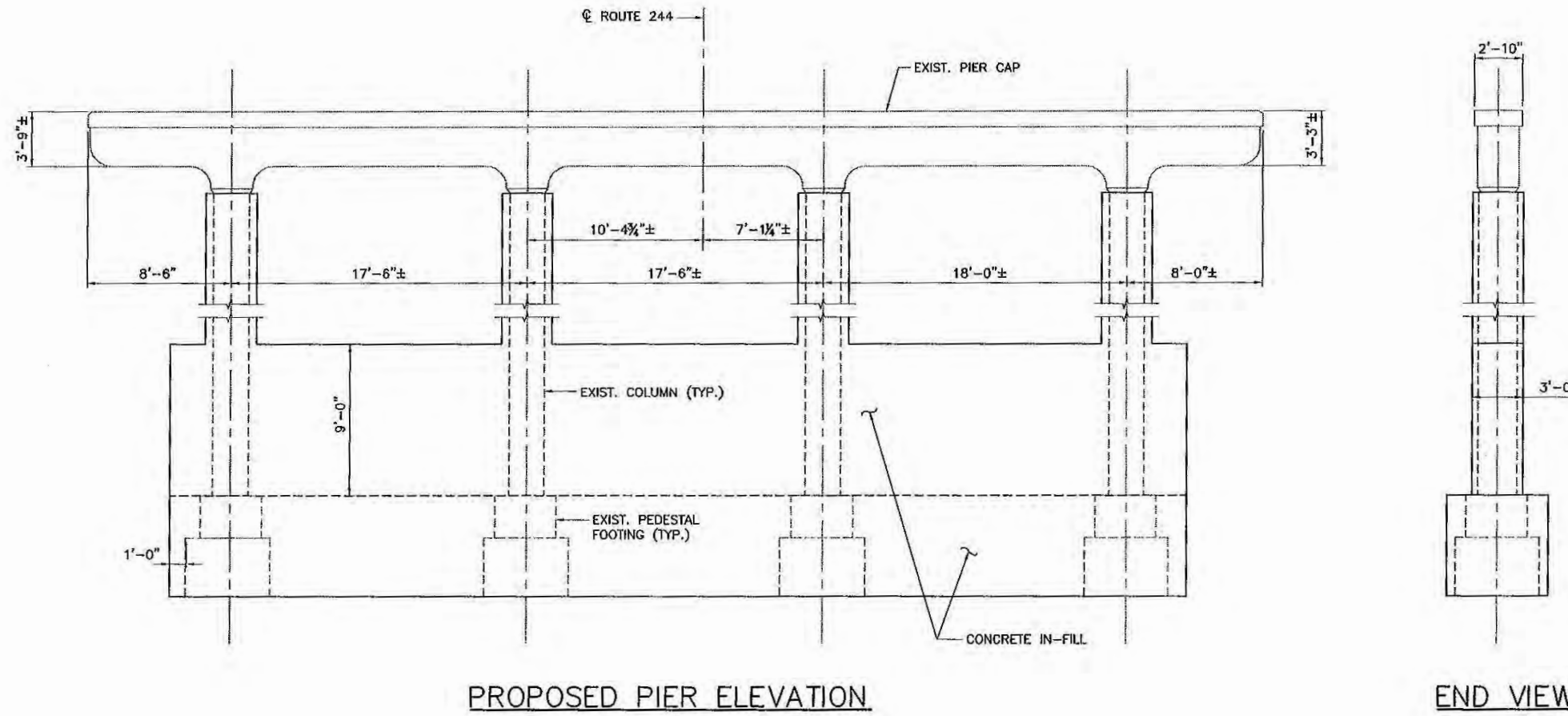
Figure 5: Existing Pier Elevation



Jul 15 2011 11:40 am G:\60020294\Structure\Presentation\Exhibits\Columbia Pike_Exist-elev.dwg By: jacinlee

	<p>Columbia Pike Streetcar Preliminary Streetcar Alignment East Option</p>	Scale:
	<p>Typical Existing Pier Elevation Attachment 4</p>	Date: 08-XX-2010
		Dwg. No.
		Sheet No: 4 of 5

Figure 6: Proposed Pier Elevation



Jul 15 2011 11:40 am G:\60020294\Structure\Presentation\Exhibits\Columbia Pike_prop-elev.dwg By: jpcitoc

	<p>Columbia Pike Streetcar Preliminary Streetcar Alignment East Option</p> <p>Proposed Typical Pier with Concrete In-Fills Attachment 5</p>	Scale: Date: 08-XX-2010 Dwg. No.: Sheet No: 5 of 5
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Figure 7: Four Mile Run Images



Four Mile Run bridge (looking northwest from Four Mile Run Trail)



Under Four Mile Run bridge



Four Mile Run



Four Mile Run bridge (looking southwest from Four Mile Run Trail)



October 6, 2011

Virginia Department of Game and Inland Fisheries
Environmental Services Section
Attn: Gladys Cason
P.O. Box 11104
4010 West Broad Street
Richmond, VA 23230

Re: Columbia Pike Transit Initiative, Alternatives Analysis/Environmental Assessment

Dear Sir or Madam:

On behalf of Arlington County and Fairfax County, the Washington Metropolitan Area Transit Authority (WMATA) is conducting an Alternatives Analysis/Environmental Assessment (AA/EA) of proposed transit improvements along the Columbia Pike corridor. In 2009 and 2010, the project team contacted the Virginia Department of Game and Inland Fisheries (VDGIF) to initiate agency contact and obtain input regarding any potential resources within the project study area under your agency's jurisdiction. In response, VDGIF relayed to WMATA in 2009 that Four Mile Run and the Potomac River, in the project vicinity, are designated Anadromous Fish Use Areas. VDGIF recommended that NEPA documentation include a discussion about proposed impacts upon these waters and how adverse effects upon the anadromous fish resources will be avoided, minimized or mitigated. Also, VDGIF provided protective recommendations as to how project work should be done in these areas. Please find VDGIF's previous correspondence with WMATA enclosed.

Since this previous correspondence, some time has passed and the definitions of the project's No Build Alternative and Streetcar Build Alternative have been revised based on more detailed analysis. As WMATA's consultant, AECOM is making further contact to provide these updates and to seek additional input from VDGIF regarding potential impacts to natural resources in the project study area.

The project study area, as shown on the enclosed USGS quadrangles (**Figure 1**) extends approximately five miles, mostly along the highly urban corridor of Columbia Pike, between Skyline and Pentagon City. The current Streetcar Build Alternative (comprised of the alignment itself, stop locations, traction power substations, an operations and maintenance facility, and transit center) are identified in the enclosed **Figure 2**. As previous correspondence with VDGIF noted, the proposed transit improvement would stay mostly within existing transportation rights-of-way that are currently paved or covered by concrete.

Previous correspondence also noted that the project would cross Four Mile Run via the existing Columbia Pike roadway bridge, but would not propose modifications to this structure, other than enhanced structural deck support. Since then, engineering analysis undertaken by the project team has identified the need for the following superstructure and substructure improvements to the bridge in order to accommodate the Streetcar Build Alternative:

1. Reconstruction of the outer lanes, sidewalk and parapet of the Four Mile Run bridge deck. (See **Figure 3** and **Figure 4** for sections of the existing and proposed bridge deck.)
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Aside from temporary impacts associated with construction staging, the improvements made to the Four Mile Run bridge deck (Items 1 and 2) would not likely impact natural resources associated with Four Mile Run. It is anticipated that construction time to complete superstructure improvements would be approximately six to eight months.

However, the addition of concrete in-fill walls between the existing bridge piers and the expansion of the footings on the existing bridge piers (Item 3) may have the potential to have temporary as well as permanent impacts on the Four Mile Run stream. In order to construct the in-fill walls and expand the footings of the existing piers, a cofferdam would be built and used to dewater the construction area. Construction time to complete substructure improvements would be approximately four to six months. See the enclosed **Figure 7** for images showing existing conditions at Four Mile Run under the Four Mile Run bridge.

The project team respectfully requests input, based on the updates presented above, regarding the Columbia Pike Transit Initiative Project's potential impacts to natural resources in the project study area. A similar letter is being submitted to the Virginia Department of Conservation and Recreation and Virginia Department of Environmental Quality. Should you have any questions or comments, please contact me at 703-340-3023 or Susan.Anderson@aecom.com for more information.

Sincerely,



Susan T. Anderson, AICP
Pike Transit Initiative, NEPA Task Leader

Enclosures: E-mail dated 11/03/09 from A. Ewing, VDGIF to J. Dittmeier, WMATA re: ESSLog# 30118_Columbia Pike Transit Initiative_NEPA Documentation

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Figure 1: Proposed Alignment and Study Area (USGS Quadrangles)

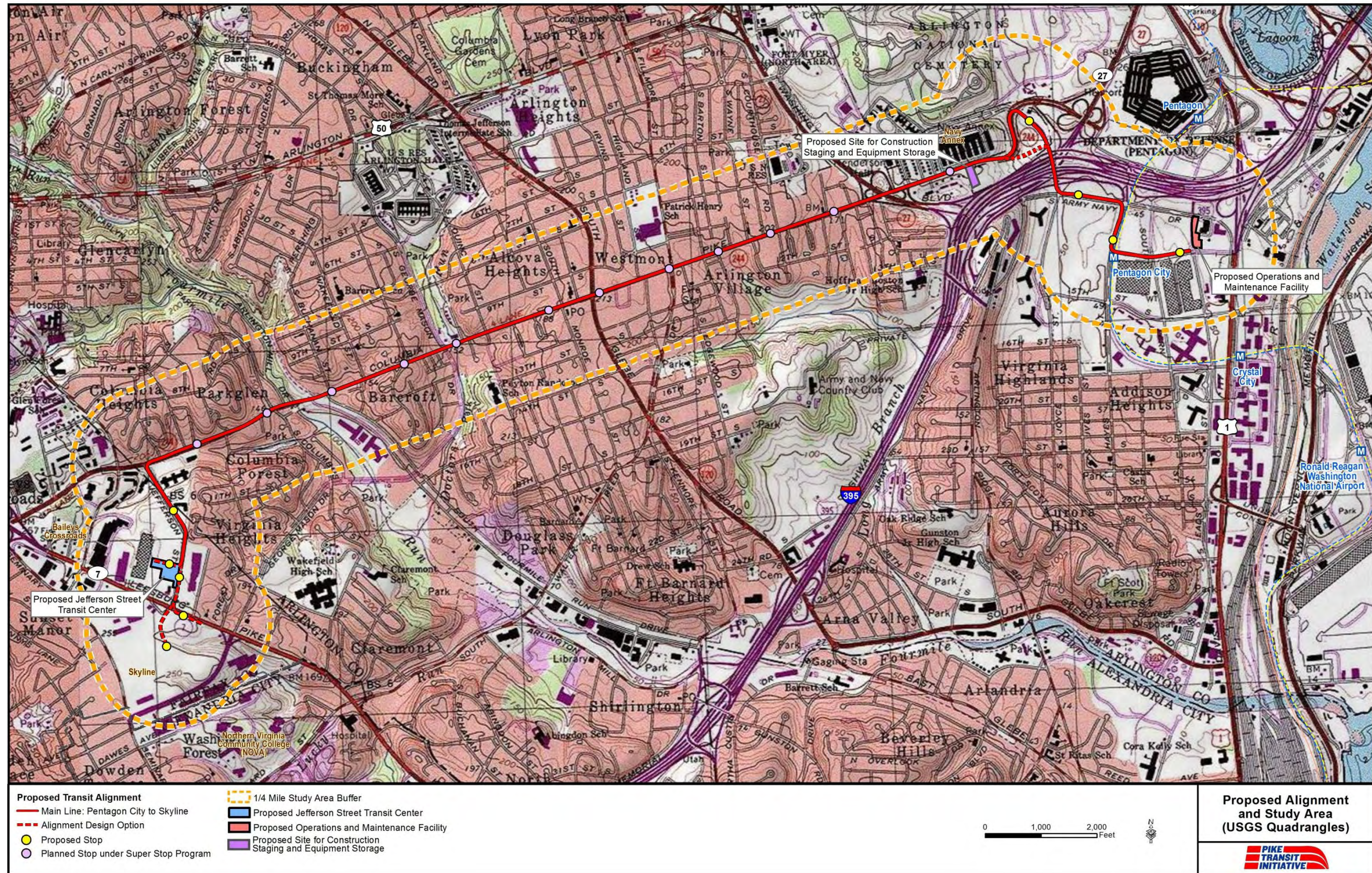
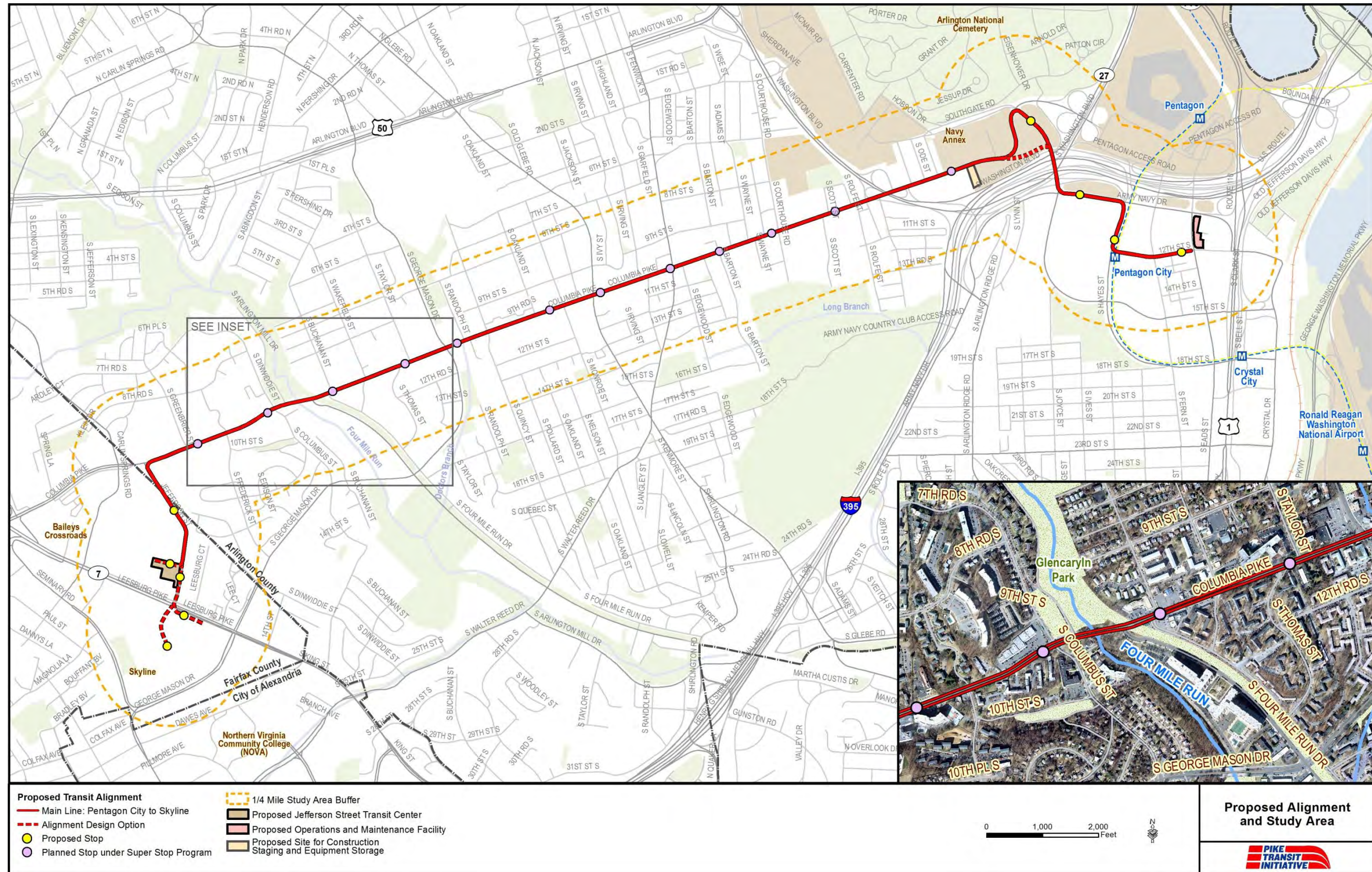
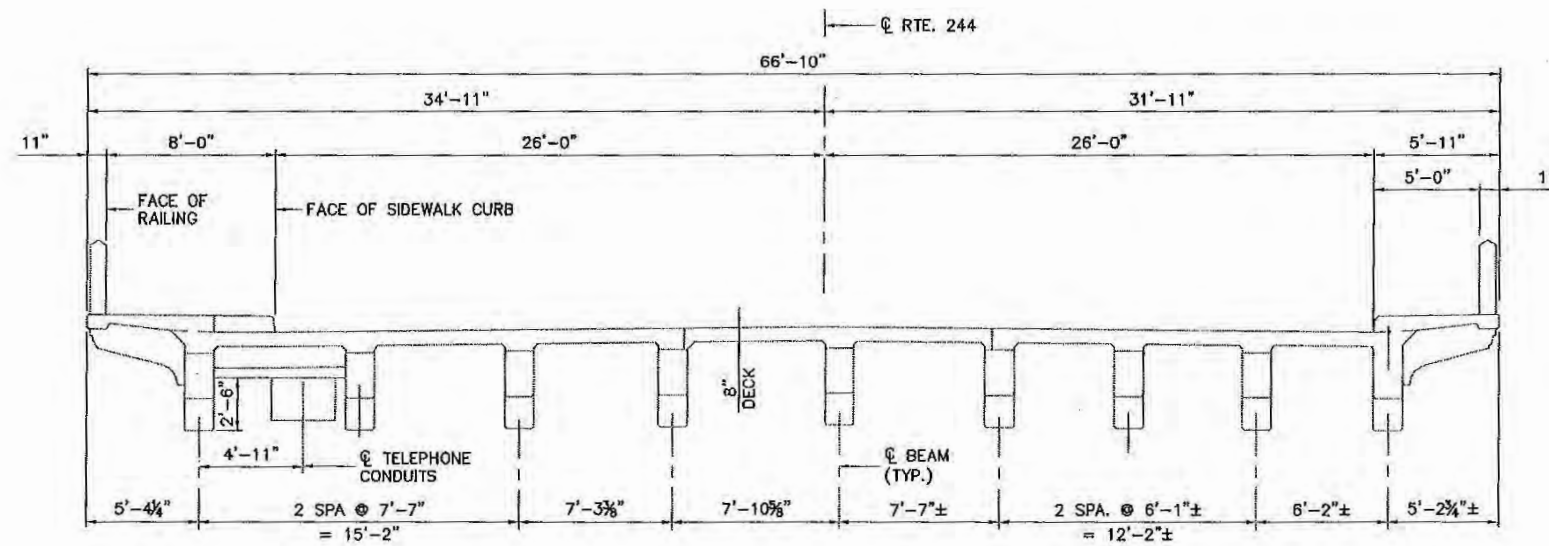


Figure 2: Proposed Alignment and Study Area



Proposed Alignment and Study Area

Figure 3: Existing Typical Section - Four Mile Run Bridge

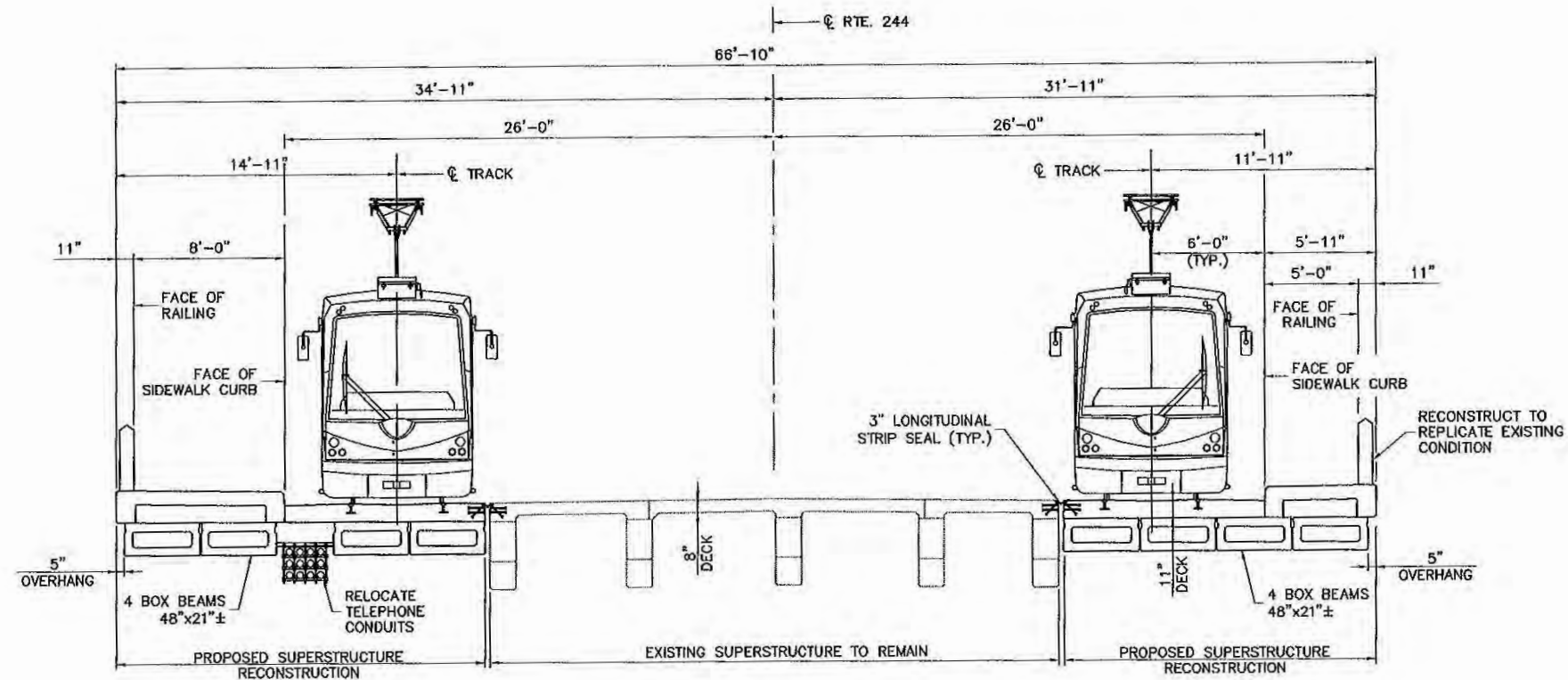


EXISTING TYPICAL SECTION
FOUR MILE RUN BRIDGE

Jul. 15 2011 11:39 am C:\60020294\Structure\Presentation\Exhibits\Columbia Pike_ExistSec.dwg By: jccintoc

	<p>Columbia Pike Streetcar Preliminary Streetcar Alignment East Option Existing Bridge Section Attachment 1</p>	Scale:
		Date: 08-XX-2010
		Dwg. No.
		Sheet No: 1 of 5

Figure 4: Proposed Typical Section - Four Mile Run Bridge

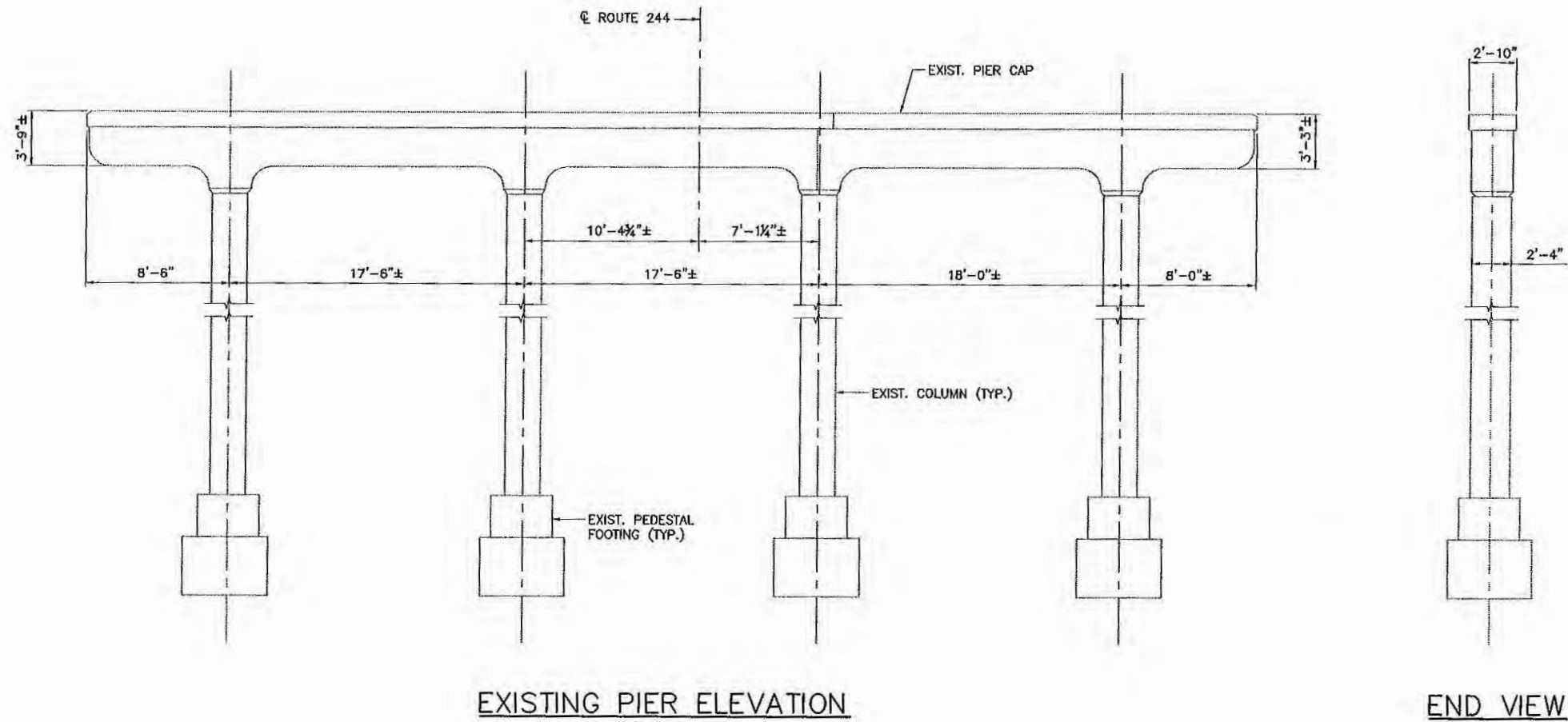


PROPOSED TYPICAL SECTION
FOUR MILE RUN BRIDGE

Jul. 15 2011 11:40 am G:\60020294\Structure\Presentation\Exhibits\columbia pike_propsec.dwg By: jacinoc

	<p>Columbia Pike Streetcar Preliminary Streetcar Alignment East Option Proposed Bridge Section Attachment 3</p>	<p>Scale:</p>
		<p>Date: 08-XX-2010</p>
		<p>Dwg. No.</p>
		<p>Sheet No: 3 of 5</p>

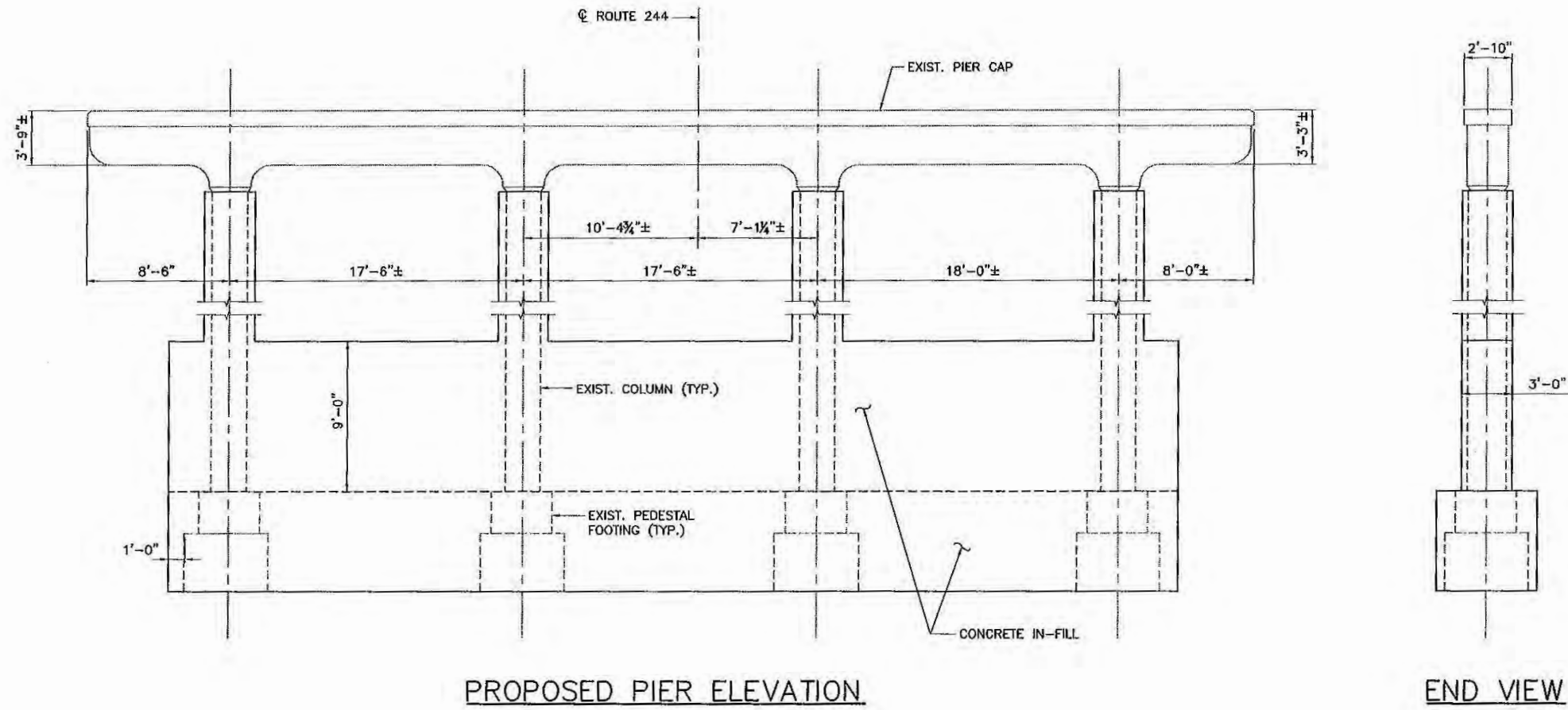
Figure 5: Existing Pier Elevation



Jul 15 2011 11:40 am G:\60020294\Structure\Presentation\Exhibits\Columbia Pike_Exist-elev.dwg By: jacinthe

	<p>Columbia Pike Streetcar Preliminary Streetcar Alignment East Option Typical Existing Pier Elevation Attachment 4</p>	Scale:
		Date: 08-XX-2010
		Dwg. No.
		Sheet No: 4 of 5

Figure 6: Proposed Pier Elevation



Jul 15 2011 11:40 am G:\60020294\Structure\Presentation\Exhibits\Columbia Pike_prop-elev.dwg By: jpcitoc

	<p>Columbia Pike Streetcar Preliminary Streetcar Alignment East Option</p> <p>Proposed Typical Pier with Concrete In-Fills Attachment 5</p>	Scale: Date: 08-XX-2010 Dwg. No.: Sheet No: 5 of 5
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Figure 7: Four Mile Run Images



Four Mile Run bridge (looking northwest from Four Mile Run Trail)



Under Four Mile Run bridge



Four Mile Run



Four Mile Run bridge (looking southwest from Four Mile Run Trail)



October 6, 2011

Virginia Department of Conservation and Recreation
Virginia Natural Heritage Program
Attn: Rene Hypes
217 Governor Street, Third Floor
Richmond, VA 23219

Re: Columbia Pike Transit Initiative, Alternatives Analysis/Environmental Assessment

Dear Sir or Madam:

On behalf of Arlington County and Fairfax County, the Washington Metropolitan Area Transit Authority (WMATA) is conducting an Alternatives Analysis/Environmental Assessment (AA/EA) of proposed transit improvements along the Columbia Pike corridor. In 2009 and 2010, the project team contacted the Virginia Department of Conservation and Recreation (VDCR) to initiate agency contact and obtain input regarding any potential resources within the project study area under your agency's jurisdiction. In response, VDCR relayed to WMATA in 2009 that no adverse impacts to natural heritage resources in the project area were anticipated due to the scope of the activity and the distance to these resources. Also, VDCR concluded that the proposed activity would not affect any documented state-listed plants or insects, and that no State Natural Area Preserves, under VDCR's jurisdiction, exist in the project vicinity. Please find VDCR's previous correspondence with WMATA enclosed.

Since this previous correspondence, some time has passed and the definitions of the project's No Build Alternative and Streetcar Build Alternative have been revised based on more detailed analysis. As WMATA's consultant, AECOM is making further contact to provide these updates and to seek additional input from VDCR regarding potential impacts to natural resources in the project study area.

The project study area, as shown on the enclosed USGS quadrangles (**Figure 1**) extends approximately five miles, mostly along the highly urban corridor of Columbia Pike, between Skyline and Pentagon City. The current Streetcar Build Alternative (comprised of the alignment itself, stop locations, traction power substations, an operations and maintenance facility, and transit center) are identified in the enclosed **Figure 2**. As previous correspondence with VDCR noted, the proposed transit improvement would stay mostly within existing transportation rights-of-way that are currently paved or covered by concrete.

Previous correspondence also noted that the project would cross Four Mile Run via the existing Columbia Pike roadway bridge, but would not propose modifications to this structure, other than enhanced structural deck support. Since then, engineering analysis undertaken by the project team has identified the need for the following superstructure and substructure improvements to the bridge in order to accommodate the Streetcar Build Alternative:

1. Reconstruction of the outer lanes, sidewalk and parapet of the Four Mile Run bridge deck. (See **Figure 3** and **Figure 4** for sections of the existing and proposed bridge deck.)
2. Installation of new structural light-weight concrete box beams to support a new streetcar concrete deck with embedded rails. (**Figure 4**)
3. Addition of concrete in-fill walls along existing bridge piers and the expansion of the footings of existing bridge piers to provide sufficient support for deck improvements. (See **Figure 5** and **Figure 6** for sections of the existing and proposed pier elevation.)

Aside from temporary impacts associated with construction staging, the improvements made to the Four Mile Run bridge deck (Items 1 and 2) would not likely impact natural resources associated with Four Mile Run. It is anticipated that construction time to complete superstructure improvements would be approximately six to eight months.

However, the addition of concrete in-fill walls between the existing bridge piers and the expansion of the footings on the existing bridge piers (Item 3) may have the potential to have temporary as well as permanent impacts on the Four Mile Run stream. In order to construct the in-fill walls and expand the footings of the existing piers, a cofferdam would be built and used to dewater the construction area. Construction time to complete substructure improvements would be approximately four to six months. See the enclosed **Figure 7** for images showing existing conditions at Four Mile Run under the Four Mile Run bridge.

The project team respectfully requests input, based on the updates presented above, regarding the Columbia Pike Transit Initiative Project's potential impacts to natural resources in the project study area. A similar letter is being submitted to the Virginia Department of Game and Inland Fisheries and Virginia Department of Environmental Quality. Should you have any questions or comments, please contact me at 703-340-3023 or Susan.Anderson@aecom.com for more information.

Sincerely,



Susan T. Anderson, AICP
Pike Transit Initiative, NEPA Task Leader

Enclosures: Letter dated 10/16/09 from R. Munson, VDCR to R. McElhenny-Smith, WMATA re: Columbia Pike Transit Initiative, Fairfax and Arlington CO

- Figure 1: Proposed Alignment and Study Area (USGS Quadrangles)
- Figure 2: Proposed Alignment and Study Area
- Figure 3: Existing Typical Section - Four Mile Run Bridge
- Figure 4: Proposed Typical Section - Four Mile Run Bridge
- Figure 5: Existing Pier Elevation
- Figure 6: Proposed Pier Elevation
- Figure 7: Four Mile Run Images

cc: WMATA – J. Dittmeier

Figure 1: Proposed Alignment and Study Area (USGS Quadrangles)

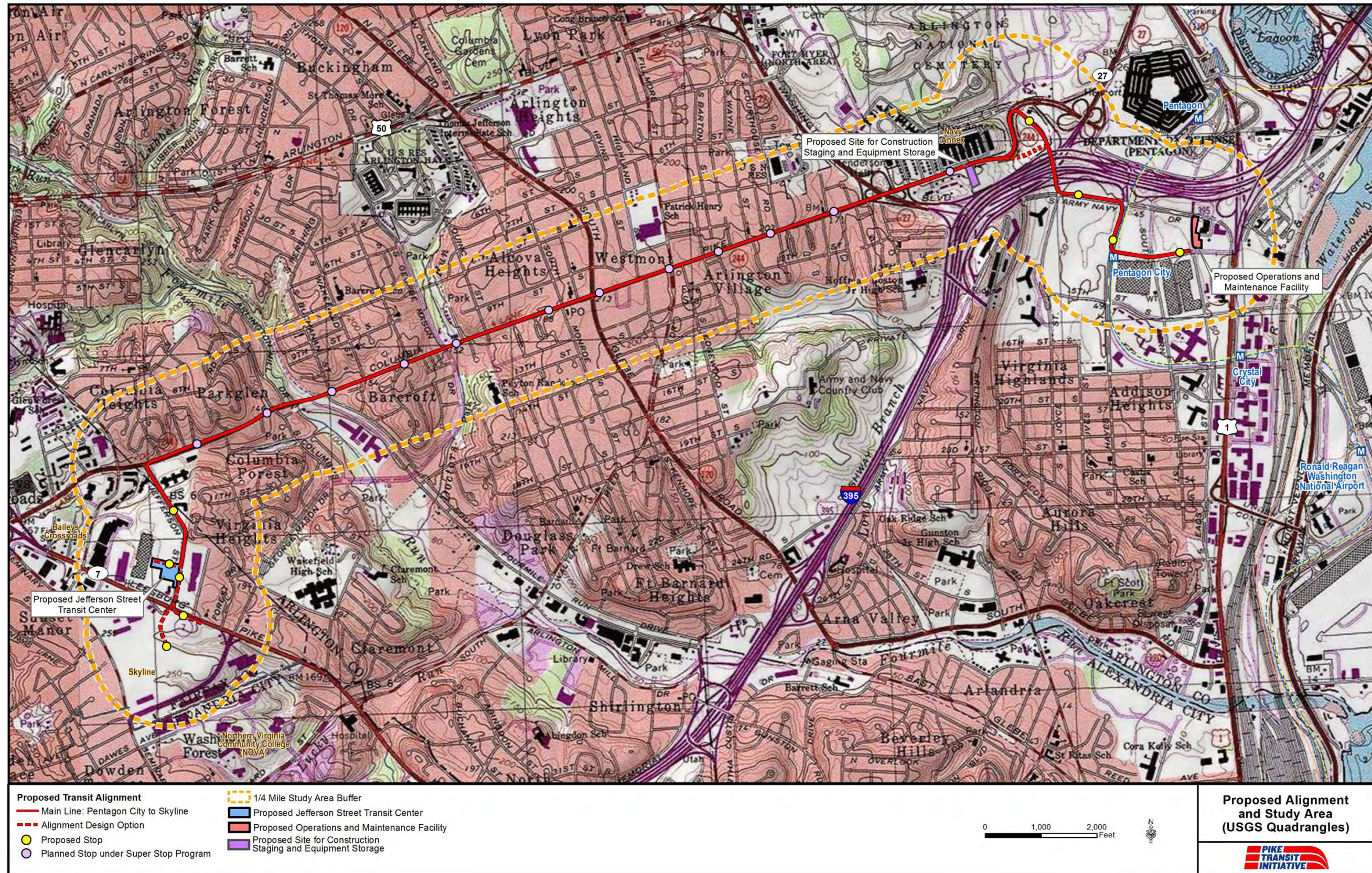
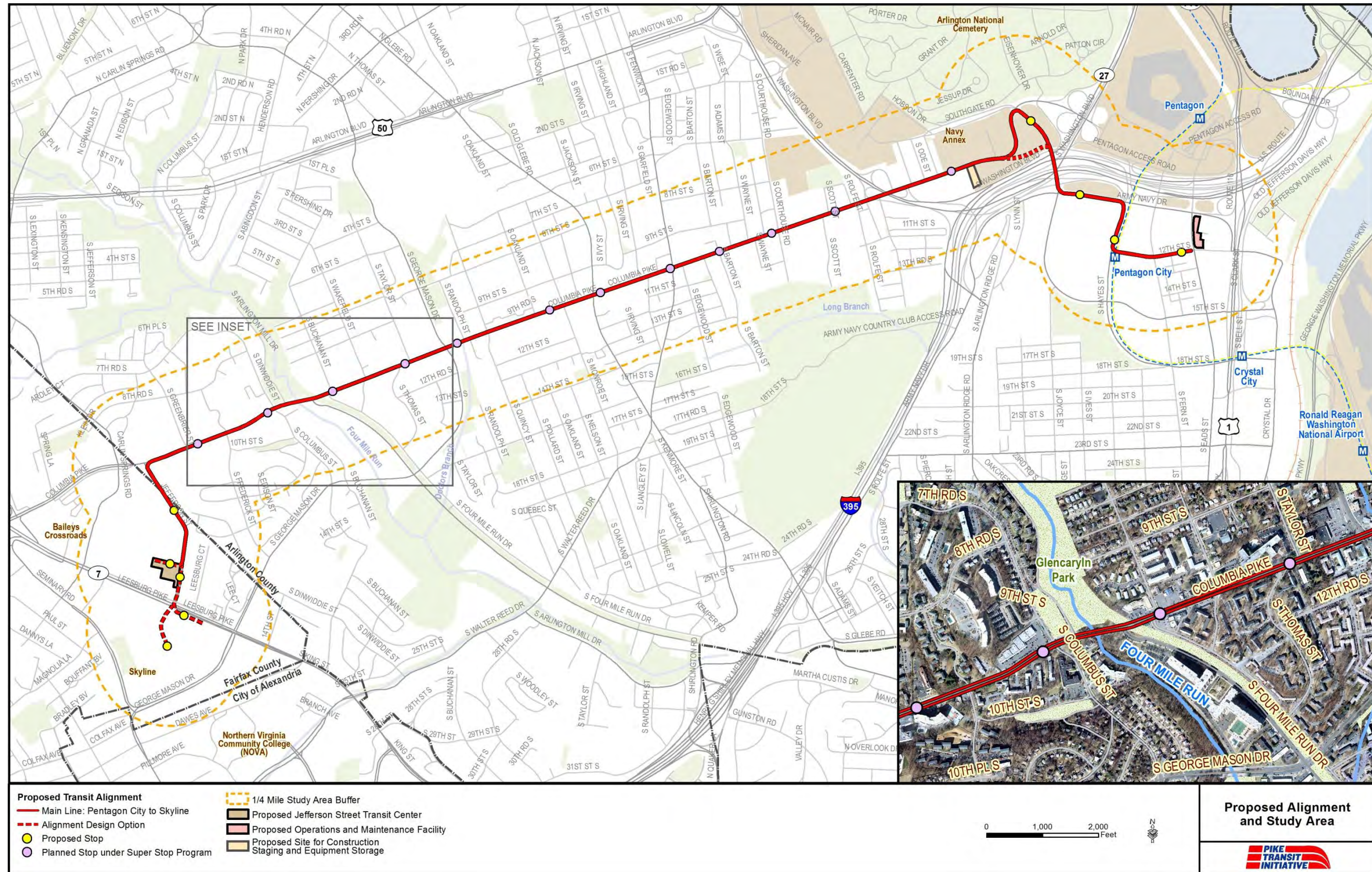
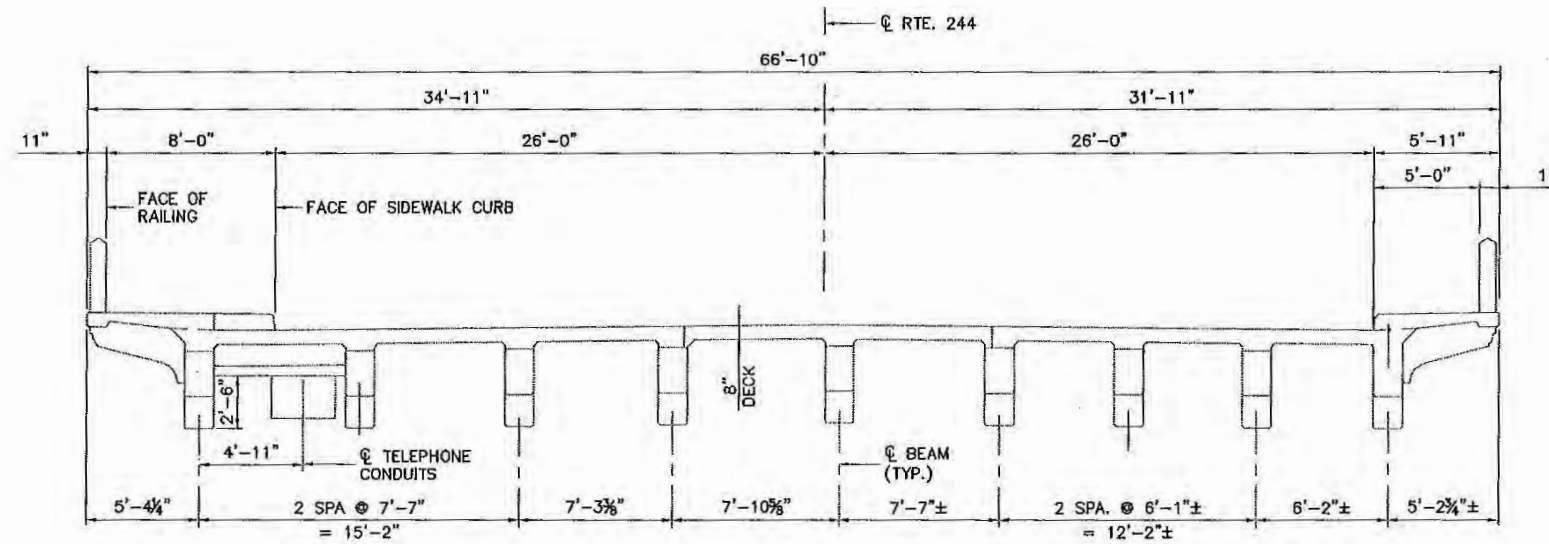


Figure 2: Proposed Alignment and Study Area



Proposed Alignment and Study Area

Figure 3: Existing Typical Section - Four Mile Run Bridge

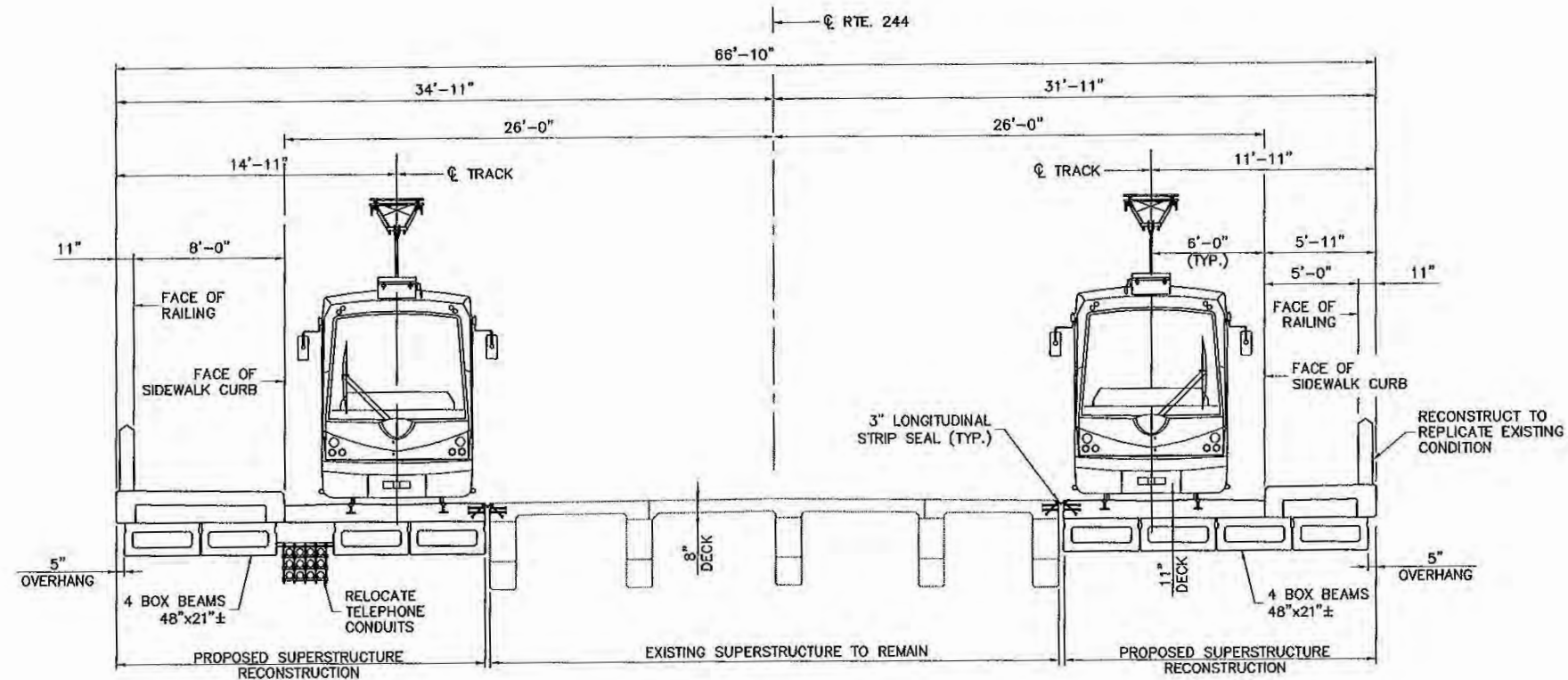


EXISTING TYPICAL SECTION
FOUR MILE RUN BRIDGE

Jul. 15 2011 11:39 am C:\60020294\Structure\Presentation\Exhibits\Columbia Pike_ExistSec.dwg By: jccintoc

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		Date: 08-XX-2010
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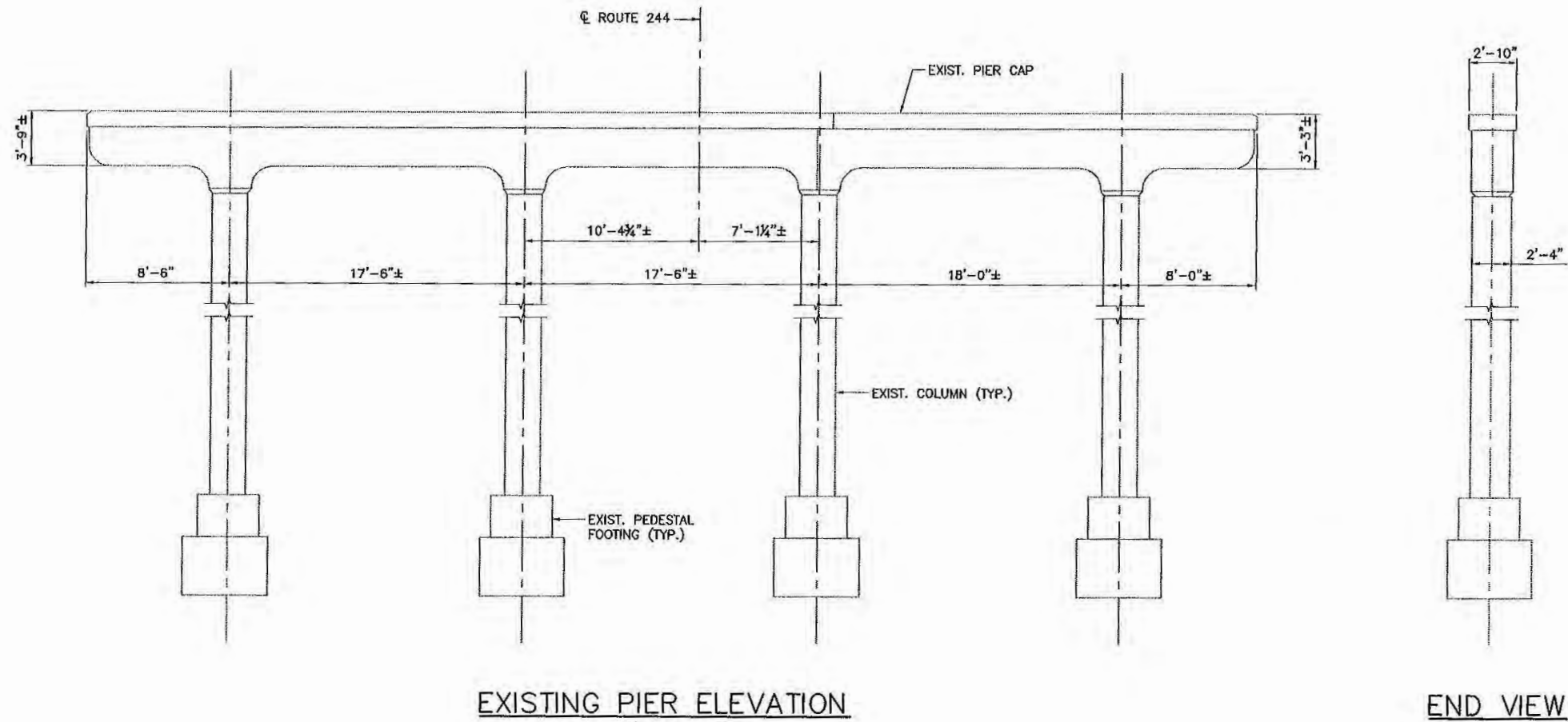


PROPOSED TYPICAL SECTION
FOUR MILE RUN BRIDGE

Jul. 15 2011 11:40 am G:\60020294\Structure\Presentation\Exhibits\Columbia Pike\prospec.dwg By: jacinoc

	<p>Columbia Pike Streetcar Preliminary Streetcar Alignment East Option Proposed Bridge Section Attachment 3</p>	<p>Scale:</p>
		<p>Date: 08-XX-2010</p>
		<p>Dwg. No.</p>
		<p>Sheet No: 3 of 5</p>

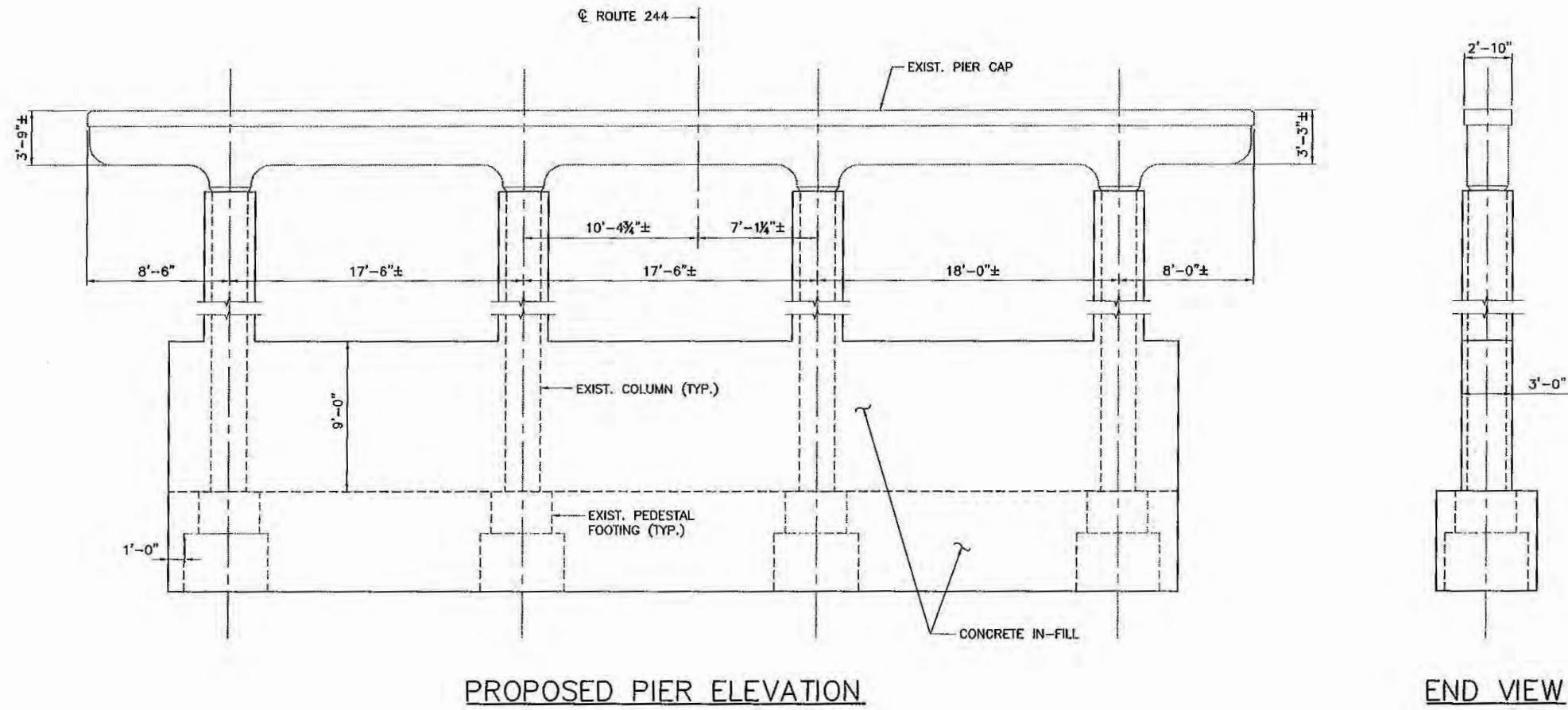
Figure 5: Existing Pier Elevation



Jul 15 2011 11:40 am G:\60020294\Structure\Presentation\Exhibits\Columbia Pike_Exist-elev.dwg By: jacinthe

	<p>Columbia Pike Streetcar Preliminary Streetcar Alignment East Option Typical Existing Pier Elevation Attachment 4</p>	Scale: Date: 08-XX-2010 Dwg. No.: Sheet No: 4 of 5
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Figure 6: Proposed Pier Elevation



Jul 15 2011 11:40 am G:\60020294\Structure\Presentation\Exhibits\Columbia Pike_prop-elev.dwg By: jpcitoc

	<p>Columbia Pike Streetcar Preliminary Streetcar Alignment East Option</p> <p>Proposed Typical Pier with Concrete In-Fills Attachment 5</p>	Scale: Date: 08-XX-2010 Dwg. No.: Sheet No: 5 of 5
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Figure 7: Four Mile Run Images



Four Mile Run bridge (looking northwest from Four Mile Run Trail)



Under Four Mile Run bridge



Four Mile Run



Four Mile Run bridge (looking southwest from Four Mile Run Trail)



September 2, 2010

Virginia Department of Game and Inland Fisheries
Environmental Services Section
P.O. Box 11104
Richmond, VA 23230

Re: Columbia Pike Transit Initiative, Environmental Assessment/Alternatives Analysis

Dear Sir or Madam:

On behalf of Arlington County and Fairfax County, the Washington Metro Transit Authority (WMATA) is conducting an Environmental Assessment of proposed transit improvements along the Columbia Pike corridor. As WMATA's consultant, AECOM is requesting Virginia Department of Game and Inland Fisheries concurrence that the project is not likely to adversely affect federally listed threatened or endangered species.

The project study area, as shown on the attached USGS quadrangles (**Figure 1**) falls within the Virginia jurisdictions of the City of Alexandria, Arlington County and Fairfax County. The study area extends approximately five miles, mostly along the highly urban corridor of Columbia Pike (State Route 244), between Bailey's Crossroads/Skyline and Pentagon City, with deviations onto other roadways at either end of the corridor. The proposed improvements will provide enhanced transit service to connect existing development and new redevelopment along the corridor, improve access to major activity centers and encourage transit ridership growth.

The Federal Transit Administration (FTA) is the lead federal agency for the project and has determined that the appropriate class of action for this project is an Environmental Assessment (EA). The project team is preparing a combined EA and Alternatives Analysis to meet federal requirements under the National Environmental Policy Act of 1969, as amended (NEPA) and to prepare an application for funding through the Federal New Starts/Small Starts program.

As part of the EA, several alternatives will be evaluated to include a No Build Alternative, Transportation Systems Management (TSM) Alternative and a Streetcar Build Alternative. Associated with both the TSM and Streetcar Build Alternatives, some additional right-of-way outside of the existing publicly owned transportation corridor may be required to accommodate relocated or new transit stops, traction power substations, and a primary operations and maintenance facility. The current alignment, potential stop locations, and sites being evaluated for a primary operations and maintenance facility are identified in **Figure 2**, attached to this letter.

September 1, 2010

Virginia Department of Game and Inland Fisheries
Environmental Services Section
P.O. Box 11104
Richmond, VA 23230

Re: Columbia Pike Transit Initiative, Environmental Assessment/Alternatives Analysis

Dear Sir or Madam:

On behalf of Arlington County and Fairfax County, the Washington Metro Transit Authority (WMATA) is conducting an Environmental Assessment of proposed transit improvements along the Columbia Pike corridor. As WMATA's consultant, AECOM is requesting Virginia Department of Game and Inland Fisheries concurrence that the project is not likely to adversely affect federally listed threatened or endangered species.

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The Federal Transit Administration (FTA) is the lead federal agency for the project and has determined that the appropriate class of action for this project is an Environmental Assessment (EA). The project team is preparing a combined EA and Alternatives Analysis to meet federal requirements under the National Environmental Policy Act of 1969, as amended (NEPA) and to prepare an application for funding through the Federal New Starts/Small Starts program.

As part of the EA, several alternatives will be evaluated to include a No Build Alternative, Transportation Systems Management (TSM) Alternative and a Streetcar Build Alternative. Associated with both the TSM and Streetcar Build Alternatives, some additional right-of-way outside of the existing publicly owned transportation corridor may be required to accommodate relocated or new transit stops, traction power substations, and a primary operations and maintenance facility. The current alignment, potential stop locations, and sites being evaluated for a primary operations and maintenance facility are identified in **Figure 2**, attached to this letter.

AECOM reviewed the U.S. Fish and Wildlife Service, Virginia Field Office's website and identified the following federally-listed species:

Scientific Name	Common Name	Status	Jurisdiction(s)
<i>Aeschynomene virginica</i>	Sensitive joint-vetch	LT	Fairfax County
<i>Isotria medeoloides</i>	Small whorled pogonia	LT	Fairfax County
<i>Haliaeetus leucocephalus</i>	Bald Eagle	BGEPA	Fairfax County
<i>Elliptio lanceolata</i>	Yellow lance	Species of Concern	Fairfax County
<i>Sphalloplana holsingeri</i>	Holsinger's groundwater planarian	Species of Concern	Fairfax County
<i>Sphalloplana subtilis</i>	Bigger's groundwater planarian	Species of Concern	Fairfax County
<i>Stygobromus kenki</i>	Rock Creek groundwater amphipod	Species of Concern	Fairfax County
<i>Stygobromus phreaticus</i>	Northern Virginia well amphipod	Species of Concern	Fairfax County, City of Alexandria
<i>Stygobromus sp. 15</i>	A groundwater amphipod	Species of Concern	Fairfax County, Arlington County
<i>Pycnanthemum torrei</i>	Torrey's mountain-mint	Species of Concern	Fairfax County, Arlington County, City of Alexandria

Source: USFWS, Virginia Field Office website, August 2010

LT – Listed Threatened

BGEPA – Bald and Golden Eagle Protection Act

Based on a review of the species fact sheet provided for the Sensitive joint-vetch, this species occurs in fresh to slightly brackish tidal river systems, typically at the outer fringe of marshes or shores. Within the study area, Columbia Pike crosses Four Mile Run within Arlington County, west of its convergence with Four Mile Run Creek in Alexandria. Four Mile Run within Arlington is not tidally influenced and therefore it is assumed that the appropriate habitat to support the Sensitive joint-vetch does not occur within the study corridor. Furthermore, the proposed transit investment would stay mostly within existing transportation rights-of-way that are currently paved or covered by concrete; the project would cross Four Mile Run via an existing bridge, and does not propose changes to this structure, other than enhanced structural deck support, if so warranted by engineering analysis.

The Small whorled pogonia fact sheet indicates that this species is found in third-growth upland forests with an open understory and a closed canopy where the topography is typically moderately sloping or almost level. This species requires acidic sandy loam soils with low nutrient content. A portion of the study area within the City of Alexandria is a potential site for an operations and maintenance facility on the Northern Virginia Community College Campus (NOVA). As currently proposed, the site is located on a level recreational field, adjacent to an upland forested area. The project understands that this area partially consists of spoil removed from other parts of the campus during various construction activities. A review of soils information from the USDA Natural Resources Conservation Service shows that the primary soil types include the Kingstowne sandy clay loam, urban land, and the Sassafras-Marumsc complex. These soils are not defined as acidic sandy loams. Therefore it appears that the site conditions do not support the habitat required for the Small whorled pogonia.

A review of the Virginia Department of Game and Inland Fisheries, Fish and Wildlife Information Service indicated that there are no protected species known to occur on the Alexandria, VA-DC-MD USGS Quadrangle and there are two state protected species listed for the Annandale, VA USGS Quadrangle. These species are the *Glyptemys insculpta* (Wood turtle) and the *Ambystoma mabeei* (Mabee's Salamander) and both are listed as State Threatened. The Wood Turtle is known to inhabit a variety of habitats, such as forested floodplains, fields, wet meadows, and farmland, as long as there is a creek or stream nearby. Mabee's Salamander is known from six localities in the coastal plain in extreme southeastern Virginia: one each in the cities of Hampton and Suffolk and the counties of York, Southampton, Gloucester, and Isle of Wight. It is also found in Newport News. Breeding sites are fish-free vernal ponds or ephemeral coastal plain sinkholes up to 1.5 meters deep, with surrounding forests generally composed of hardwoods mixed with pine. It is also found in low areas adjacent to coastal rivers and pine savannas, and in bogs, ponds, low wet woods, and swamps.

It is unlikely that habitat exists within this developed urban corridor that supports either the Wood Turtle or Mabee's Salamander. The one surface water feature that the study area crosses is Four Mile Run in Arlington County. The project proposes to use the existing bridge and would not impact Four Mile Run. A floodplain is associated with Four Mile Run within the study area, but would not be impacted by the proposed project. Based on the existing conditions of the study corridor, it is not expected that either species would be affected by the proposed project.



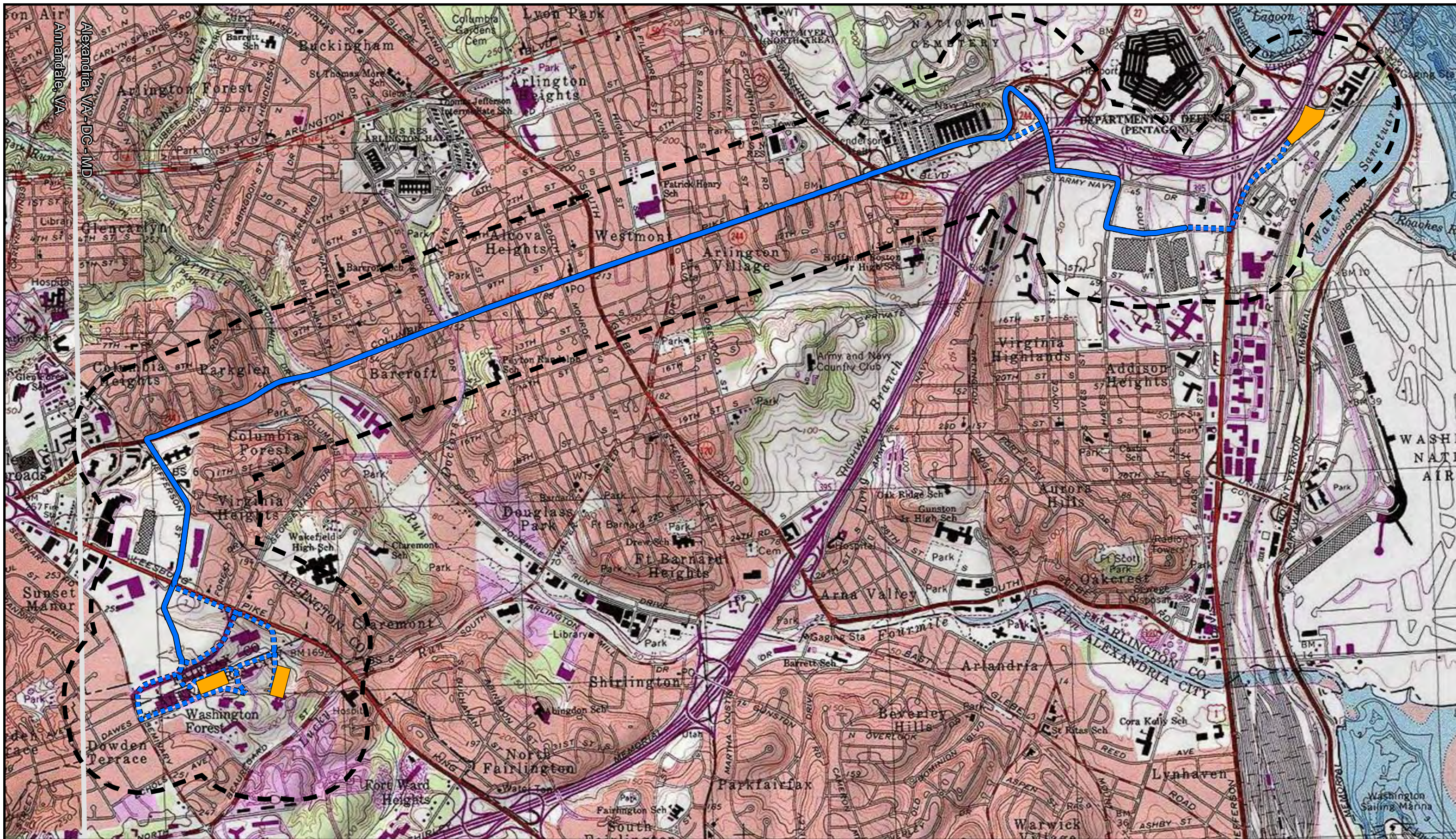
The project team respectfully requests concurrence, based on the information presented above, that the Columbia Pike Transit Initiative Project would not likely adversely affect federally listed threatened or endangered species or state listed threatened or endangered species. A similar letter is being submitted to both the USFWS Virginia Field Office and the Virginia Department of Conservation and Recreation. Should you have any questions or comments, please contact me at 703-682-5008 or Susan.Anderson@aecom.com for more information.

Sincerely,

A handwritten signature in black ink that reads "Susan T. Anderson".

Susan T. Anderson, AICP
Pike Transit Initiative, NEPA Task Leader

Enclosures: Figure 1: Project Study Area USGS Quadrangles
Figure 2: Project Alignment Map



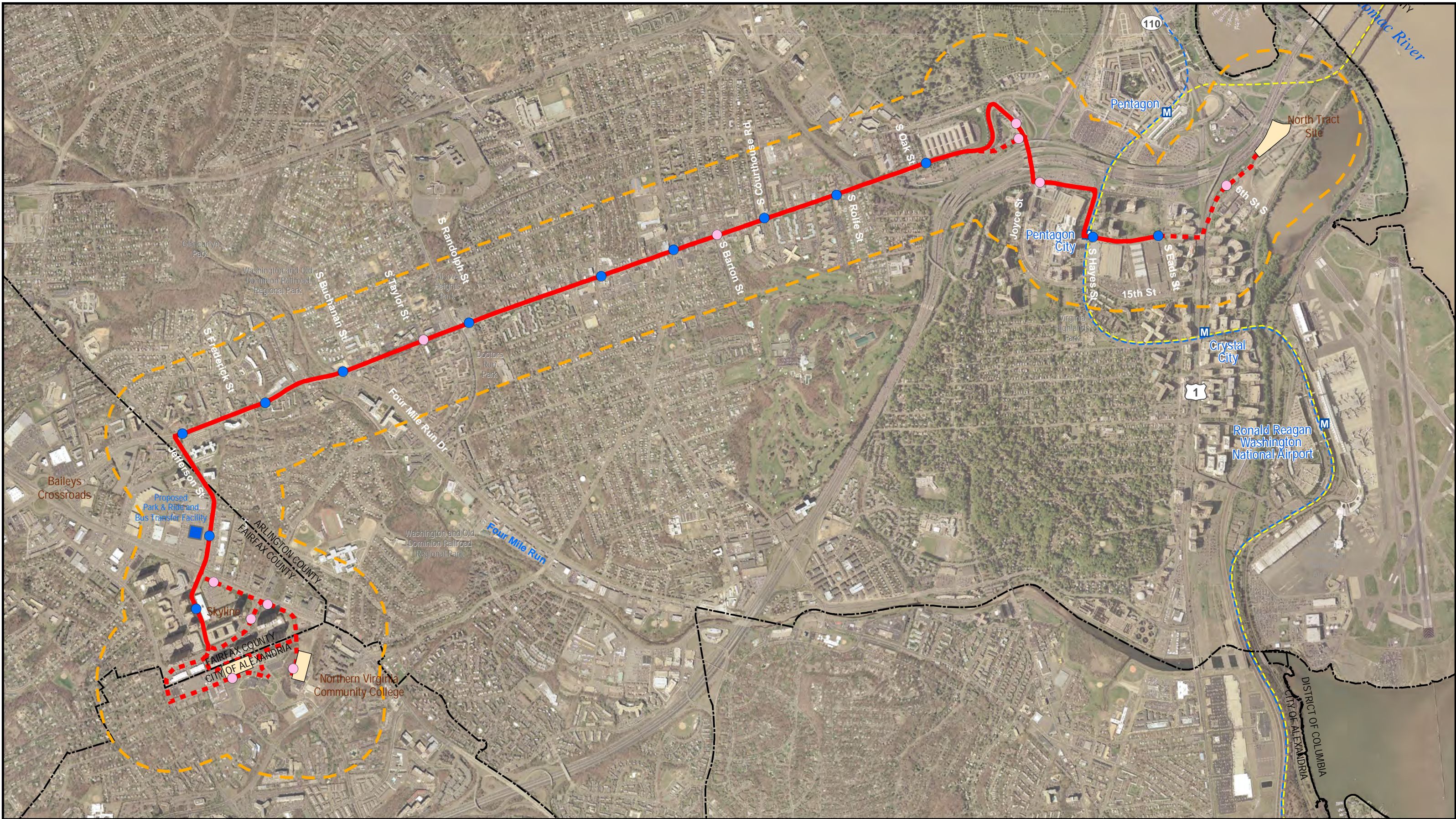
LEGEND

- 1/4 Mile Study Area Buffer
- Transit Alignment
- Alternative Alignment Option
- Maintenance Facility Option
- USGS_Quad_Sheets

Source: USGS Annandale, VA and Alexandria, VA - DC - MD Quadrangles, 2009

Figure 1

Proposed Alignment and Study Area



LEGEND

- Station Location
- Optional Station
- Transit Alignment
- - - Alternate Alignment Option
- Maintenance Facility Option
- M Metrorail Station
- - - 1/4 Mile Study Area Buffer
- - - Metrorail Blue Line
- - - Metrorail Yellow Line

Figure 2



0 0.25 0.5 Miles



Proposed Alignment and Stations



September 2, 2010

Virginia Department of Conservation and Recreation
Division of Natural Heritage
Attn: Rene Hypes
217 Governor Street, 2nd Floor
Richmond, VA 23219

Re: Columbia Pike Transit Initiative, Environmental Assessment/Alternatives Analysis

Dear Sir or Madam:

On behalf of Arlington County and Fairfax County, the Washington Metro Transit Authority (WMATA) is conducting an Environmental Assessment of proposed transit improvements along the Columbia Pike corridor. As WMATA's consultant, AECOM is requesting Virginia Department of Conservation and Recreation concurrence that the project is not likely to adversely affect federally listed threatened or endangered species.

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The Federal Transit Administration (FTA) is the lead federal agency for the project and has determined that the appropriate class of action for this project is an Environmental Assessment (EA). The project team is preparing a combined EA and Alternatives Analysis to meet federal requirements under the National Environmental Policy Act of 1969, as amended (NEPA) and to prepare an application for funding through the Federal New Starts/Small Starts program.

As part of the EA, several alternatives will be evaluated to include a No Build Alternative, Transportation Systems Management (TSM) Alternative and a Streetcar Build Alternative. Associated with both the TSM and Streetcar Build Alternatives, some additional right-of-way outside of the existing publicly owned transportation corridor may be required to accommodate relocated or new transit stops, traction power substations, and a primary operations and maintenance facility. The current alignment, potential stop locations, and sites being evaluated for a primary operations and maintenance facility are identified in **Figure 2**, attached to this letter.

AECOM reviewed the U.S. Fish and Wildlife Service, Virginia Field Office's website and identified the following federally-listed species:

Scientific Name	Common Name	Status	Jurisdiction(s)
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<i>Haliaeetus leucocephalus</i>	Bald Eagle	BGEPA	Fairfax County
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Source: USFWS, Virginia Field Office website, August 2010

LT – Listed Threatened

BGEPA – Bald and Golden Eagle Protection Act



Based on a review of the species fact sheet provided for the Sensitive joint-vetch, this species occurs in fresh to slightly brackish tidal river systems, typically at the outer fringe of marshes or shores. Within the study area, Columbia Pike crosses Four Mile Run within Arlington County, west of its convergence with Four Mile Run Creek in Alexandria. Four Mile Run within Arlington is not tidally influenced and therefore it is assumed that the appropriate habitat to support the Sensitive joint-vetch does not occur within the study corridor. Furthermore, the proposed transit investment would stay mostly within existing transportation rights-of-way that are currently paved or covered by concrete; the project would cross Four Mile Run via an existing bridge, and does not propose changes to this structure, other than enhanced structural deck support, if so warranted by engineering analysis.

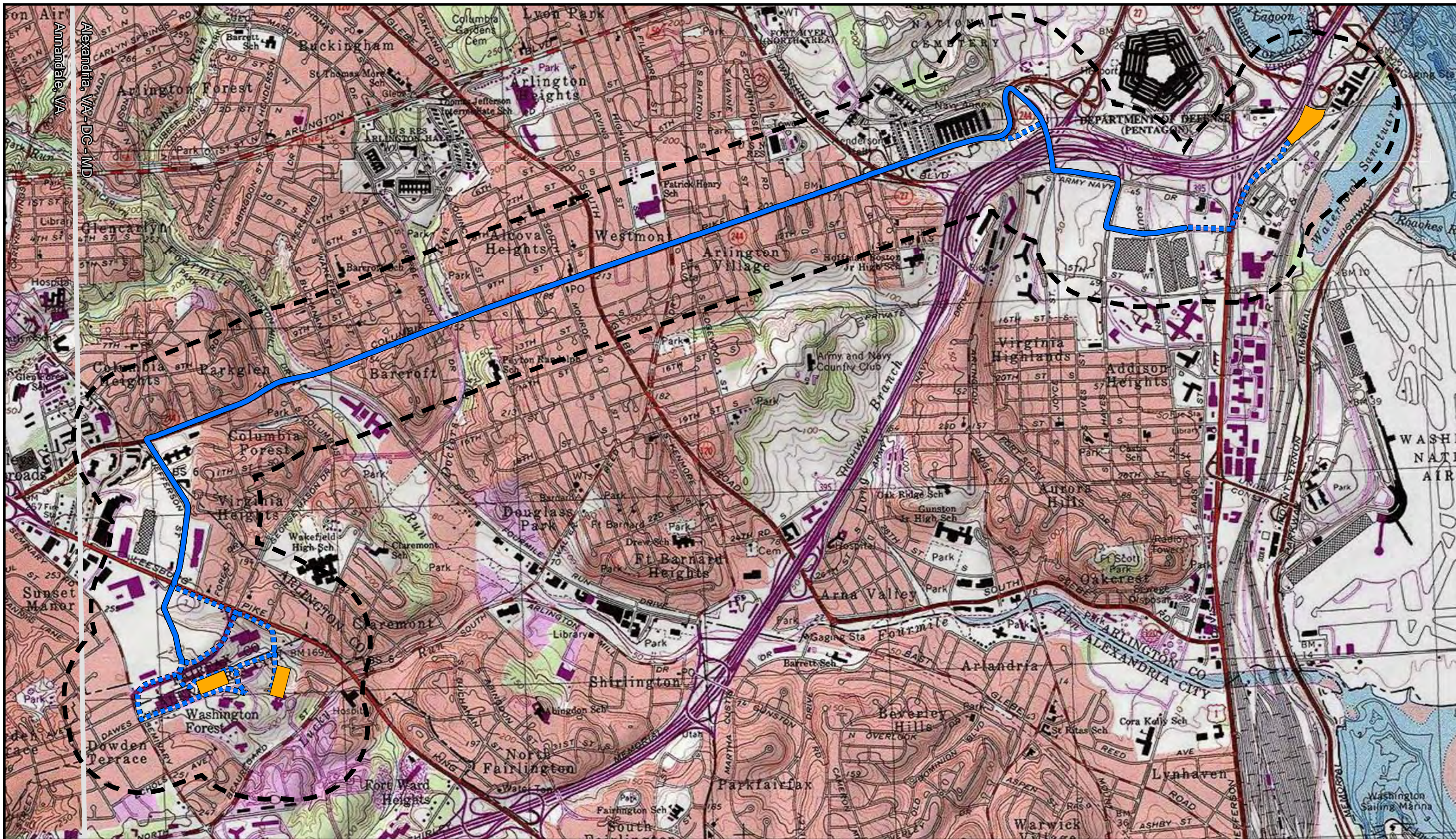
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The project team respectfully requests concurrence, based on the information presented above, that the Columbia Pike Transit Initiative Project would not likely adversely affect federally listed threatened or endangered species of concern. The project also requests relevant information on species under your purview. A similar letter is being submitted to both the USFWS Virginia Field Office and the Department of Game and Inland Fisheries. Should you have any questions or comments, please contact me at 703-682-5008 or Susan.Anderson@aecom.com for more information.

Sincerely,

Susan T. Anderson, AICP
Pike Transit Initiative, NEPA Task Leader

Enclosures: Figure 1: Project Study Area USGS Quadrangles
Figure 2: Project Alignment Map



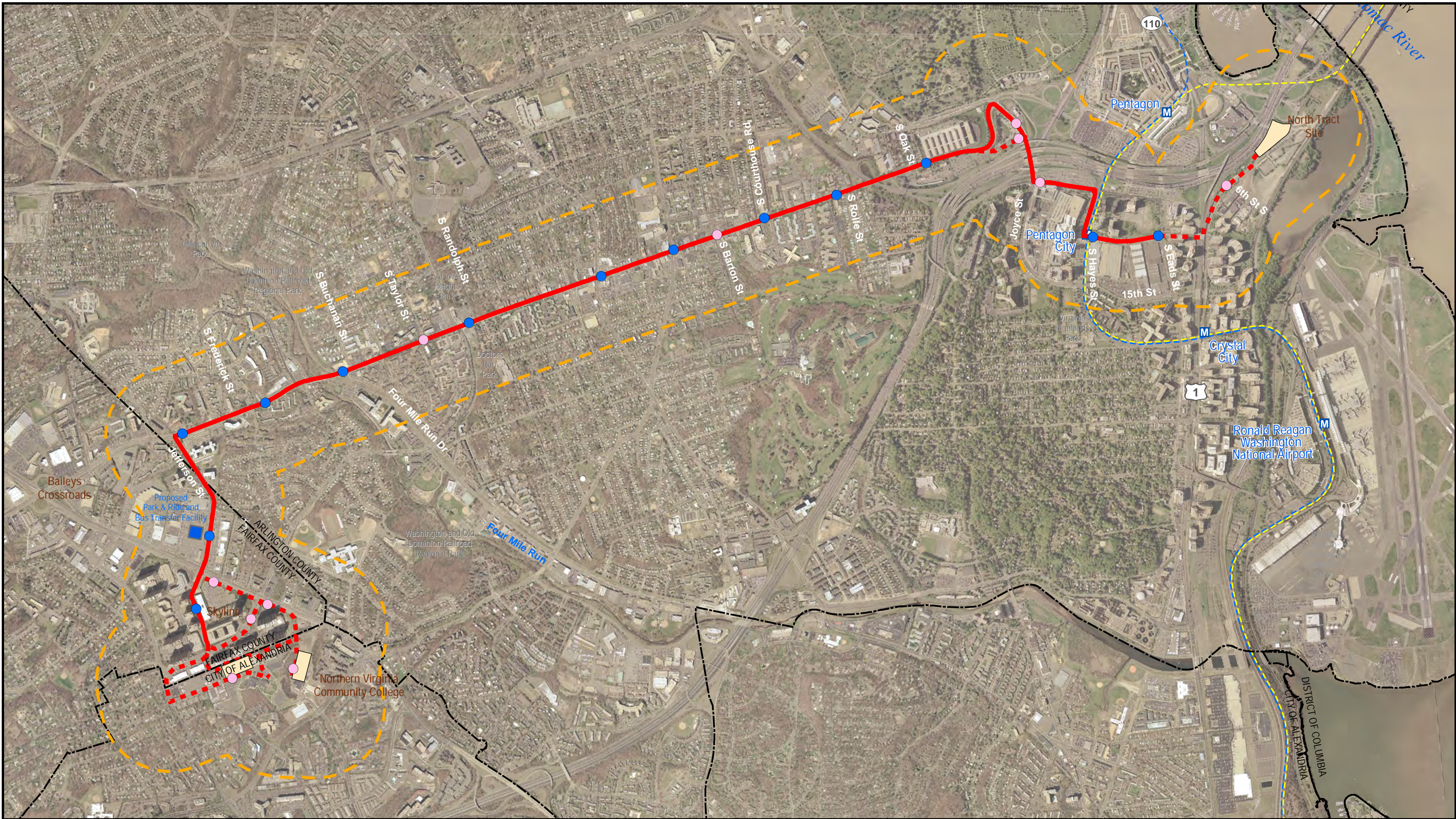
LEGEND

- 1/4 Mile Study Area Buffer
- Transit Alignment
- Alternative Alignment Option
- Maintenance Facility Option
- USGS_Quad_Sheets

Source: USGS Annandale, VA and Alexandria, VA - DC - MD Quadrangles, 2009

Figure 1

Proposed Alignment and Study Area



LEGEND

- Station Location
- Optional Station
- Transit Alignment
- - - Alternate Alignment Option
- Maintenance Facility Option
- M Metrorail Station
- - - 1/4 Mile Study Area Buffer
- - - Metrorail Blue Line
- - - Metrorail Yellow Line

Figure 2



0 0.25 0.5 Miles



Proposed Alignment and Stations



August 9, 2010

Mr. Marc Holma, Architectural Historian
Office of Review and Compliance
Virginia Department of Historic Resources
2801 Kensington Avenue
Richmond, VA 23221

Re: Project Review Application for the Columbia Pike Transit Initiative

Dear Mr. Holma:

AECOM, on behalf of the Columbia Pike Transit Initiative project management team, is submitting for your review a Project Review Application Form for the above referenced project. The current phase of the Columbia Pike Transit Initiative includes environmental documentation for proposed transit improvements extending five miles along Columbia Pike between Bailey's Crossroads/Skyline area in Fairfax County and Pentagon City in Arlington County, Virginia. The improvements will provide enhanced transit service to connect existing and new land development along the corridor, improve access to major activity centers, and encourage transit ridership growth.

An Environmental Assessment (EA), in accordance with the National Environmental Policy Act (NEPA) of 1969, as amended, is being prepared for the project. The Federal Transit Administration (FTA) is the lead federal agency for the project. The Washington Metropolitan Area Transit Authority (WMATA) is leading the technical effort on behalf of Arlington and Fairfax Counties.

The project management team (two Counties, WMATA and AECOM) requests that you review the information presented and provide any relevant information on existing or proposed conditions under your agency's purview for the study areas identified. The team respectfully requests your responses at your earliest convenience. Your input on this study is greatly appreciated. Should you have any questions, please contact me at 609-310-3194.

Sincerely,

Alan Tabachnick
National Director of Cultural Resources, Transportation
AECOM

Enclosures:
Project Review Application Form

CC: Susan Anderson, AECOM ✓
John M. Dittmeier, WMATA
Jason Mumford, AECOM

WMATA-PLAN
600 Fifth Street, NW, Room 6F-16
Washington, DC 20001

www.piketranst.com

8. *Project Acreage*

The proposed project alignment extends northwest from the Washington Forest area of Alexandria City to Baileys Crossroads in Fairfax County and then continues east-northeast along Columbia Pike (SR 244) in Arlington County to the Navy Annex Building. It then turns southeast and continues to the east along Army Navy Drive, South Hayes Street, 12th Street South and South Old Jefferson Davis Highway in Pentagon City. In addition to the project corridor, approximately 20 station locations and two storage and maintenance facilities are currently under consideration. As currently envisioned, the proposed transit investment would generally operate in shared traffic lanes within existing streets. Alternatives under evaluation include the No Build; a Transportation System Management (TSM), focused on enhancing current bus service within the corridor; and a Streetcar Build Alternative.

Table 8-1: Estimated Acreage

Component	Length (meters)	Approximate Area (sq. meters)	Approximate Area (acres)
Station Facilities	NA	121,405	30
Transit Corridor	22,126	331,890 ¹	82
Total			112

9. *Previous Architectural and Archaeological Surveys in the General Study Area*

A reconnaissance level survey has been undertaken as part of this current project. This survey has consisted of initial background research and a file search at the VDHR.

Historic Resources

Within ¼ mile of the proposed corridor and facilities, 328 eligible, potentially eligible and unevaluated historic resources (buildings, structures, districts, etc.) have been recorded with VDHR. Of these, eleven are currently listed in the National Register of Historic Places (NRHP) and in the Virginia Landmarks Register (VLR). These resources are:

- Boundary Markers of the Original District of Columbia MPD (000-0022) (NRHP, VLR)
- Arlington Village Historic District (000-0024) (NRHP, VLR)
- Barcroft Community House (000-0040) (NRHP, VLR)
- Harry W. Gray House, 1005 South Quinn Street (000-0515) (NRHP, VLR)
- Arlington Heights Historic District (000-3383) (NRHP, VLR)
- Penrose Historic District (000-8823) (NRHP, VLR)
- Walter Reed Gardens Historic District (000-8824) (NRHP, VLR)
- Columbia Forest Historic District (000-9416) (NRHP, VLR)
- Claremont Historic District (000-9700) (NRHP, VLR)
- Virginia Heights Historic District (000-9701) (NRHP, VLR)
- Mount Vernon Memorial Highway (029-0218) (NRHP, VLR)

Additionally five potentially eligible resources that have not been formally listed in the NRHP or the VLR were identified within ¼ mile of the proposed corridor and facilities. They are:

- Arlington National Cemetery (000-0042)
- The Pentagon Navy Annex (000-3371)

¹ Assumes a 15.0 meter wide corridor.

- Barcroft Historic District (000-7818)
- Barcroft Apartments Historic District (000-9419) (Potentially Eligible: 12/13/07)
- Washington and Old Dominion Railroad Regional Park (W&OD Trail) (053-0276) (Potentially Eligible: 2/4/99)

The proposed corridor and facilities pose potential direct impacts to fourteen historic resources recorded with VDHR. In each case the proposed project corridor or facilities either directly intersects the resource in question or passes in close proximity thereto. The twelve resources are:

- Arlington Village Historic District (000-0024) (NRHP, VLR)
- VDOT Central Control Complex, 1440 Columbia Pike (000-1990) (Unevaluated)
- Arlington Village Center (000-2132) (Unevaluated)
- Arlington Post Office (000-2197) (Unevaluated)
- Commercial Building, 919-927 Walter Reed Drive South (000-2199) (Unevaluated)
- Old Dominion Bank (000-2201) (Unevaluated)
- Arlington Hardware (000-2203) (Unevaluated)
- Arlington Theater (000-2214) (Unevaluated)
- Saint Coletta Thrift Store (000-2216) (Unevaluated)
- Barcroft Historic District (000-7818) (Potentially Eligible: 7/12/01)
- Columbia Pike/S. Walter Reed Drive Commercial Historic District (000-9418) (Unevaluated)
- Barcroft Apartments Historic District (000-9419) (Potentially Eligible: 12/13/07)
- Washington, Arlington and Falls Church Electric railway Co. Trolley Line (029-5470)
- Washington and Old Dominion Railroad Regional Park (W&OD Trail) (053-0276) (Potentially Eligible: 2/4/99)

Archaeological Resources

With regard to archaeological resources, six archaeological sites have been recorded with VDHR within ¼ mile of the proposed corridor and facilities. They are:

- Foxcroft Heights (44AR0019)
- Jackson City (44AR0037)
- Alexandria Canal (44AX0028)
- 44AX0032
- 4419 Seminary Road (44AX0121)
- Dowden Terrace Cemetery (44FX1370)

None of the recorded sites listed above have been evaluated for NRHP or VLR eligibility. The proposed project corridor and facilities may pose potential direct impacts to the Alexandria Canal (44AX0028) which it intersects.

Previous Cultural Resource Surveys on File at VDHR

Adams, Robert M., Martha W. McCartney, Michael F. Johnson and Lawrence E. Moore
1993 *Archaeological Investigation of the Stonegate Development (Including Sites 44AX31, 166 & 167), West Braddock Road, City of Alexandria, Virginia.* Report AX-45v.1 on file, DHR, Richmond, VA.

Bromberg, Francine W. and Elizabeth Crowell

1989 *Phase I and II Archaeological Investigations of the West Entrance Road, Washington National Airport, Arlington County, Virginia.* Report AR-10 on file, DHR, Richmond, VA.

DMJM

1991 *Environmental Assessment of the Pentagon Reservation Master Plan.* Report AR-11v.1 on file, DHR, Richmond, VA.

Einhorn Yaffe Prescott, P.C.

1990 *Study Report: Renovation of the Memorial Amphitheater, Arlington National Cemetery, Virginia.* Report AR-12 on file, DHR, Richmond, VA.

Embrey, James W., Lynn D. Jones and Joseph Balicki

2005 *Documentary Study, Archaeological Evaluation and Resource Management Plan for Virginia Theological Seminary Faculty Housing, Alexandria, Virginia.* Report AX-95 on file, DHR, Richmond, VA.

Gardner, William M. and Jennifer Schmidt

1997 *Phase I Archaeological Investigations at the Proposed 7 Acre Parking Lot, First Baptist Church, Alexandria, Virginia.* Report AX-81 on file, DHR, Richmond, VA.

Greenhorne & O'Mara, Inc.

1999 *Archaeology of the Abingdon Plantation Site (44AR18), Ronald Reagan Washington National Airport, Arlington County, Virginia.* Report AR-46 on file, DHR, Richmond, VA.

Higgins, Thomas F., Donald W. Linebaugh, Scott M. Hudlow and Anna L. Gray

1993 *A Phase I Cultural Resources Survey of the Proposed Traffic Management System Building Associated with the Interstate 395 Project, Arlington County, Virginia.* Report AR-34 on file, DHR, Richmond, VA.

Hutson, Jarod

2008 *Addendum to the Phase I Archaeological Investigations of the I-95/395 HOV/Bus/Hot Lanes Project, Arlington, Fairfax, Prince William and Stafford Counties and the City of Alexandria, Virginia.* Report PW-321 on file, DHR, Richmond, VA.

Jirikowic, Christine, John Mullen and Gwen J. Hurst

2004 *Phase I Archaeological Investigations of the Circa Five Acre Summers Property, Fairfax County, Virginia.* Report FX-409 on file, DHR, Richmond, VA.

Keyes Condon Florance

1986 *Master Development Plan and Environmental Assessment, Pentagon Complex and Adjacent Areas, Arlington County, Virginia.* Report AR-7v.1 on file, DHR, Richmond, VA.

KFS Historic Preservation Group

1992 *Phase I Archaeological Survey, BRAC Project Areas, Fort Myer, Arlington County, Virginia.* Report AR-31 on file, DHR, Richmond, VA.

Millis, Heather, Jeff Holland, Todd Cleveland and Bill Nethery

1998 *Cultural Investigations at Section 29 at Arlington House, The Robert E. Lee Memorial, Arlington County, Virginia.* Report AR-47 on file, DHR, Richmond, VA.

Parson Management Consultants

1989 *Historic and Archaeological Survey Report, Washington National Airport.* Report on file, DHR, Richmond, VA.

R. Christopher Goodwin & Associates, Inc.

2004 *Archaeological Background Study and Field Reconnaissance for the Proposed Arlington Main Post Office Redevelopment Project, Arlington, Virginia.* Report AR-56 on file, DHR, Richmond, VA.

Seifert, Donna J.

1992 *Phase IA Archaeological Assessment for the Consolidation of the Naval Systems Commands, Alexandria and Arlington County, Virginia.* Report AX-31 on file, DHR, Richmond, VA.

Simmons, Scott E. and Nancy J. Kassner

1991 *Intensive Archaeological Investigations of the Ramp 'En' at Washington National Airport, Arlington County, Virginia.* Report AR-13 on file, DHR, Richmond, VA.

Tracerics

1992 *Evaluation and Historic Documentation for the Quincy Street Extension Project, Arlington, Virginia.* Report AR-18 on file, DHR, Richmond, VA.

United States Army Corps of Engineers, Baltimore District

2002 *Cultural Resource Investigation, National Park Service Property, Arlington National Cemetery, Land Development 90, Stormwater Outfall Tunnel, Arlington County, Virginia.* Report AR-52 on file, DHR, Richmond, VA.

10. Structures 50 years old or older

The project runs primarily down a heavily developed commercial and residential corridor consisting of shopping centers, apartment complexes, stores, restaurants, office buildings, government buildings and residential areas. The project alignment extends northwest from the Washington Forest area of Alexandria City to Baileys Crossroads in Fairfax County. From there most of the project corridor travels along Columbia Pike (SR 244) to the Navy Annex Building west of the Pentagon. Thereafter it continues south of I-395 terminating at Interchange 10.

Area of Potential Effects – Architectural Resources

An Area of Potential Effect (APE) for this project is anticipated to include both sides of Columbia Pike from its eastern terminus to its western terminus, as depicted on Figure 10-1. The APE may be expanded and/or shifted as the design develops over time. The APE as depicted is large enough to include all resources over 50 years of age with the potential to be affected by the proposed project. The APE will be developed in consultation with VDHR.

Columbia Pike Transit Initiative
 COLUMBIA PIKE CORRIDOR

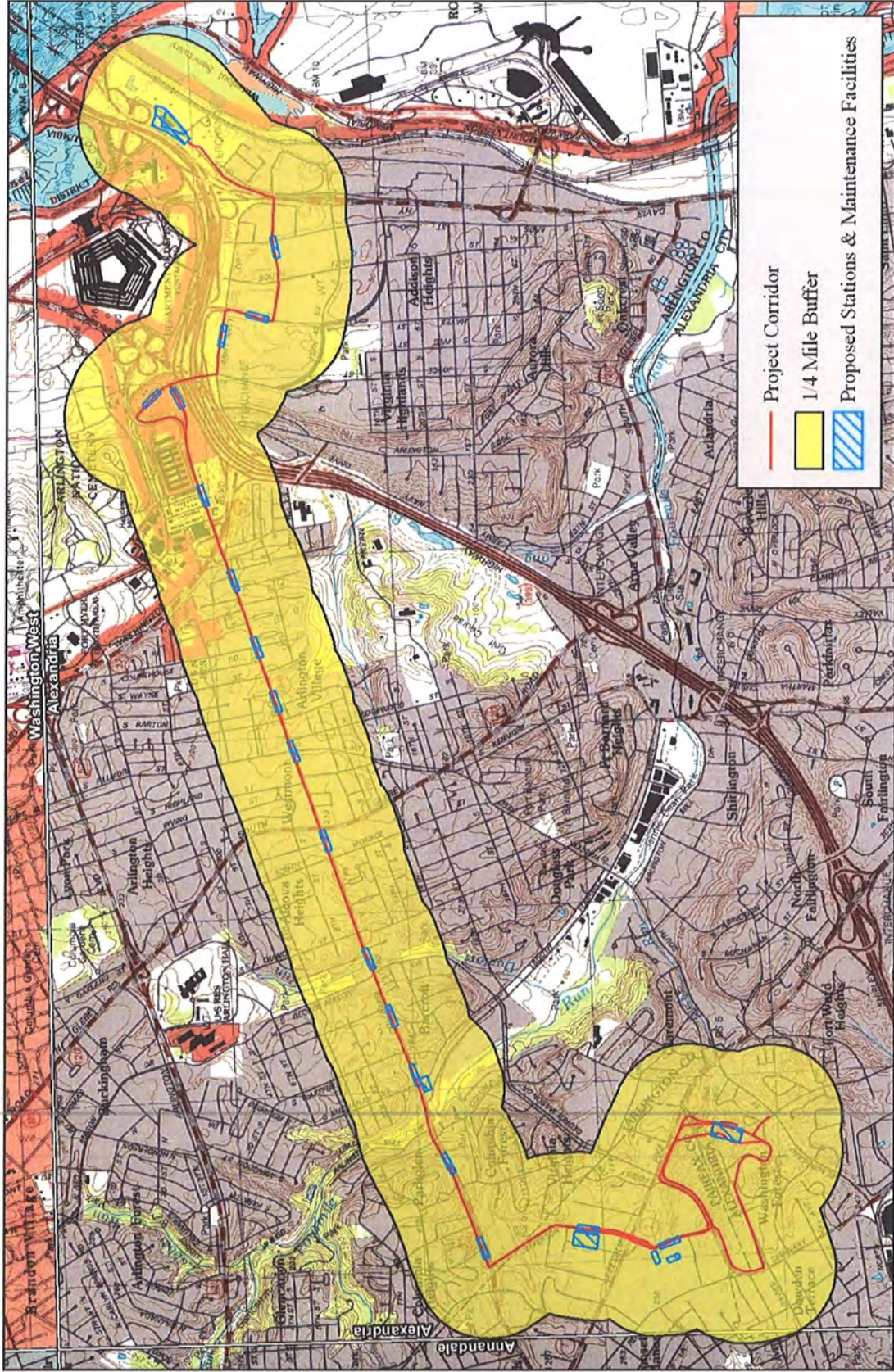


Figure 10-1: Project Area.



Project Description and Fact Sheet

The current phase of the Columbia Pike Transit Initiative includes environmental documentation and preliminary engineering for proposed transit improvements extending five miles along Columbia Pike between Bailey's Crossroads/Skyline area in Fairfax County and Pentagon City in Arlington County, Virginia. This documentation continues the local alternatives analysis of 2005 and prepares the project for possible Federal funding.

The Columbia Pike Transit Initiative is one element of a decade-long effort by Arlington and Fairfax Counties to accommodate growing demand for transit service along this quickly redeveloping urban corridor. Citizens, planners, and community leaders have expressed desire for a modern, higher capacity transit system that supports expected levels of ridership and reinforces the "Main Street" environment envisioned for Columbia Pike.

Demographic Characteristics

- Population: 67,000 residents after growth of 10,000 between 1990 and 2000.
85,000 residents by 2030.
- Employment: 73,000 jobs within the study area.
100,000 jobs by 2030.

Transit Characteristics

- 15,000 weekday corridor ridership (WMATA and Arlington Transit (ART) bus services).
- Significant ridership increase with expanded PikeRide bus service.
- Transit and walk/bike trips are 25 to 30 percent of all corridor trips.
- Current PikeRide: branded service, some limited stop service, and signal priority on some routes.
- Future PikeRide: expanded signal priority and passenger information, plus "Super Stops".

Proposed Improvements

The environmental documentation will evaluate the alternatives of no build, enhanced bus, and streetcar. As proposed, the transit improvements are expected to have the following features:

- Transit would generally operate in shared traffic lanes within existing streets.
- Stations/stops with improved shelters, passenger amenities, and real-time information.
- Fare pre-payment and integration with WMATA's SmartTrip system.
- Operations: six-minute all-day service supplemented by transit bus during peak hours.
- One primary vehicle storage and maintenance facility

Conditions along the corridor are very urban with a mix of commercial and residential land uses. Most of the corridor has been disturbed over the years to make way for the various developments that exist. Very little natural environment exists with the exception of designated recreation areas, landscaped areas, and Four Mile Run and Doctor's Branch. Please visit the project web site, www.piketransit.com, for more information.

Requesting a Project Review from the Department of Historic Resources

The Department of Historic Resources (DHR) is Virginia's State Historic Preservation Office (SHPO). Section 106 of the National Historic Preservation Act of 1966, as amended, requires federal agencies to consult with the SHPO and others who may have knowledge of historic properties in identifying known historic properties which may be affected by a federal undertaking, and in determining the need for further survey efforts to identify previously unrecorded historic properties. Information on Section 106 and the text of the Section 106 regulations are available on the web site of the Advisory Council on Historic Preservation (www.achp.gov).

THIS APPLICATION MUST BE COMPLETED FOR ALL FEDERAL UNDERTAKINGS AND SUBMITTED TO THE VIRGINIA DEPARTMENT OF HISTORIC RESOURCES FOR REVIEW. A federal undertaking is defined in the Section 106 regulations as "a project, activity, or program funded in whole or in part under the direct or indirect jurisdiction of a Federal agency, including those carried out by or on behalf of a Federal agency; those carried out with Federal financial assistance; those requiring a Federal permit, license or approval; and those subject to State or local regulation administered pursuant to a delegation or approval by a Federal agency." **This form may also be used to obtain the comments of DHR as part of a state review process.** Please provide a completed form even in cases where project information is included in a separate document, such as an Environmental Impact Report. Environmental documents may be submitted as attachments to the form if they provide an important part of the project description.

A program specific review application form for cell tower projects is available on DHR's website along with several other attachments to the project review application relating to the rehabilitation and demolition of historic structures which are intended to streamline the process.

Before You Complete the Project Review Application Form

1. Determine if your project constitutes an undertaking that has the potential to impact historic properties, assuming such historic properties were present (for the definition of an undertaking, go to the Section 106 Regulations, Definitions section, 36 CFR 800.16, on the web at www.achp.gov/regs.html).
2. Determine the Area(s) of Potential Effect (APE) for the project. For the purposes of Section 106, the area of potential effect (APE) is defined as the entire geographical area in which changes may occur to historic properties if any are present. The APE for archaeological resources may be different than for architectural resources. The viewshed of historic properties often extends well beyond their boundaries and is often an important contributing element to their historic significance. Therefore, projects which alter the landscape drastically - large scale subdivisions, highway construction - or those which insert a large, intrusive structure into the landscape - cell towers, water towers - must take into account the surrounding viewshed when determining the APE. A field inspection of the project area will help to establish the APE. Establishing the APE is the responsibility of the federal agency in consultation with DHR. When acting on the behalf of a federal agency, the APE that is presented to DHR must be the APE that is approved by that agency. The boundaries of the APE should be clearly described and indicated on a U.S.G.S. quad map (original or clear copy). If there are two different APEs - one where ground disturbance is going to occur and one where viewshed is the only concern, for instance, these should be clearly indicated.
3. Gather information to identify the historic properties within or adjacent to the APE that may be affected by your project. Information on recorded historic properties is available in the DHR Archives, and this information **must** be collected prior to submitting project review application. The Archives are open to the public, and the only charges for use are 15 cents per page for copies. If it is not possible to visit the DHR Archives, the archivist will provide information on recorded properties for a fee (telephone the Archives at 804-367-2323, extension 125 for more information). Please be aware that survey in Virginia is far from complete, and the absence of historic resources in DHR records does not necessarily mean that no historic

properties are present. Information that should be considered in the identification process may also be available in other repositories, such as county planning offices and historical societies. On-site inspections are an essential component of the identification process. Photographs of the subject property and any nearby properties that may be over 50 years old should be provided with your project review application. Please attach the available information on recorded historic properties within the APE and documentation resulting from field inspection to the project review application form. If no historic properties are recorded in the APE, and if no potentially historic properties were observed during field inspection, note this on the application form.

4. Following the identification process, you should complete the project review application form in its entirety by referring to the following instructions. Attach or enclose the required additional information, and submit your application packet to DHR. The Department of Historic Resources will respond to your request within 30 days.

How to Complete the Project Review Application Form

I. GENERAL PROJECT INFORMATION

1. Indicate if the project, or any part thereof, has been previously reviewed by DHR and if so, insert the file number. If we know that a project has been previously reviewed, we can often avoid asking for duplicate information.
- 2-3. Complete this section in its entirety providing the name and location (independent city or town and county) of the project. If your project involves work on a specific building, please include the street address of the building.
4. Refer to the attached list of agencies and their abbreviations and indicate the abbreviation(s) for the federal and/or state agencies involved in the project (permitting, licensing, funding, etc.). If more than one agency is involved, one must be designated the lead agency for Section 106 compliance. If the appropriate agency is not included on the list, please write the full agency name in the space provided.
- 5-6. It is important that complete mailing addresses be provided for both the lead federal or state agency contact and the applicant.

II. PROJECT LOCATION AND DESCRIPTION

7. Indicate the name of the USGS quadrangle on which your project area is located. An original or clear photocopy of the 7.5 minute USGS topographic quadrangle, or a **clearly labeled** portion thereof, showing the exact boundaries of the project location, and the project's Area(s) of Potential Effect (APE) **must** be attached to this application. Do **not** reduce or enlarge the map. Topographic maps may be downloaded free of charge from Topozone© (www.topozone.com).
8. Indicate the acreage of the project area.
9. Indicate if an architectural or archaeological survey has been conducted as part of the identification process or in a different context by consulting DHR's Archives. Indicate the author, title, and date of the report and if a copy of it is on file at DHR. If a survey has been completed and a copy is not on file, a copy should be included with the application materials.
10. During the identification stage of the Section 106 process you should determine the presence/absence of structures 50 years old or older. Indicate if the Archives search revealed any historic properties in the APE and if the site inspection revealed any properties over 50 years of age within or adjacent to the project area which may or may not be recorded at DHR. The date of construction for structures is often indicated in county or state tax records. Photographs of all structures over 50 years of age must be included with the application materials.

MAIL COMPLETED FORM AND ATTACHMENTS TO:
Virginia Department of Historic Resources
Attention: Project Review
2801 Kensington Avenue, Richmond, VA 23221
www.dhr.virginia.gov

- 11-12. These questions are designed to help DHR determine if your project needs to be reviewed by an architectural historian or an archaeologist or both. If the answer to either of these questions is *yes*, a complete explanation is required in the Description.
13. Description. Attach a detailed description of the project area and the proposed undertaking, making sure to include the following information:
- a) Description of the existing land use. Include photographs of the project area.
 - b) Description of any recent modifications to the landscape. [Note: If the existing landscape appears to be markedly different from that shown on the attached quad map, please include information to that effect explaining what changes have occurred since the map was last updated.]
 - c) For projects involving the rehabilitation, alteration, or demolition of a structure over 50 years of age, a detailed description of the extent of the proposed alterations, along with photographs, architectural and engineering drawings, project specifications, and maps will be required.
 - d) Detailed project description that includes the precise location of all construction, destruction, and other proposed disturbance, the horizontal and vertical dimensions of all above and below ground construction, and the nature and extent of any previous disturbances – i.e. it is in a plowed field or disturbed VDOT right-of-way – within the APE.

Please Note: A complete project review application consists not only of the fully completed form, but also a completed Archives search, a USGS topographic map with the APE marked, a detailed project description, and all required photographs and project plans. A checklist is provided at the end of the application. Accurate and complete information will help in obtaining a timely response. If all required materials are not submitted, you will receive notification that your application is incomplete and the 30-day review period will not begin until all necessary materials are received.

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**COMMONLY USED FEDERAL AND STATE AGENCIES
AND ABBREVIATIONS**

Federal Agencies

Advisory Council on Historic Preservation	ACHP
Department of the Interior, Bureau of Land Management	BLM
Central Intelligence Agency	CIA
Department of Defense, Army Corps of Engineers	COE
Drug Enforcement Administration	DEA
Department of Defense	DOD
Department of Defense, Army	Army
Department of Defense, Navy	Navy
Department of Defense, Marines	Marines
Department of Defense, Air Force	Air Force
Department of the Interior	DOI
Department of Justice	DOJ
Department of Labor	DOL
Defense Security Service	DSS
Department of Education	ED
Department of Commerce, Economic Development Administration	EDA
Environmental Protection Agency	EPA
Department of Transportation, Federal Aviation Administration	FAA
Federal Bureau of Investigation	FBI
Federal Communications Commission	FCC
Federal Deposit Insurance Corporation	FDIC
Federal Emergency Management Agency	FEMA
Department of Energy, Federal Energy Regulatory Commission	FERC
Federal Highway Administration	FHWA
Federal Railroad Administration	FRA
Department of Transportation, Federal Transit Administration	FTA
Department of Housing and Urban Development	HUD
General Services Administration	GSA
Department of Health and Human Services	HHS
Interstate Commerce Commission	ICC
Library of Congress	LC
Metropolitan Washington Airports Authority	MWAA
National Aeronautics and Space Administration	NASA
National Capital Planning Commission	NCPC
National Endowment for the Humanities	NEH
National Imagery and Mapping Center	NIMA
Nuclear Regulatory Commission	NRC
Department of Commerce, National Oceanic and Atmospheric Administration	NOAA
Department of the Interior, National Park Service	NPS
Department of Agriculture, Natural Resources Conservation Service	NRCS

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Comptroller of the Currency	OCC
Department of the Interior, Office of Surface Mining	OSM
Department of Agriculture, Rural Development	RD
Rural Utilities Service	RUS
Small Business Administration	SBA
Smithsonian Institute	SI
Surface Transportation Board	STB
Technology Administration	TA
Tennessee Valley Authority	TVA
United States Coast Guard	USCG
United States Department of Agriculture	USDA
United States Department of Commerce	USDOC
United States Department of Energy	USDOE
Department of Agriculture, Forest Service	USFS
Department of the Interior, Fish and Wildlife Service	USFWS
United States Geological Survey	USGS
United States Postal Service	USPS
Department of Veterans Affairs	VA

State Agencies

Christopher Newport University	CNU
Central Virginia Community College	CVCC
College of William and Mary	CWM
Department of Criminal Justice Services	DCJS
Department of Conservation and Recreation	DCR
Department of Environmental Quality	DEQ
Department of Game and Inland Fisheries	DGIF
Department of General Services	DGS
Department of Housing and Community Development	DHCD
Department of Historic Resources	DHR
Department of Juvenile Justice	DJJ
Department of Mental Health, Mental Retardation and Substance Abuse Services	DMHMRSAS
Department of Mines, Minerals and Energy	DMME
Department of Motor Vehicles	DMV
Department of Accounts	DOA
Department of Corrections	DOC
Department of Education	DOE
Department of Forestry	DOF
Department of Veterans Affairs	DVA
Frontier Culture Museum of Virginia	FCM
Germanna Community College	GCC
Gunston Hall	GH
George Mason University	GMU
James Madison University	JMU

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John Tyler Community College	JTCC
Jamestown-Yorktown Foundation	JYF
Medical College of Virginia	MCV
North Carolina Department of Transportation	NCDOT
Norfolk State University	NSU
Old Dominion University	ODU
Piedmont Virginia Community College	PVCC
Radford University	RU
State Corporation Commission	SCC
Science Museum of Virginia	SMV
Tidewater Community College	TCC
Thomas Nelson Community College	TNCC
University of Mary Washington	UMW
University of Virginia	UVA
Virginia Community College System	VCCS
Virginia Commonwealth University	VCU
Department of Agriculture and Consumer Services	VDACS
Department of Health	VDH
Department of Transportation	VDOT
Virginia Employment Commission	VEC
Virginia Institute of Marine Science	VIMS
Virginia Museum of Fine Arts	VMFA
Virginia Military Institute	VMI
Virginia Museum of Natural History	VMNH
Virginia Outdoors Foundation	VOF
Virginia Port Authority	VPA
Virginia Polytechnic Institute and State University	VPISU
Virginia Resources Authority	VRA
Virginia School for the Deaf and Blind	VSDB
Library of Virginia	VSLA
Department of State Police	VSP
Virginia State University	VSU
Virginia Western Community College	VWCC
Wytheville Community College	WCC
West Virginia Department of Transportation	WVDOT

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Project Review Application Form

This application must be completed for all projects that will be federally funded, licensed, or permitted, or that are subject to state review. Please allow 30 days from receipt for the review of a project. All information must be completed before review of a project can begin and incomplete forms will be returned for completion.

I. GENERAL PROJECT INFORMATION

1. Has this project been previously reviewed by DHR? YES ___ NO DHR File # _____

2. Project Name Columbia Pike Transit Initiative

3. Project Location City of Alexandria Bailey's Crossroads Fairfax County
Pentagon City Arlington County
City Town County

4. Specify Federal and State agencies involved in project (providing funding, assistance, license or permit). Refer to the list of agencies and abbreviations in the instructions.

Lead Federal Agency FTA

Other Federal Agency _____

State Agency _____

5. Lead Agency Contact Information

Contact Person Melissa Barlow (FTA)

Mailing Address 1990 K Street NW, Suite 510, Washington D.C., 20006

Phone Number 202-219-3565 Fax Number _____

Email Address Melissa.Barlow@dot.gov

6. Applicant Contact Information

Contact Person John Dittmeier (WMATA)

Mailing Address 600 Fifth Street NW, Room 6F-16, Washington D.C., 20001

Phone Number 202-962-1027 Fax Number _____

Email Address jdittmeier@wmata.com

II. PROJECT LOCATION AND DESCRIPTION

7. USGS Quadrangle Name Alexandria, VA and Annandale, VA

8. Number of acres included in the project (see attached text)

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9. Have any architectural or archaeological surveys of the area been conducted? YES ___
NO X

If yes, list author, title, and date of report here. Indicate if a copy is on file at DHR.
See attached list with full itemization of previous studies around this study corridor. No detailed survey has been undertaken within this corridor.

10. Are any structures 50 years old or older within or adjacent to the project area? YES X
NO ___

If yes, give date(s) of construction and provide photographs.
See attached text.

11. Does the project involve the rehabilitation, alteration, removal, or demolition of any structure, building, designed site (e.g. park, cemetery), or district that is 50 years or older? If yes, this must be explained fully in the project description. YES X
NO ___

12. Does the project involve any ground disturbance (e.g. excavating for footings, installing sewer or water lines or utilities, grading roads, etc.)? If yes, this must be explained fully in the project description. YES X
NO ___

13. DESCRIPTION: Attach a complete description of the project. Refer to the instructions for the required information.

To the best of my knowledge, I have accurately described the proposed project and its likely impacts.

Signature of Applicant/Agent

Date

The following information must be attached to this form:

- x Completed DHR Archives search
- x USGS map with APE shown
- x Complete project description
- x Any required photographs and plans

<p>____ No historic properties affected ____ No adverse effect</p> <p>____ Additional information is needed in order to complete our review.</p> <p>____ We have previously reviewed this project. A copy of our correspondence is attached.</p> <p>Comments: _____</p> <p>_____</p> <p>_____</p> <p>Signature _____ Date _____</p> <p>Phone number _____ DHR File # _____</p> <p><i>This Space For Department Of Historic Resources Use Only</i></p>
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April 30, 2010

Ms. Christiana Briganti-Dunn, Project Manager
Virginia Department of Transportation, Northern Virginia District
14685 Avion Parkway
Chantilly, VA 20151-1104

Re: Vertical Clearance over Columbia Pike at Proposed Washington Boulevard Bridge

Dear Ms. Briganti-Dunn:

This letter confirms the vertical clearance of the proposed Washington Boulevard (VA 27) bridge over Columbia Pike (VA 244) to allow future streetcar operations along Columbia Pike. As discussed via teleconference on January 27, 2010, the required minimum vertical clearance from the street surface (and top of streetcar rail) to the bottom of bridge structure is 16'-8".

The Columbia Pike Transit Initiative is currently evaluating alternatives and completing environmental documentation for a major transit improvement—including a streetcar alternative—for service along the Columbia Pike corridor. Ongoing streetcar design uses the 2003 WMATA Tram/LRT Guideline Design Criteria for guidance on allowable vertical and horizontal clearances. Attachments to this letter (Section 3.3.3 and Figure 3-6a) summarize this guidance. Under bridges, the minimum clearance from top of rail to contact wire is 16'-0"; assuming single-wire overhead contact system (OCS), the absolute minimum dimension between contact wire and overhead structure is 8".

A minimum clearance of 16'-8" restricts the use of a two-wire OCS configuration, requiring use of a single-wire system that may need to attach directly to the bottom of the bridge structure. Such attachment would require electrical bonding and grounding of the bridge, and would be a factor in bridge inspection and maintenance. If attachment to the bridge structure is not possible, supports directly adjacent to the structure would be required.

Although a larger vertical clearance is desirable to allow flexibility in the design of the streetcar OCS, the Columbia Pike transit team understands that a 16'-8" minimum clearance would allow design and construction of the Washington Boulevard bridge to advance with minimal changes, by retaining the proposed bridge profile and interchange ramp configuration.

Please contact me at 202.962.1027 if you require further clarification or additional information.

Sincerely,

John Dittmeier
WMATA Project Manager, Columbia Pike Transit Initiative

Attachments

Attachment 1

WMATA Tram/LRT Guideline Design Criteria

fully worn wheels. The minimum vertical clearance envelope will be 2 inches, except in paved track areas between the rails where a drainage crown may extend not more than 0.75 inches above TOR, thus reducing the worst case vertical clearance to 1.25 inches.

The horizontal component of the 2 inch vertical clearance envelope will be defined by the points shown on Figure 3-3 as a function of curve radius plus the appropriate CT, MT, CW, and running clearance described above.

3.3.3 Vertical Clearances

The Tram/LRT system will be powered by an overhead contact system, which will consist of catenary wires supported by poles that may be located either along the wayside of the tracks or between dual track centers. Designated vertical clearances between the contact and messenger wires hanging above the track and any overhead obstructions must be maintained to allow efficient functioning of the vehicle pantograph.

The established vertical clearances take into account the vehicle clearance envelope based on the worst-case condition. Vertical clearances will be measured between the top of highest rail and an overhead obstruction.

Lateral clearances between catenary poles located along the wayside of the track and adjacent obstructions must be maintained to allow unobstructed access to each pole for maintenance and repairs of the overhead contact wires. The following minimum clearances are established:

- Desirable clearance between top of highest rail and soffit of overhead structure: 22 feet 0 inches
- Minimum clearance (pantograph locked down): 13 feet 6 inches
- Minimum vertical and lateral clearance between catenary pole and adjacent tree line or vegetation: 10 feet 0 inches

Figure 3-6 includes these clearances and more specific overhead electrical wire clearance requirements.

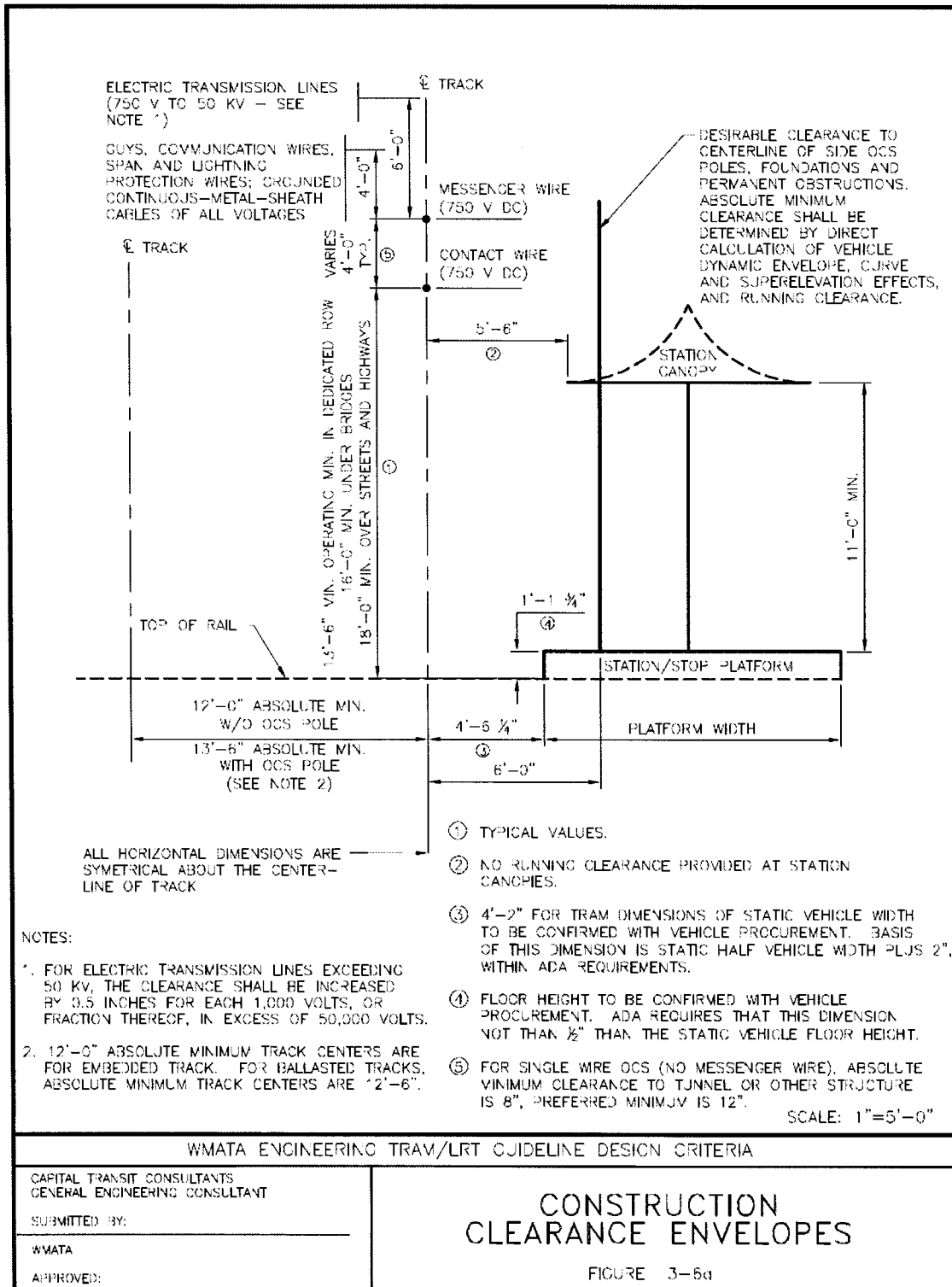
3.3.4 Track Centers

The minimum allowable distance between two mainline Tram/LRT tracks, with equally applied superelevation and no catenary poles between them, will be determined by the following relationship:

$$(Eq. 3.3.4-1) \quad W_{TC} = DW_T(\text{Track 1}) - DW_A(\text{Track 2}) - (2CT) - TCM - RC$$

- W_{TC} = Minimum track center (feet)
- DW_T = Dynamic half-width toward curve center (feet)
- DW_A = Dynamic half-width away from curve center (feet)
- RC = Running clearance (feet)
- CT = Fixed facility construction tolerances (feet)
- TCM = Track construction and maintenance tolerances (feet)

Attachment 2





December 11, 2009

Mr. Dana Kauffman, Director
College Government Affairs and Community Relations
Northern Virginia Community College
4001 Wakefield Chapel Road
Annandale, Virginia 22003-3796

Re: Columbia Pike Transit Initiative

Dear Mr. Kauffman:

Thank you for your follow-up letter to the initial project initiation letter sent in September 2009. The Columbia Pike Transit Initiative project team reviewed your letter and is excited about your continued interest in the Columbia Pike Transit Initiative project.

Arlington and Fairfax County staff would like to meet with you and representatives from Northern Virginia Community College (NVCC) to further discuss the possibilities of including a station site and possible maintenance facility on your Alexandria campus. At the direction of the project management team, WMATA is preparing to address in greater detail some of the questions raised in your September 30, 2009 letter. The project team looks forward to working together with NVCC on making the Columbia Pike Transit Initiative a reality.

Until we can arrange for a meeting, should you have any further questions or concerns, please contact me at (202) 962-1114, or rmcelhennysmith@wmata.com. Please send your written input to:

Robin McElhenny-Smith, Deputy Project Manager
Washington Metropolitan Area Transit Authority
600 5th Street, NW
Room 5B-26
Washington, DC 20001

Sincerely,

Robin McElhenny-Smith
WMATA Deputy Project Manager, Columbia Pike Transit Initiative

Cc: Stephen Del Giudice, Arlington County
Leonard Wolfenstein, Fairfax County

From: Dittmeier, John [mailto:JDittmeier@wmata.com]
Sent: Tuesday, November 03, 2009 11:57 AM
To: Amy (DGIF) Ewing; Ashe, James A.; McElhenny-Smith, Robin; Mumford, Jason; Anderson, Susan
Cc: Ellie (DEQ) Irons
Subject: RE: ESSLog# 30118_Columbia Pike Transit Initiative_NEPA Documentation

Amy Ewing:

Thank you for your e-mail response below, which the project team will officially enter into agency correspondence. The environmental analyses will address the issues that your message identifies.

Team:

For your action. Please update agency list for both individuals, if not already included in the list.

John M. Dittmeier
Assistant Project Manager
WMATA Office of Major Capital Projects (MCAP)
WMATA Office of Joint Development and Adjacent Construction (JDAC)
3500 Pennsy Drive, Room C-104A
Landover, MD 20785

301-618-1016 Line
202-302-4127 Cell
301-583-3006 Fax
jdittmeier@wmata.com

Visit the JDAC web page at

http://www.wmata.com/business/joint_development_opportunities/adjacent_construction_information.cfm

From: Amy (DGIF) Ewing [mailto:Amy.Ewing@dgif.virginia.gov]
Sent: Tuesday, November 03, 2009 10:55 AM
To: Dittmeier, John
Cc: Ellie (DEQ) Irons
Subject: ESSLog# 30118_Columbia Pike Transit Initiative_NEPA Documentation

We have reviewed your request for information for inclusion in NEPA documentation being prepared for the subject project. The project proposes transit improvements in Arlington and Fairfax counties, VA.

According to our records, Fourmile Run and the Potomac River are located within the project vicinity and have been designated Anadromous Fish Use Areas. We recommend that any NEPA documentation include a discussion about proposed impacts upon these waters and how adverse effects upon the anadromous fish resources known from these waters will be avoided, minimized or mitigated. We typically make the following protective recommendations regarding anadromous fish use areas and general wildlife resources:

We recommend that all instream work in Fourmile Run, the Potomac River, and/or their tributaries adhere to a time of year restriction from February 15 through June 30 of any year. In addition, we recommend conducting any in-stream activities during low or no-flow conditions, using non-erodible cofferdams or turbidity curtains to isolate the construction area, blocking no more than 50% of the streamflow at any given time, stockpiling excavated material in a manner that prevents reentry into the stream, restoring original streambed and streambank contours, revegetating barren areas with native vegetation, and

implementing strict erosion and sediment control measures. Due to future maintenance costs associated with culverts, and the loss of riparian and aquatic habitat, we prefer stream crossings to be constructed via clear-span bridges. However, if this is not possible, we recommend countersinking any culverts below the streambed at least 6 inches, or the use of bottomless culverts, to allow passage of aquatic organisms. We also recommend the installation of floodplain culverts to carry bankfull discharges.

To minimize overall impacts to wildlife and our natural resources, we offer the following comments about development activities: We recommend that the applicant avoid and minimize impacts to undisturbed forest, wetlands, and streams to the fullest extent practicable. Avoidance and minimization of impact may include relocating stream channels as opposed to filling or channelizing them. We recommend maintaining undisturbed wooded buffers of at least 100 feet in width around all on-site wetlands and on both sides of all perennial and intermittent streams. We recommend that the stormwater controls for this project be designed to replicate and maintain the hydrographic condition of the site prior to the change in landscape. This should include, but not be limited to, utilizing bioretention areas, and minimizing the use of curb and gutter in favor of grassed swales. Bioretention areas (also called rain gardens) and grass swales are components of Low Impact Development (LID). They are designed to capture stormwater runoff as close to the source as possible and allow it to slowly infiltrate into the surrounding soil. They benefit natural resources by filtering pollutants and decreasing downstream runoff volumes.

Please contact us if you need further information. Thank you.

Amy

Amy M. Ewing
Environmental Services Biologist
Virginia Dept. of Game and Inland Fisheries
4010 West Broad Street
Richmond, VA 23230
804-367-2211
amy.ewing@dgif.virginia.gov

From: Ewing, Amy (DGIF) [Amy.Ewing@dgif.virginia.gov]
Sent: Tuesday, November 03, 2009 10:55 AM
To: Dittmeier, John
Cc: Irons, Ellie (DEQ)
Subject: ESSLog# 30118_Columbia Pike Transit Initiative_NEPA Documentation

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To minimize overall impacts to wildlife and our natural resources, we offer the following comments about development activities: We recommend that the applicant avoid and minimize impacts to undisturbed forest, wetlands, and streams to the fullest extent practicable. Avoidance and minimization of impact may include relocating stream channels as opposed to filling or channelizing them. We recommend maintaining undisturbed wooded buffers of at least 100 feet in width around all on-site wetlands and on both sides of all perennial and intermittent streams. We recommend that the stormwater controls for this project be designed to replicate and maintain the hydrographic condition of the site prior to the change in landscape. This should include, but not be limited to, utilizing bioretention areas, and minimizing the use of curb and gutter in favor of grassed swales. Bioretention areas (also called rain gardens) and grass swales are components of Low Impact Development (LID). They are designed to capture stormwater runoff as close to the source as possible and allow it to slowly infiltrate into the surrounding soil. They benefit natural resources by filtering pollutants and decreasing downstream runoff volumes.

Please contact us if you need further information. Thank you.

Amy

Amy M. Ewing
Environmental Services Biologist
Virginia Dept. of Game and Inland Fisheries
4010 West Broad Street
Richmond, VA 23230
804-367-2211
amy.ewing@dgif.virginia.gov



COMMONWEALTH of VIRGINIA

DAVID S. EKERN, P.E.
COMMISSIONER

DEPARTMENT OF TRANSPORTATION

14685 Avion Parkway
Chantilly, VA 20151
(703) 383-VDOT (8368)

October 28, 2009

Ms. Robin McElhenny-Smith
Deputy Project Manager
Washington Metropolitan Area Transit Authority
600 5th Street, N.W., Room 5B-26
Washington, DC 20001

Dear Ms. McElhenny-Smith:

Thank you for the opportunity to participate in the NEPA study for the Columbia Pike Transit Initiative Project. Columbia Pike is a busy thoroughfare in Northern Virginia and will be well served by enhanced transit services in the corridor.

Valerie Pardo of my staff is being designated as our Virginia Department of Transportation (VDOT) representative on this effort, as she serves as our liaison for transit studies in Northern Virginia. Ms. Pardo can be contacted at Valerie.Pardo@VDOT.Virginia.gov or (703) 383-2227.

Sincerely,

A handwritten signature in black ink that reads "Morteza Salehi".

Morteza Salehi
District Administrator

Copy: Jo Anne Sorenson
Valerie Pardo



COMMONWEALTH of VIRGINIA
DEPARTMENT OF CONSERVATION AND RECREATION

203 Governor Street
Richmond, Virginia 23219-2010
(804) 786-6124

October 16, 2009

Robin McElhenny-Smith
WMATA
600 5th Street, NW
Room 5B-26
Washington DC 20001

RE Columbia Pike Transit Initiative, Fairfax and Arlington CO

Dear Ms. McElhenny-Smith:

The Department of Conservation and Recreation's Division of Natural Heritage (DCR) has searched its Biotics Data System for occurrences of natural heritage resources from the area outlined on the submitted map. Natural heritage resources are defined as the habitat of rare, threatened, or endangered plant and animal species, unique or exemplary natural communities, and significant geologic formations.

Biotics documents the presence of natural heritage resources in the project area. However, due to the scope of the activity and the distance to the resources, we do not anticipate that this project will adversely impact these natural heritage resources.

Under a Memorandum of Agreement established between the Virginia Department of Agriculture and Consumer Services (VDACS) and DCR represents VDACS in comments regarding potential impacts on state-listed threatened and endangered plant and insect species. The current activity will not affect any documented state-listed plants or insects.

In addition, our files do not indicate the presence of any State Natural Area Preserves under DCR's jurisdiction in the project vicinity.

New and updated information is continually added to Biotics. Please contact DCR for an update on this natural heritage information if a significant amount of time passes before it is utilized.

The Virginia Department of Game and Inland Fisheries maintains a database of wildlife locations, including threatened and endangered species, trout streams, and anadromous fish waters that may contain information not documented in this letter. Their database may be accessed from <http://vafwis.org/fwis/> or contact Shirl Dressler at (804) 367-6913.

Regarding alternative transportation, the distance between the Skyline area in Fairfax County and Pentagon City in Arlington County, the proposed transit initiative is ideal for alternative modes of transportation such as walking and bicycling. The alternatives for this transit initiative should

accommodate bicycle and pedestrian traffic. The feasibility of connecting a trail from Fairfax to Pentagon City via the Columbia Pike Corridor should be explored and considered as part of this project. In addition, the proposed Columbia Pike Transit corridor is near a prospective Potomac River blueway as well as within ½ mile of the Potomac Historic National Scenic Trail and the George Washington National Memorial Parkway. The proximity of these existing and proposed alternative transportation resources creates a priority for making connections to the Columbia Pike Transit Initiative, thereby connection to other areas in the region.

Lastly, in Fairfax County and Arlington County, the areas protected by the *Chesapeake Bay Preservation Act*, as locally implemented, require conformance with performance criteria. These areas include Resource Protection Areas (RPAs) and Resource Management Areas (RMAs) as designated by the local government. RPAs include tidal wetlands, certain non-tidal wetlands and tidal shores. RPAs also include a 100-foot vegetated buffer area located adjacent to and landward of these features and along both sides of any water body with perennial flow. All areas of Fairfax County not included in the RPA are designated as RMAs. In Arlington County, the RPA buffer also includes intermittent streams and steep slopes. All areas of Arlington County not included in the RPA are designated as RMAs.

Railroads, public roads and their appurtenant structures are conditionally exempt from the *Chesapeake Bay Preservation Area Designation and Management Regulations* provided they are constructed in accordance with (i) regulations promulgated pursuant to the Erosion and Sediment Control Law (§ 10.1-560 et seq. of the Code of Virginia) and the Stormwater Management Act (§ 10.1-603.1 et seq. of the Code of Virginia), (ii) an erosion and sediment control plan and a stormwater management plan approved by the Virginia Department of Conservation and Recreation, or (iii) local water quality protection criteria at least as stringent as the above state requirements. The exemption of public roads is further conditioned on the following:

Optimization of the road alignment and design, consistent with other applicable requirements, to prevent or otherwise minimize (i) encroachment in the Resource Protection Areas and (ii) adverse effects on water quality; and

Local governments may choose to exempt (i) all public roads as defined in 9 VAC 10-20-40 or (ii) only those public roads constructed by the Virginia Department of Transportation.

Pursuant to the Coastal Zone Management Act of 1972, as amended, Federal activities affecting Virginia's coastal resources or coastal uses must be consistent with the Virginia Coastal Resources Management Program (VCRMP)(see section 307(c)(1) of the Act and the *Federal Consistency Regulations*, 15 CFR Part 930, sub-part C). The 1998 Federal Agencies' Chesapeake Ecosystem Unified Plan requires the signatories, including the Federal Transit Administration, to fully cooperate with local and state governments in carrying out voluntary and mandatory actions to comply with the management of stormwater. The agencies also committed to encouraging construction design that a) minimizes natural area loss on new and rehabilitated federal facilities; b) adopts low impact development and best management technologies for storm water, sediment and erosion control, and reduces impervious surfaces; and c) considers the Conservation Landscaping and Bay-Scapes Guide for Federal Land Managers. In addition, the Chesapeake 2000 Agreement committed the government agencies to a number of sound land use and stormwater quality controls. The signatories additionally committed the agencies to lead by example with respect to controlling nutrient, sediment and chemical contaminant runoff from government properties. In December 2001, the Executive Council of the Chesapeake Bay Program issued Directive No. 01-1, Managing Storm Water on State, Federal and District-owned Lands and Facilities, which includes specific commitments for agencies to lead by example with respect to stormwater control.

Provided adherence to the above requirements, the project would be consistent with the *Chesapeake Bay Preservation Act* and *Regulations*.

The remaining DCR divisions have no comments regarding the scope of this project. Thank you for the opportunity to comment.

Sincerely,

A handwritten signature in cursive script that reads "Robert S. Munson".

Robert S. Munson
Planning Bureau Manager
DCR-DPRR



COMMONWEALTH of VIRGINIA

L. Preston Bryant, Jr.
Secretary of Natural Resources

Department of Historic Resources

Kathleen S. Kilpatrick
Director

7 October 2009 2801 Kensington Avenue, Richmond, Virginia 23221-0311

Tel: (804) 367-2323
Fax: (804) 367-2391
TDD: (804) 367-2386
www.dhr.virginia.gov

Ms Robin McElhlenny-Smith
Washington Metropolitan Area Transit Authority
600 Fifth Street, NW, Room 5B-26
Washington, DC 20001

RE: Environmental document for proposed transit improvements
Arlington and Fairfax Counties
DHR File No. 2009-1506

Dear Ms McElhlenny-Smith:

The Department of Historic Resources (DHR) has received notification that the Washington Metropolitan Area Transit Authority (WMATA), on behalf of the counties of Arlington and Fairfax, are preparing an environmental document for proposed transit improvements collectively known as the Columbia Pike Transit Initiative. This initiative would extend along Columbia Pike (Route 244) from the Skyline complex in Fairfax County to Pentagon City in Arlington County. The undertaking has the potential for federal funding from the Federal Transit Authority (FTA), therefore is subject to the review of DHR pursuant to Section 106 of the National Historic Preservation Act, as amended, and its implementing regulation 36 CFR Part 800.

We believe that the undertaking has the potential to affect historic properties. We request that you reference our website at http://www.dhr.virginia.gov/review/section_106.htm for guidance on what information the DHR needs in order to conduct our review. We look forward to working with you in the future on this undertaking.

If you have any questions about our comments, please contact me at (804) 367-2323, Ext. 114.

Sincerely,

Marc Holma, Architectural Historian
Office of Review and Compliance

C: Ms Linda Blank, Preservation Planner, Fairfax County
Mr. Michael Leventhal, Preservation Planner, Arlington County

Administrative Services
10 Courthouse Avenue
Petersburg, VA 23803
Tel: (804) 862-6416
Fax: (804) 862-6196

Capital Region Office
2801 Kensington Ave.
Richmond, VA 23221
Tel: (804) 367-2323
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Tidewater Region Office
14415 Old Courthouse Way, 2nd Floor
Newport News, VA 23608
Tel: (757) 886-2807
Fax: (757) 886-2808

Roanoke Region Office
1030 Penmar Ave., SE
Roanoke, VA 24013
Tel: (540) 857-7585
Fax: (540) 857-7588

Northern Region Office
5357 Main Street
PO Box 519
Stephens City, VA 22655
Tel: (540) 868-7029
Fax: (540) 868-7033



COMMONWEALTH of VIRGINIA

DEPARTMENT OF ENVIRONMENTAL QUALITY

Street address: 629 East Main Street, Richmond, Virginia 23219

Mailing address: P.O. Box 1105, Richmond, Virginia 23218

TDD (804) 698-4021

www.deq.virginia.gov

L. Preston Bryant, Jr.
Secretary of Natural Resources

David K. Paylor
Director

(804) 698-4020
1-800-592-5482

October 1, 2009

Ms. Robin McElhenny-Smith
Washington Metropolitan area Transit Authority
600 5th Street, NW
Room 5B-26
Washington, DC 20001

RE: Proposed Columbia Pike Transit Initiative, request for scoping comments for the preparation of a National Environmental Policy Act document

Dear Ms. McElhenny-Smith:

This is in response to your letter announcing the preparation of a National Environmental Policy Act (NEPA) document for the proposed Columbia Pike transit Initiative in Arlington and Fairfax Counties, and soliciting comments on the scope of the document.

PROJECT DESCRIPTION

According to your letter, the Washington Metropolitan Area Transit Authority (WMATA), on behalf of Arlington and Fairfax Counties and in cooperation with the Federal Transit Administration (FTA), is preparing an environmental document in accordance with NEPA for proposed transit improvements along Columbia Pike in Arlington and Fairfax Counties. The document will evaluate the alternatives of no build, enhanced bus, and streetcar. As proposed, the transit improvements are expected to have the following features:

- Transit would generally operate in shared traffic lanes within existing streets.
- Stations/stops with improved shelters, passenger amenities, and real-time information.
- Fare pre-payment and integration with WMATA's SmartTrip system.
- Operations: six-minute all-day service supplemented by transit bus during peak hours.
- One primary vehicle storage and maintenance facility at the western end of the corridor.

ENVIRONMENTAL REVIEW

The roles of the Virginia Department of Environmental Quality (DEQ) in relation to the project under consideration are as follows. First, DEQ's Office of Environmental Impact Review (OEIR) will coordinate Virginia's review of any environmental documents prepared pursuant to NEPA and comment to WMATA on behalf of the Commonwealth. According to WMATA (Ashe/Fisher, September 30, 2009), the project proponents intend to seek federal funding for the proposal. Should federal funding be procured or federal permits, license or approval are required, a similar review process will be conducted for the review of a Federal Consistency Certification (FCC) document that would be required pursuant to the Coastal Zone Management Act (CZMA).

FEDERAL CONSISTENCY UNDER THE COASTAL ZONE MANAGEMENT ACT

Pursuant to the Coastal Zone Management Act of 1972, as amended, federal financial assistance activities affecting Virginia's coastal resources or coastal uses must be consistent with the Virginia Coastal Resources Management Program (VCP) (see section 307(c)(1) of the Act and the *Federal Consistency Regulations*, 15 CFR Part 930, sub-part D and sub-part F). WMATA must provide a consistency certification which involves an analysis of the activities in light of the enforceable policies of the VCP (first enclosure), and a commitment to comply with the enforceable policies. In addition, we invite your attention to the advisory policies of the VCP (second enclosure). The FCC may be provided as part of the NEPA documentation or independently, depending on your agency's preference; we recommend, in the interests of efficiency for all concerned, that it be provided together with the NEPA document. For further information see Virginia's *Federal Consistency Information Package* at <http://www.deq.virginia.gov/eir/federal.html>.

PROJECT SCOPING

While this Office does not participate in scoping efforts beyond the advice given herein, other agencies are free to provide scoping comments concerning the preparation of the NEPA document for the proposed project. Therefore, we are sharing your letter with selected state and local Virginia agencies, which are likely to include the following (note: starred (*) agencies administer one or more of the Enforceable Policies of the Virginia Coastal Resources Management Program (also called the Virginia Coastal Zone Management Program)):

- Department of Environmental Quality:
 - Office of Environmental Impact Review
 - Northern Regional Office*
 - Air Division*
 - Waste Division
- Department of Game and Inland Fisheries*
- Department of Conservation and Recreation:

Ms. Robin McElhenny-Smith
Pike Transit Initiative

- Division of Soil and Water Conservation*
- Division of Chesapeake Bay Local Assistance*
- Division of Planning and Recreation Resources
- Marine Resources Commission*
- Department of Agriculture and Consumer Services
- Department of Health
- Department of Mines, Minerals, and Energy
- Department of Historic Resources
- Northern Virginia Regional Commission.

In order to ensure an effective coordinated review of the EA and FCC, we will require 18 copies of the document when it is published. The submission may include 4 hard copies and 14 CDs (or 4 hard copies and an electronic copy available for download at a WMATA web or ftp site). The document should include a U.S. Geological Survey topographic map as part of its information. We recommend, as well, that project details unfamiliar to people outside WMATA be adequately described.

If you have questions about the environmental review process or the federal consistency review process, please feel free to call me at (804) 698-4325 or John Fisher of this Office at (804) 698-4339.

I hope this information is helpful to you.

Sincerely,



Ellie L. Irons, Manager
Office of Environmental Impact Review

Attachments

Ec: David Hartshorn, DEQ-NRO
Kotur S. Narasimhan, DEQ-Air
Paul Kohler, DEQ-Waste
Amy Ewing, DGIF
Robbie Rhur, DCR
Tony Watkinson, VMRC
Barry Matthews, VDH
Todd Groh, VDF
Matt Heller, DMME
Roger Kirchen, DHR
Keith Tignor, VDACS
G. Mark Gibb, Northern Virginia Regional Commission

Correspondence with Local Agencies and Officials



October 6, 2011

Department of Parks, Recreation & Cultural Resources
Attn: David Goodman
2100 Clarendon Boulevard, Suite 900
Arlington, VA 22201

Re: Columbia Pike Transit Initiative, Alternatives Analysis/Environmental Assessment

Dear Sir:

On behalf of Arlington County and Fairfax County, the Washington Metropolitan Area Transit Authority (WMATA) is conducting an Alternatives Analysis/Environmental Assessment (AA/EA) of proposed transit improvements along the Columbia Pike corridor. As WMATA's consultant, AECOM is making contact with the Arlington County Department of Parks, Recreation & Cultural Resources (PRCR) to provide information about the proposed project and to seek input from PRCR regarding potential impacts to resources under your jurisdiction.

The project study area, as shown on the enclosed USGS quadrangles (**Figure 1**) extends approximately five miles, mostly along the highly urban corridor of Columbia Pike, between Skyline and Pentagon City. The current Streetcar Build Alternative (comprised of the alignment itself, stop locations, traction power substations, an operations and maintenance facility, and transit center) are identified in the enclosed **Figure 2**. The proposed transit improvement would stay mostly within existing transportation rights-of-way that are currently paved or covered by concrete.

The project would cross Four Mile Run and the Four Mile Run Park/Trail via the existing Columbia Pike roadway bridge. However, recent engineering analysis undertaken by the project team has identified the need for the following superstructure and substructure improvements to the bridge in order to accommodate the Streetcar Build Alternative:

1. Reconstruction of the outer lanes, sidewalk and parapet of the Four Mile Run bridge deck. (See **Figure 3** and **Figure 4** for sections of the existing and proposed bridge deck.)
2. Installation of new structural light-weight concrete box beams to support a new streetcar concrete deck with embedded rails. (**Figure 4**)
3. Addition of concrete in-fill walls along existing bridge piers and the expansion of the footings of existing bridge piers to provide sufficient support for deck improvements. (See **Figure 5** and **Figure 6** for sections of the existing and proposed pier elevation.)

It is anticipated that construction time to complete superstructure improvements would be approximately six to eight months. Construction time to complete substructure improvements would be approximately four to six months. In order to construct the in-fill walls and expand the footings of the existing piers, a cofferdam would be built and used to dewater the construction area. The project team recognizes that construction activities may have temporary effects on the Four Mile Run



Park/Trail. See the enclosed **Figure 7** for images showing existing conditions at Four Mile Run Park/Trail under the Four Mile Run bridge.

The project team respectfully requests input, based on the information presented above, regarding the Columbia Pike Transit Initiative Project's potential impacts to Four Mile Run Park/Trail. A similar letter is being submitted to the Northern Virginia Regional Park Authority, owner of the W&OD Trail, which crosses Columbia Pike at-grade and is also in the vicinity of the Four Mile Run bridge. Additional letters are being provided to the Virginia Department of Conservation and Recreation, the Virginia Department of Game and Inland Fisheries, and the Virginia Department of Environmental Quality regarding the natural resources associated with Four Mile Run. Should you have any questions or comments, please contact me at 703-340-3023 or Susan.Anderson@aecom.com for more information.

Sincerely,

A handwritten signature in blue ink that reads "Susan T. Anderson".

Susan T. Anderson, AICP
Pike Transit Initiative, NEPA Task Leader

Enclosures: Figure 1: Proposed Alignment and Study Area (USGS Quadrangles)
 Figure 2: Proposed Alignment and Study Area
 Figure 3: Existing Typical Section - Four Mile Run Bridge
 Figure 4: Proposed Typical Section - Four Mile Run Bridge
 Figure 5: Existing Pier Elevation
 Figure 6: Proposed Pier Elevation
 Figure 7: Four Mile Run Images

cc: WMATA – J. Dittmeier
 Arlington County – S. Del Giudice

Figure 1: Proposed Alignment and Study Area (USGS Quadrangles)

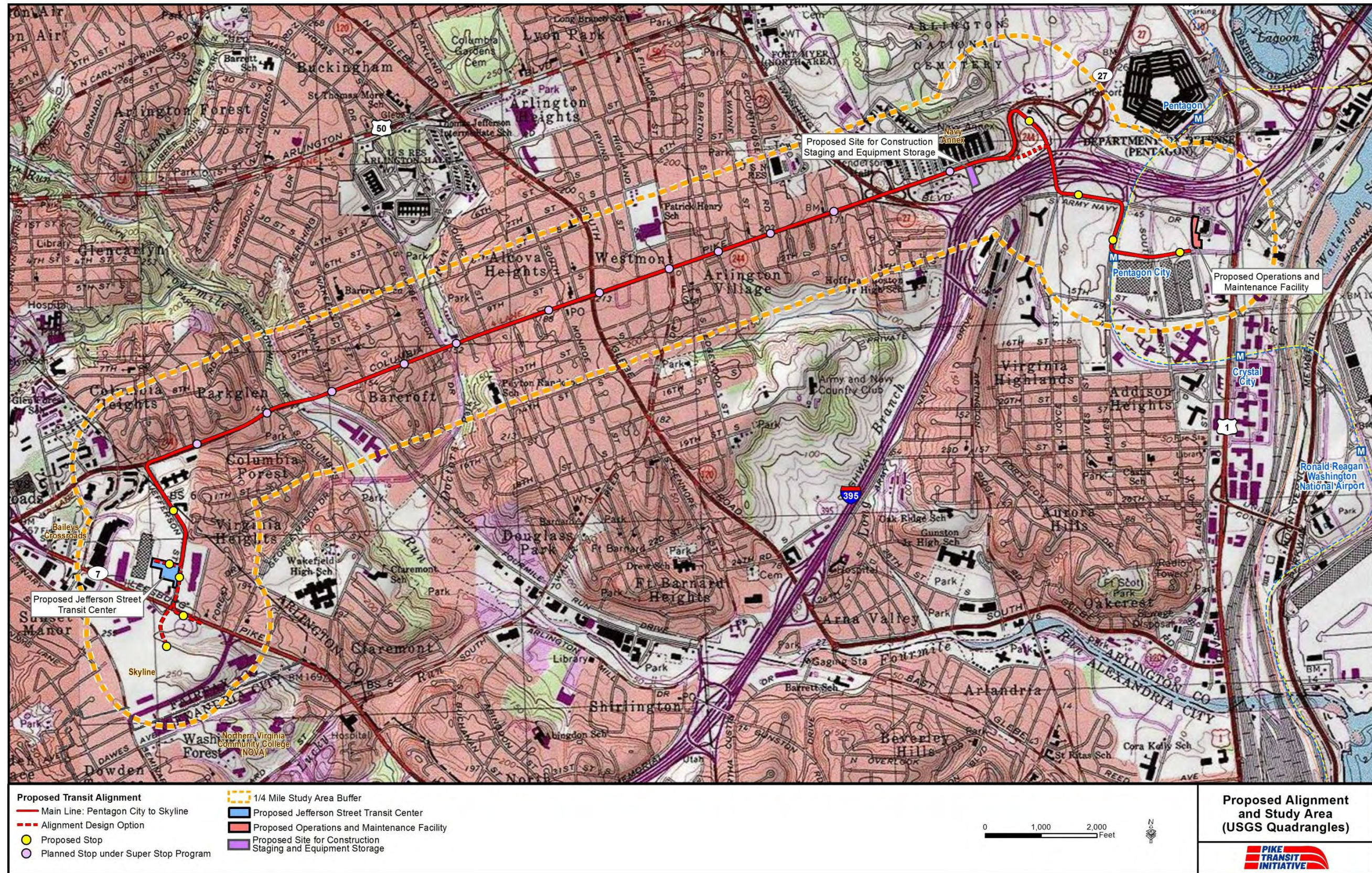


Figure 2: Proposed Alignment and Study Area

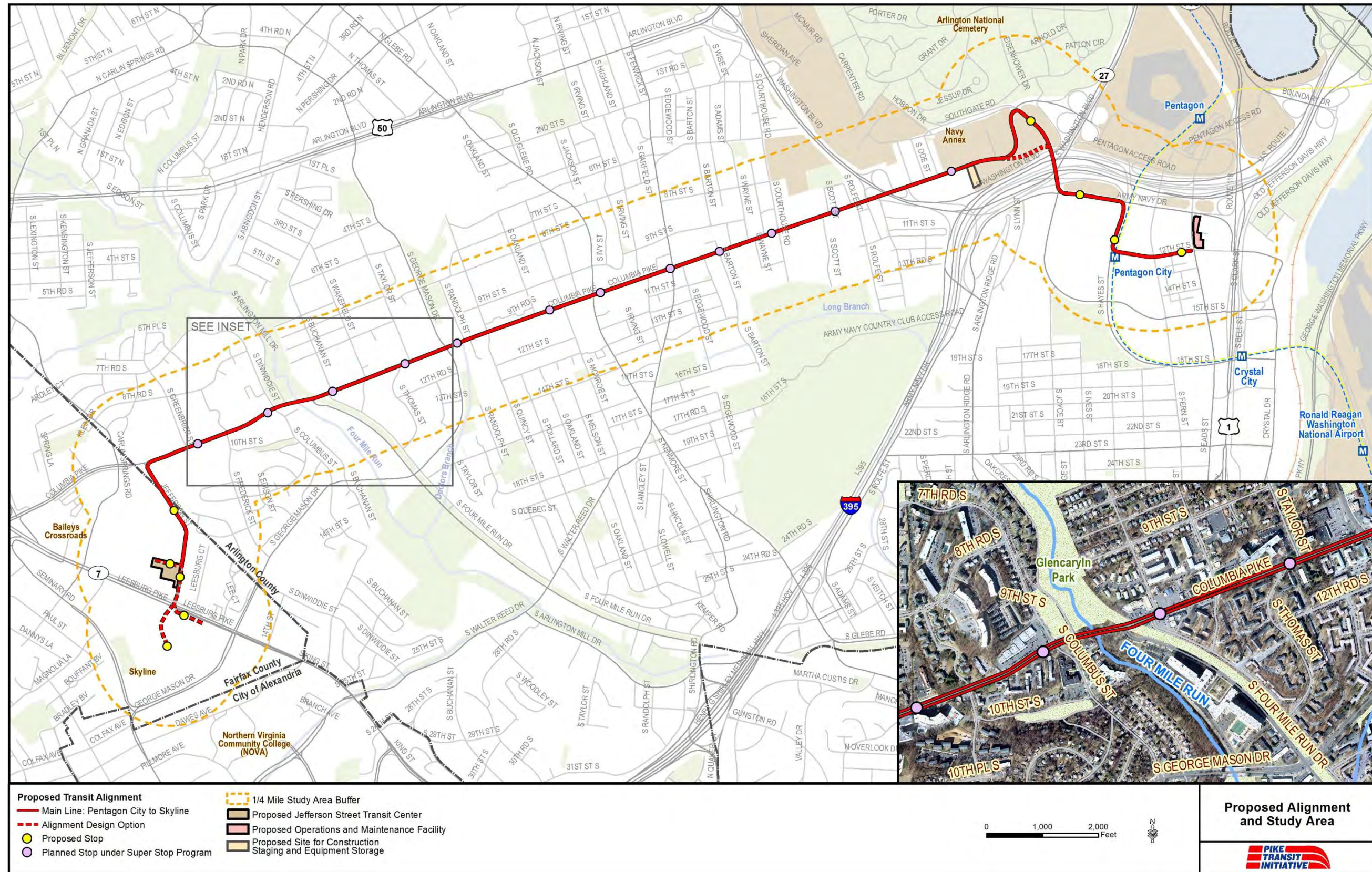
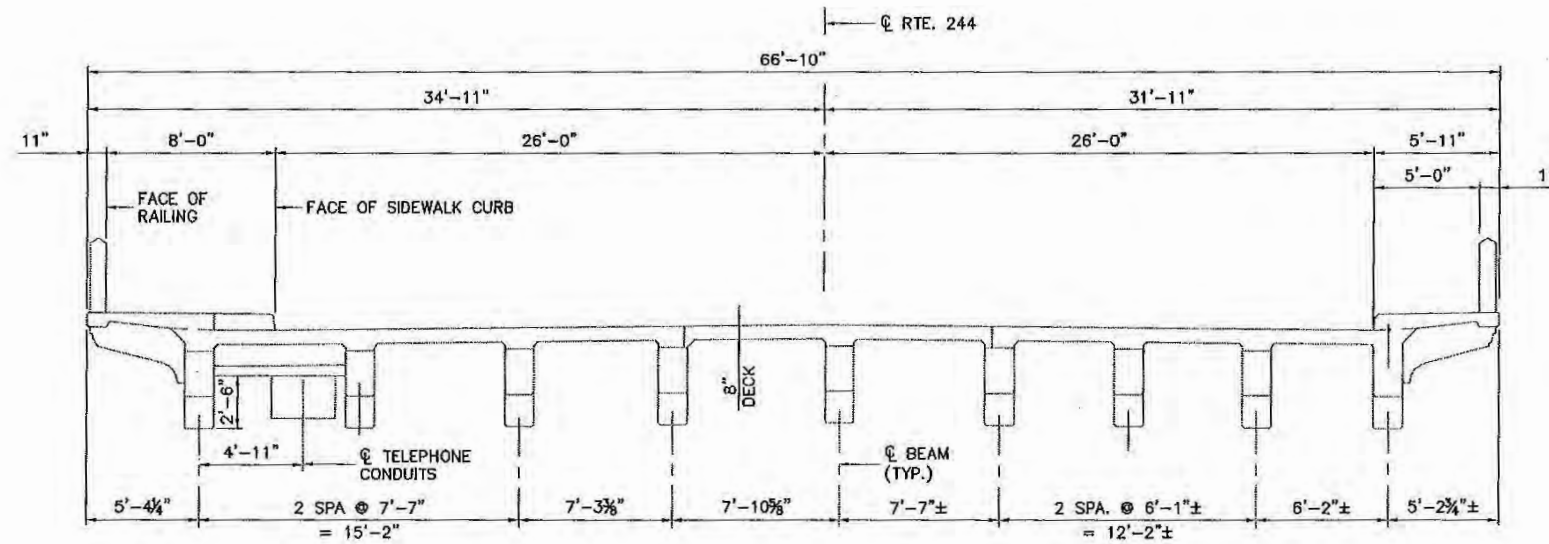


Figure 3: Existing Typical Section - Four Mile Run Bridge

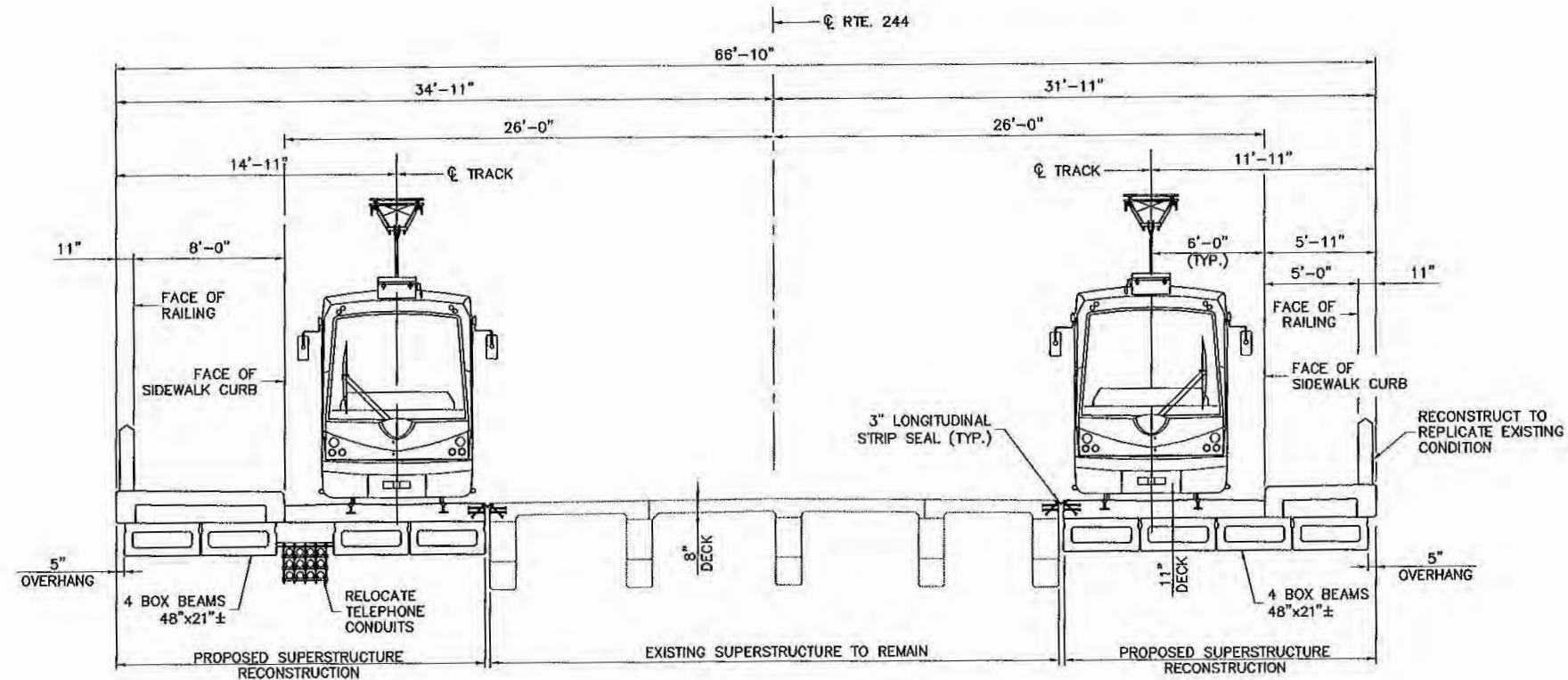


EXISTING TYPICAL SECTION
FOUR MILE RUN BRIDGE

Jul. 15 2011 11:39 am C:\60020294\Structure\Presentation\Exhibits\Columbia Pike_ExistSec.dwg By: jccintoc

	<p>Columbia Pike Streetcar Preliminary Streetcar Alignment East Option Existing Bridge Section Attachment 1</p>	Scale:
		Date: 08-XX-2010
		Dwg. No.
		Sheet No: 1 of 5

Figure 4: Proposed Typical Section - Four Mile Run Bridge

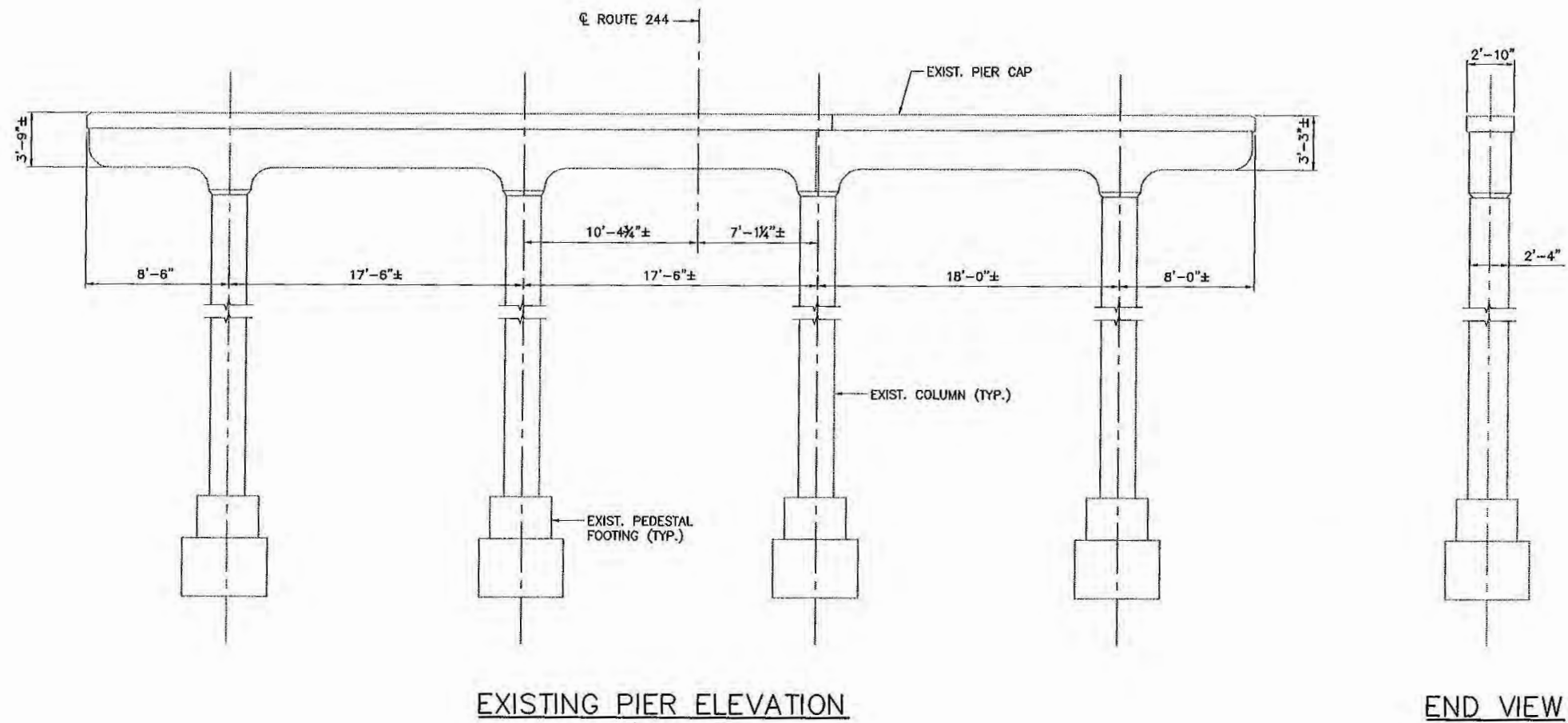


PROPOSED TYPICAL SECTION
FOUR MILE RUN BRIDGE

Jul. 15 2011 11:40 am G:\60020294\Structure\Presentation\Exhibits\columbia pike_propsec.dwg By: jacinoc

	<p>Columbia Pike Streetcar Preliminary Streetcar Alignment East Option Proposed Bridge Section Attachment 3</p>	<p>Scale:</p>
		<p>Date: 08-XX-2010</p>
		<p>Dwg. No.</p>
		<p>Sheet No: 3 of 5</p>

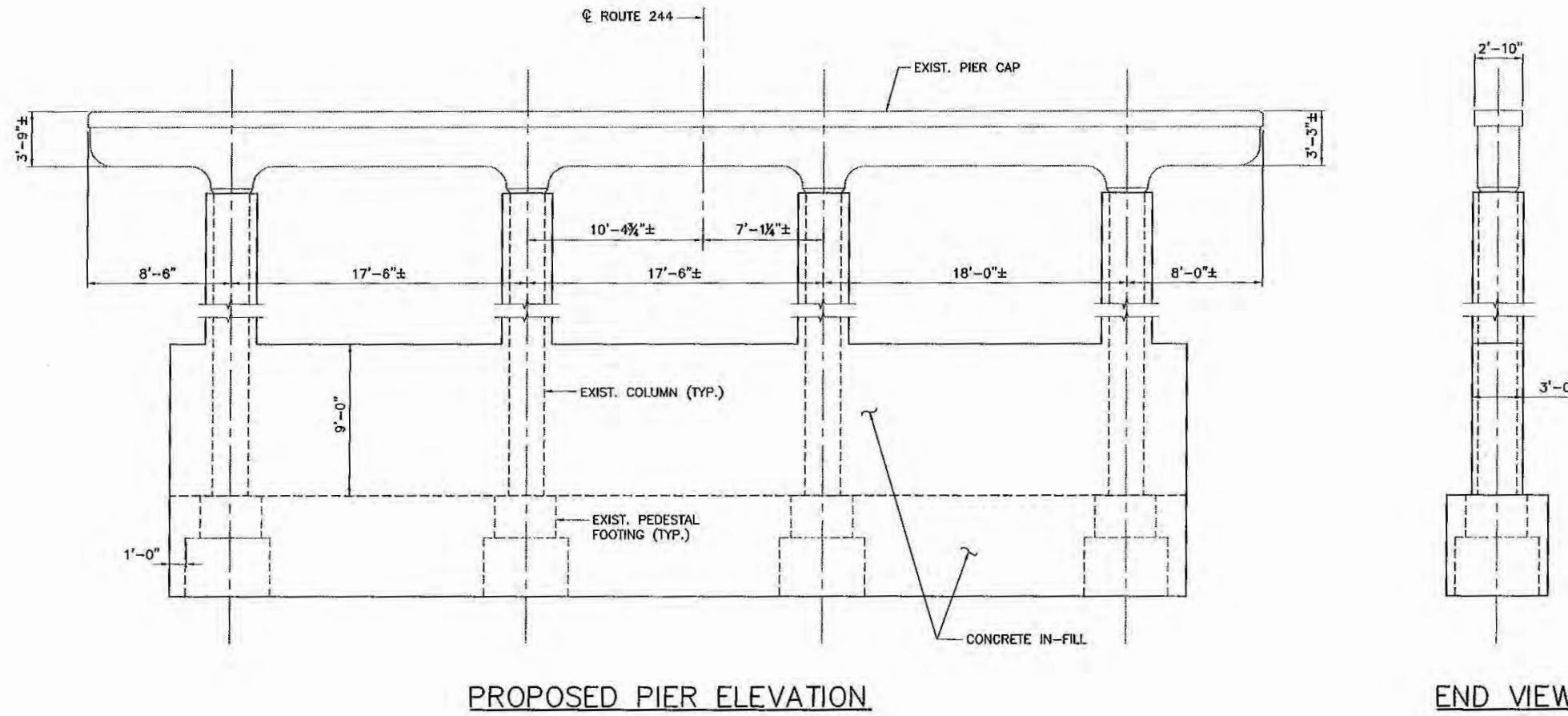
Figure 5: Existing Pier Elevation



Jul 15 2011 11:40 am G:\60020294\Structure\Presentation\Exhibits\Columbia Pike_Exist-elev.dwg By: jacinlee

	<p>Columbia Pike Streetcar Preliminary Streetcar Alignment East Option Typical Existing Pier Elevation Attachment 4</p>	Scale: Date: 08-XX-2010 Dwg. No.: Sheet No: 4 of 5
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Figure 6: Proposed Pier Elevation



Jul 15 2011 11:40 am G:\60020294\Structure\Presentation\Exhibits\Columbia Pike_prop-elev.dwg By: jpcitoc

	<p>Columbia Pike Streetcar Preliminary Streetcar Alignment East Option</p> <p>Proposed Typical Pier with Concrete In-Fills Attachment 5</p>	Scale: Date: 08-XX-2010 Dwg. No.: Sheet No: 5 of 5
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Figure 7: Four Mile Run Images



Four Mile Run bridge (looking northwest from Four Mile Run Trail)



Under Four Mile Run bridge



Four Mile Run



Four Mile Run bridge (looking southwest from Four Mile Run Trail)



October 6, 2011

Daniel Iglhaut
Land Manager
Northern Virginia Regional Park Authority
5400 Ox Road
Fairfax Station, VA 22039

Re: Columbia Pike Transit Initiative, Alternatives Analysis/Environmental Assessment

Dear Sir or Madam:

On behalf of Arlington County and Fairfax County, the Washington Metropolitan Area Transit Authority (WMATA) is conducting an Alternatives Analysis/Environmental Assessment (AA/EA) of proposed transit improvements along the Columbia Pike corridor. Earlier this year, the project team contacted the Northern Virginia Regional Park Authority (NVRPA) to provide project updates and obtain input regarding the Washington & Old Dominion Railroad Regional Park (W&OD Trail). In response, NVRPA relayed to WMATA in February that any non-park use within the park's jurisdiction would require a "conversion of use" under Section 6(f) of the federal Land & Water Conservation Fund Act. Please find NVRPA's previous correspondence with WMATA enclosed.

Since this previous correspondence, some time has passed and the definitions of the project's No Build Alternative and Streetcar Build Alternative have been revised based on more detailed analysis. As WMATA's consultant, AECOM is making further contact to provide these updates and to seek additional input from NVRPA regarding potential impacts to the W&OD Trail.

The project study area, as shown on the enclosed USGS quadrangles (**Figure 1**) extends approximately five miles, mostly along the highly urban corridor of Columbia Pike, between Skyline and Pentagon City. The current Streetcar Build Alternative (comprised of the alignment itself, stop locations, traction power substations, an operations and maintenance facility, and transit center) are identified in the enclosed **Figure 2**. The proposed transit improvement would stay mostly within existing transportation rights-of-way that are currently paved or covered by concrete. One notable update since WMATA's previous correspondence with NVRPA includes the project team's decision to relocate the traction power substation and station stop previously located in the vicinity of the W&OD Trail from the project's Streetcar Build Alternative.

One other notable update includes improvements to the existing roadway bridge over Four Mile Run, which the project would cross. Recent engineering analysis undertaken by the project team has identified the need for the following superstructure and substructure improvements to the bridge in order to accommodate the Streetcar Build Alternative:

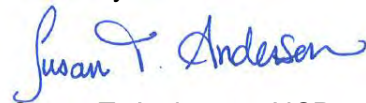
1. Reconstruction of the outer lanes, sidewalk and parapet of the Four Mile Run bridge deck. (See **Figure 3** and **Figure 4** for sections of the existing and proposed bridge deck.)

2. Installation of new structural light-weight concrete box beams to support a new streetcar concrete deck with embedded rails. (**Figure 4**)
3. Addition of concrete in-fill walls along existing bridge piers and the expansion of the footings of existing bridge piers to provide sufficient support for deck improvements. (See **Figure 5** and **Figure 6** for sections of the existing and proposed pier elevation.)

It is anticipated that superstructure improvements would require temporary lane closures of the roadway and that the time to complete these improvements would be approximately six to eight months. Construction time to complete substructure improvements would be approximately four to six months. The project team recognizes that construction activities may have temporary effects on the W&OD Trail.

The project team respectfully requests input, based on the updates presented above, regarding the Columbia Pike Transit Initiative Project's potential temporary impacts to the W&OD Trail. A similar letter is being submitted to the Arlington County Department of Parks, Recreation & Cultural Resources, owner of the Four Mile Run Trail. Additional letters are being provided to the Virginia Department of Conservation and Recreation, the Virginia Department of Game and Inland Fisheries, and the Virginia Department of Environmental Quality regarding the natural resources associated with Four Mile Run. Should you have any questions or comments, please contact me at 703-340-3023 or Susan.Anderson@aecom.com for more information.

Sincerely,



Susan T. Anderson, AICP
Pike Transit Initiative, NEPA Task Leader

Enclosures: Letter dated 02/08/11 from D. Iglhaut, NVRPA to J. Dittmeier, WMATA re: Columbia Pike Transit Initiative

Figure 1: Proposed Alignment and Study Area (USGS Quadrangles)

Figure 2: Proposed Alignment and Study Area

Figure 3: Existing Typical Section - Four Mile Run Bridge

Figure 4: Proposed Typical Section - Four Mile Run Bridge

Figure 5: Existing Pier Elevation

Figure 6: Proposed Pier Elevation

cc: WMATA – J. Dittmeier

Figure 1: Proposed Alignment and Study Area (USGS Quadrangles)

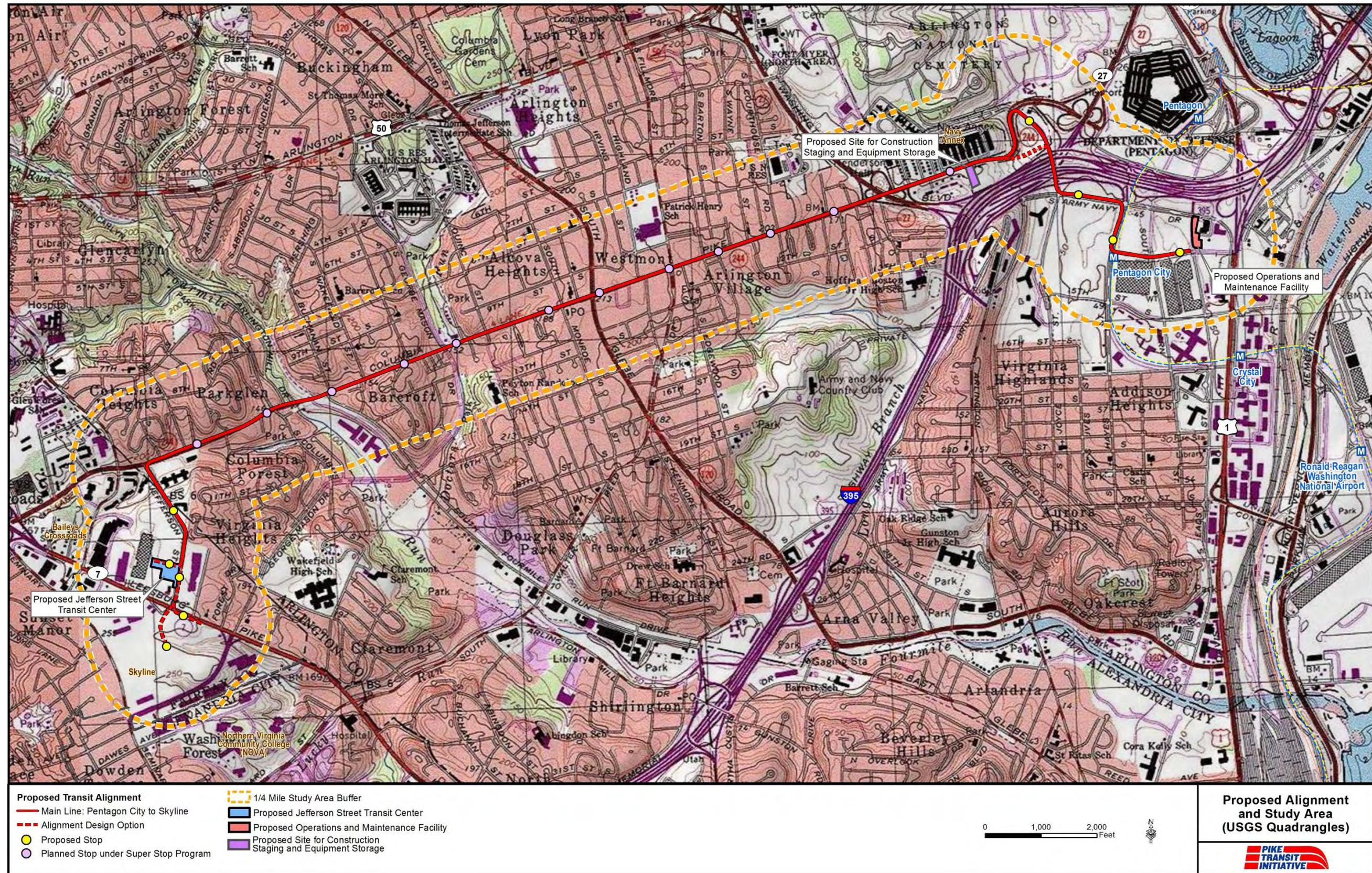
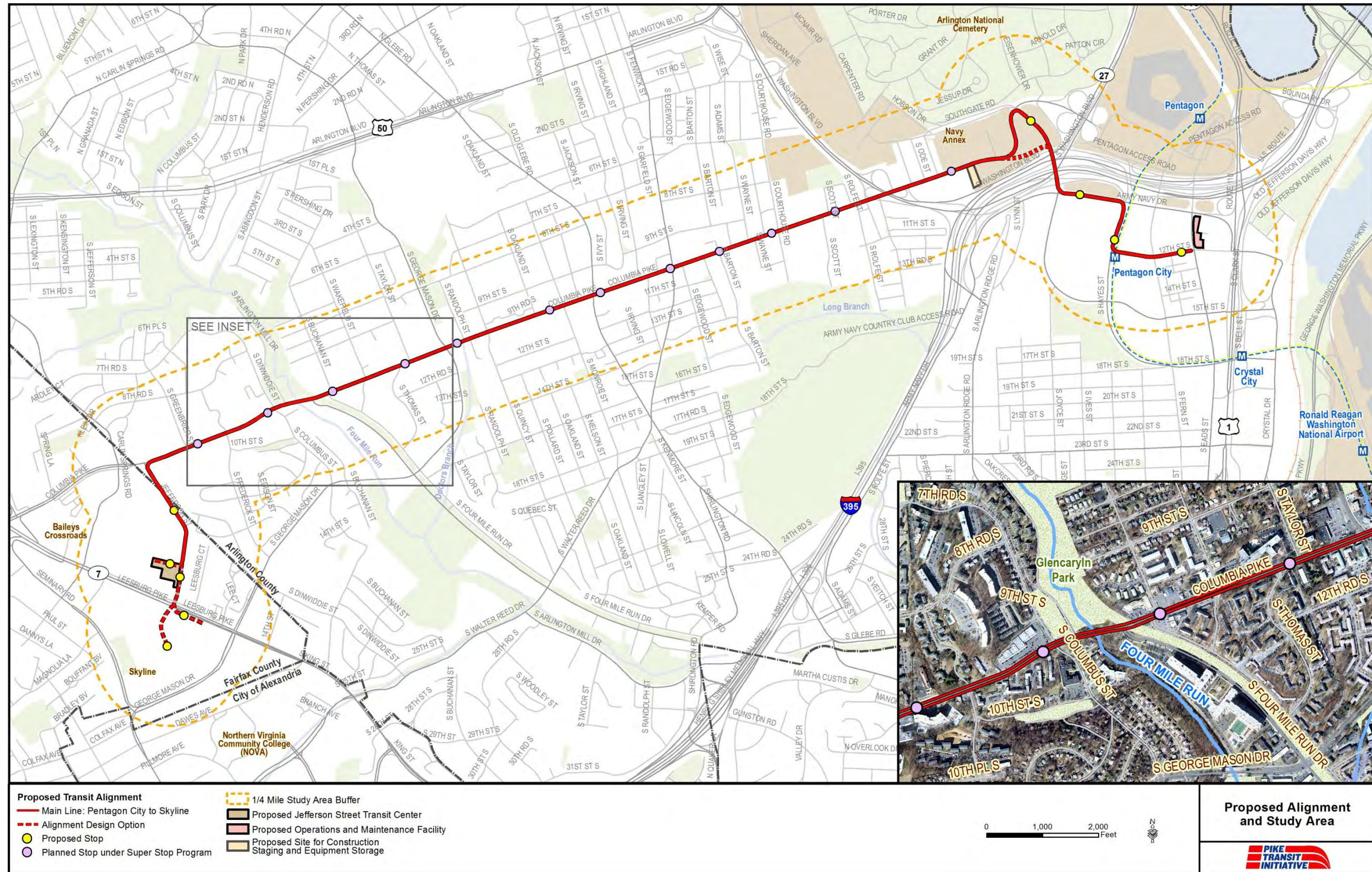
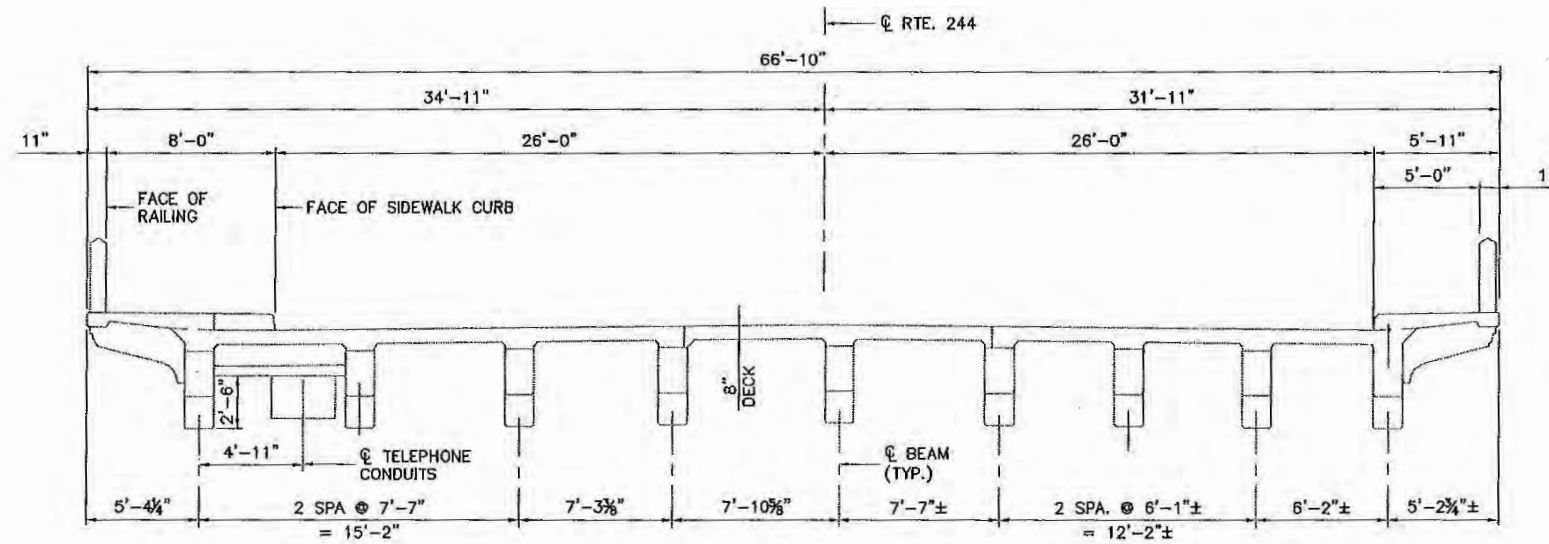


Figure 2: Proposed Alignment and Study Area



Proposed Alignment and Study Area

Figure 3: Existing Typical Section - Four Mile Run Bridge

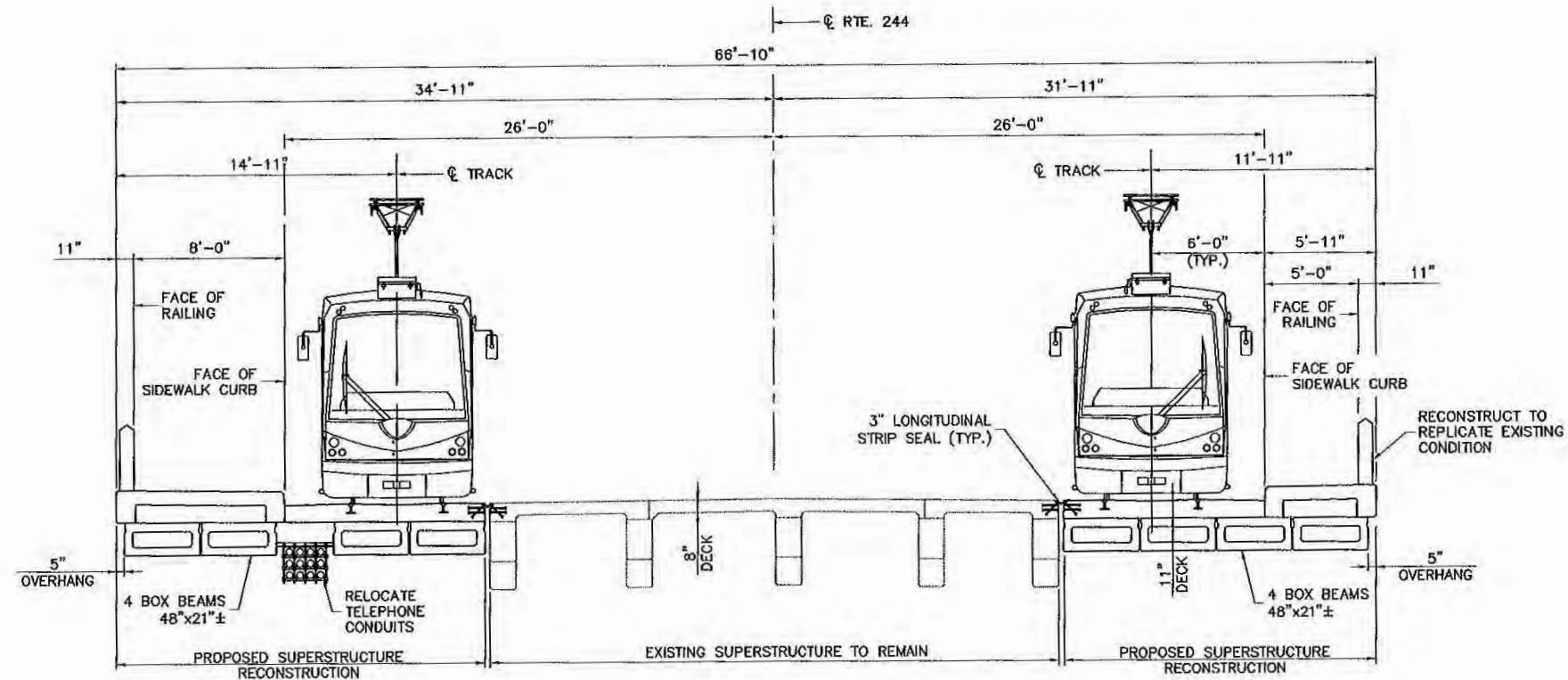


EXISTING TYPICAL SECTION
FOUR MILE RUN BRIDGE

Jul. 15 2011 11:39 am C:\60020294\Structure\Presentation\Exhibits\Columbia Pike_ExistSec.dwg By: jccintoc

	<p>Columbia Pike Streetcar Preliminary Streetcar Alignment East Option Existing Bridge Section Attachment 1</p>	Scale:
		Date: 08-XX-2010
		Dwg. No.
		Sheet No: 1 of 5

Figure 4: Proposed Typical Section - Four Mile Run Bridge

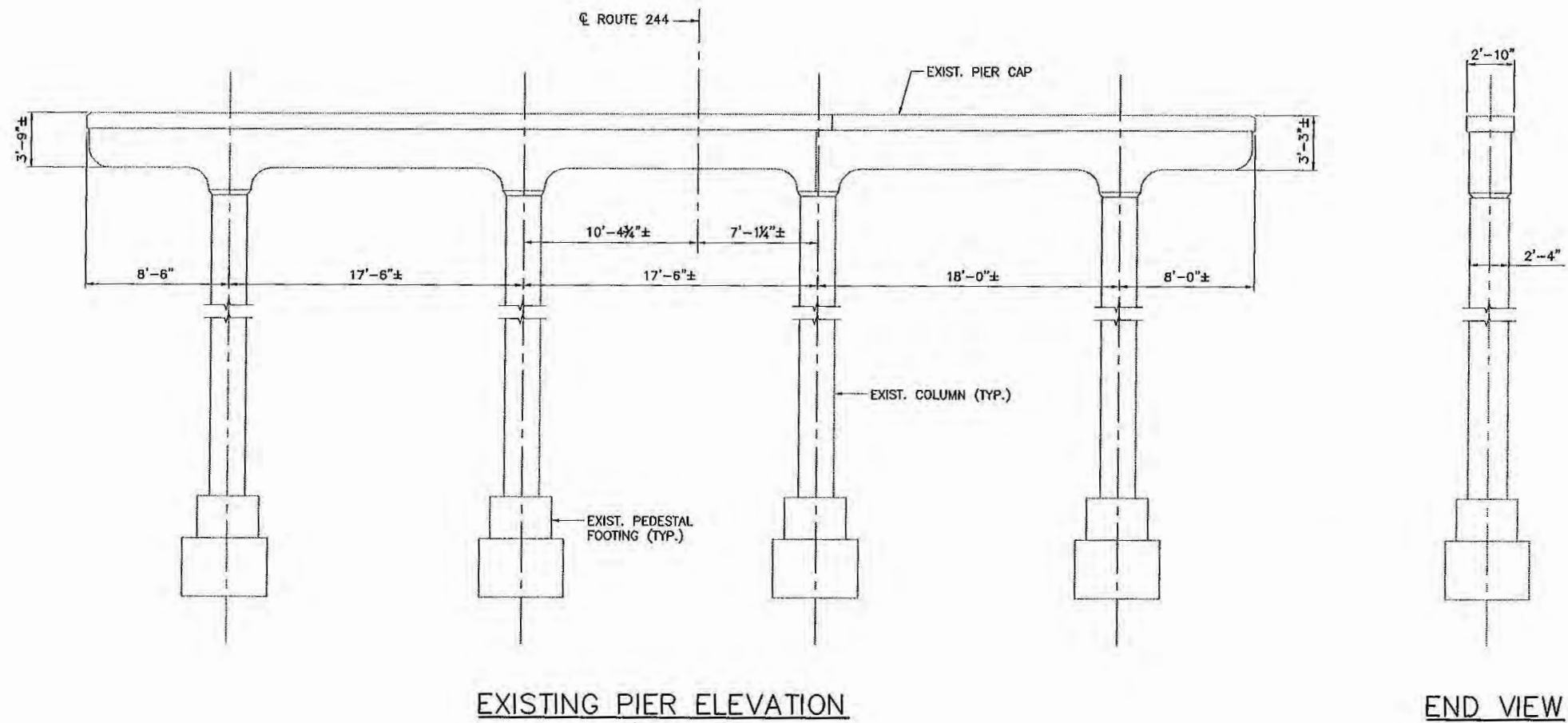


PROPOSED TYPICAL SECTION
FOUR MILE RUN BRIDGE

Jul. 15 2011 11:40 am G:\60020294\Structure\Presentation\Exhibits\columbia pike_propsec.dwg By: jacinoc

	<p>Columbia Pike Streetcar Preliminary Streetcar Alignment East Option Proposed Bridge Section Attachment 3</p>	<p>Scale:</p>
		<p>Date: 08-XX-2010</p>
		<p>Dwg. No.</p>
		<p>Sheet No: 3 of 5</p>

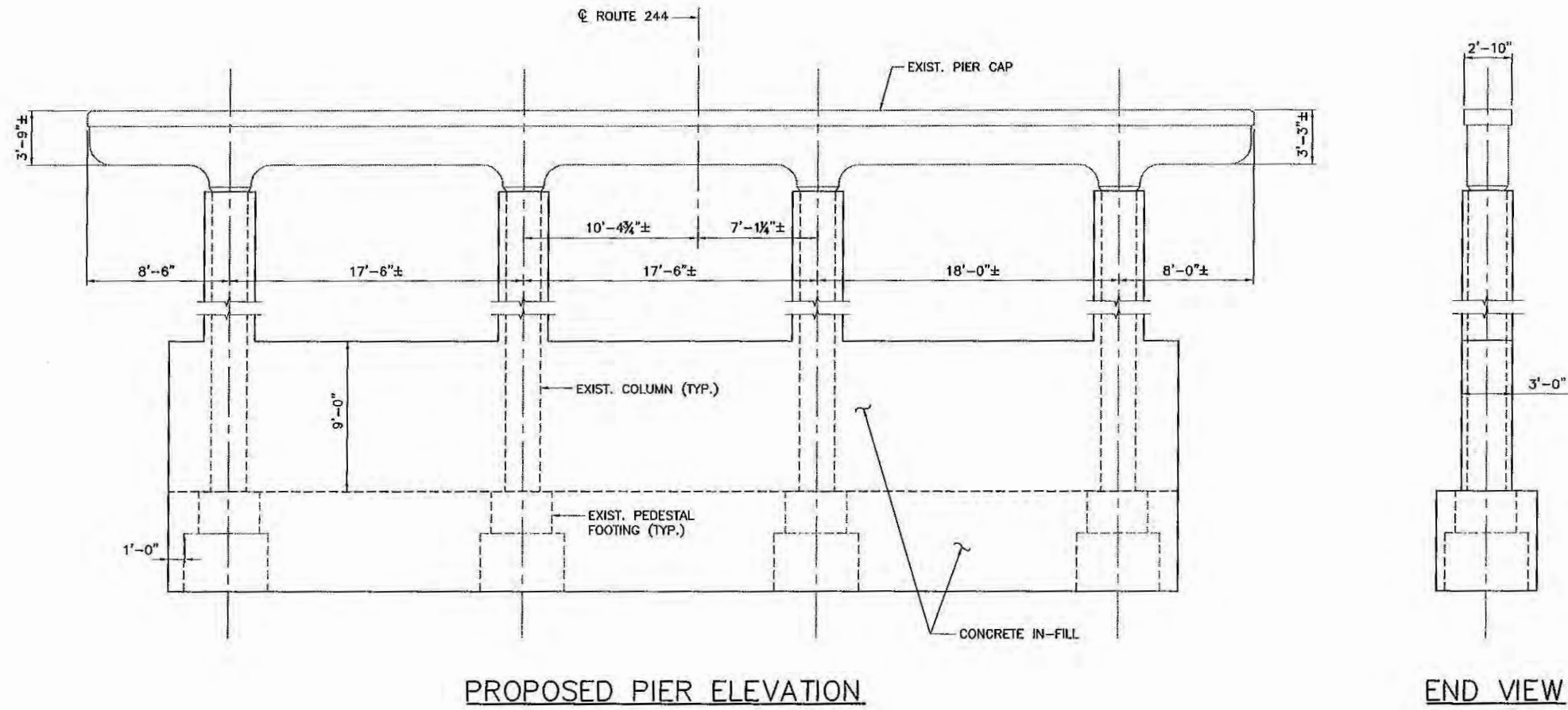
Figure 5: Existing Pier Elevation



Jul 15 2011 11:40 am G:\60020294\Structure\Presentation\Exhibits\columbia_pike_exist-elev.dwg By: jacinthe

	<p>Columbia Pike Streetcar Preliminary Streetcar Alignment East Option Typical Existing Pier Elevation Attachment 4</p>	Scale: Date: 08-XX-2010 Dwg. No.: Sheet No: 4 of 5
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Figure 6: Proposed Pier Elevation



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	<p>Columbia Pike Streetcar Preliminary Streetcar Alignment East Option</p> <p>Proposed Typical Pier with Concrete In-Fills Attachment 5</p>	Scale: Date: 08-XX-2010 Dwg. No.: Sheet No: 5 of 5
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Northern Virginia Regional Park Authority

5400 Ox Road, Fairfax Station, VA 22039 | 703-352-5900 | Fax: 703-273-0905 | www.nvrpa.org

February 8, 2011

John M. Dittmeier
Project Manager
Columbia Pike Transit Initiative
WMATA
600 Fifth Street
Room 6F-16
Washington, DC 20001

RE: Columbia Pike Transit Initiative

Dear Mr. Dittmeier:

Thank you for the recent update on the Columbia Pike Transit Initiative. As you know, the Northern Virginia Regional Park Authority owns and operates the Washington & Old Dominion Railroad Regional Park (W&OD Trail), which intersects Columbia Pike near Four Mile Run Drive. This 45-mile-long, 100-foot wide, linear park property traverses Northern Virginia between Shirlington in Arlington County and Purcellville in Loudoun County. The park features paved and unpaved multi-use trails, interpretive exhibits, wayside areas, and parking for trail users. The W&OD hosts an estimated two million visitors a year and was designated a National Recreation Trail in 1987 by the U.S. Department of Interior. The W&OD Railroad features make the park eligible for listing on the National Register of Historic Places.

Pike Transit Initiative concept plans show a station stop and traction power substation in the vicinity of the W&OD Trail. The 100-foot wide W&OD Trail property is continuous across Columbia Pike, which is within a roadway easement that traverses the park. If the Park Authority is to allow a non-park use such as a station stop, above ground utilities, or road widening outside of existing right of way, a "conversion of use" under section 6(f) of the federal Land & Water Conservation Fund Act is required. The conversion of use must be approved by the National Park Service, the Virginia Department of Conservation and Recreation, and the Park Authority. As part of the conversion process, the project sponsor is required to replace the park land or interest to be conveyed. The project sponsor must also demonstrate that there is no feasible alternative to the use of park land. In addition to the LWCF requirements, Park Authority policies state that in no event shall any road be widened within park property unless the project sponsor establishes a grade-separated crossing at no cost to the Park Authority.

Construction outside of the existing Columbia Pike roadway easement for any streetscape improvements will require permission from the Park Authority. Trail continuity will be required during construction through the use of trail detours and construction phasing.

City of Alexandria
David M. Pritzker
Justin Wilson

Arlington County
Paul Ferguson
Michael A. Nardolilli

Fairfax County
Stella Koch
Jean R. Packard

City of Fairfax
Brian D. Knapp
Arthur F. Little

City of Falls Church
Barry D. Buschow
Jeffrey Tarbert

Loudoun County
Joan G. Rokus
Su Webb

Thank you for the opportunity to provide comments. We look forward to reviewing more detailed project plans as they develop. Please contact me at 703-359-4628 or at dighaut@nvrpa.org if you would like to discuss our comments in more detail.

Sincerely,

A handwritten signature in black ink, appearing to read "D. Iglhaut". The signature is fluid and cursive, with a large initial "D" and a long, sweeping underline.

Daniel Iglhaut
Land Manager

c: Kate Rudacille, Deputy Director of Planning and Grants
Karl Mohle, Manager, W&OD Railroad Regional Park



December 11, 2009

Mr. James K. Hartmann
City Manager
Office of the City Manager
301 King Street, Suite 3500
Alexandria, VA 22314-3211

Re: Columbia Pike Transit Initiative

Dear Mr. Hartmann:

Thank you for your follow-up letter dated October 27, 2009 regarding Alexandria's involvement in the Columbia Pike Transit Initiative project. The project team reviewed your letter and is excited about your continued interest in the initiative.

Arlington and Fairfax Counties agree that the long term vision for Columbia Pike transit improvements should consider proposed and planned development in the vicinity of the Mark Center and Beauregard Street corridor. In the near term, the counties are advancing a project alignment as adopted by both County Boards in 2006, with a western terminus at Skyline or Northern Virginia Community College. This project may be the first segment in a larger system that could include the Mark Center and Beauregard Street corridor. As you stated in your letter, the City of Alexandria's role in the project will be important in the near-term and in any future long-term planning. The counties welcome your involvement and will notify you of upcoming meetings.

Should the City have any questions or wish to discuss this matter further, please contact me at (202) 962-1114, or rmcelhennysmith@wmata.com. Please send your written input to:

Robin McElhenny-Smith, Deputy Project Manager
Washington Metropolitan Area Transit Authority
600 5th Street, NW
Room 5B-26
Washington, DC 20001

Sincerely,

Robin McElhenny-Smith
WMATA Deputy Project Manager, Columbia Pike Transit Initiative

Cc: Stephen Del Giudice, Arlington County
Leonard Wolfenstein, Fairfax County



County of Fairfax, Virginia

To protect and enrich the quality of life for the people, neighborhoods and diverse communities of Fairfax County

November 23, 2009

Robin McElhenny-Smith
Office of Station Area Planning and Asset Management
Washington Metropolitan Area Transit Authority
600 Fifth Street, N.W., Room 5B-26
Washington, D.C. 20001

Re: Columbia Pike Transit Initiative – NEPA Documentation

Dear Ms. ^{Robin}McElhenny-Smith,

On September 18, 2009 a letter was sent to the Fairfax County Department of Transportation (FCDOT) asking the County to identify significant environmental issues for analysis and suggest reasonable alternatives for evaluation. The Director of FCDOT, Katharine Ichter, subsequently forwarded this letter to several departments in Fairfax County requesting their comments. Attached are the two responses received: one from the Park Authority and one from the Department of Public Works and Environmental Services – Stormwater Planning Division.

If you have any questions regarding these comments, please contact Michael Garcia at 703-877-5673.

Sincerely,

Leonard Wolfenstein, Chief
Transportation Planning Section
Fairfax County Department of Transportation

Attachments: Fairfax County Park Authority Memorandum dated October 22, 2009
Department of Public Works and Environmental Services – Stormwater
Planning Division Letter dated October 28, 2009

Cc: John Dittmeier, Washington Metropolitan Area Transit Authority
Katharine D. Ichter, Director, FCDOT
Tom Biesiadny, FCDOT
Jay Guy, FCDOT
Michael Garcia, FCDOT

Fairfax County Department of Transportation
4050 Legato Road, Suite 400
Fairfax, VA 22033-2895
Phone: (703) 877-5600 TTY: 711
Fax: (703) 877-5723
www.fairfaxcounty.gov/fcdot






FAIRFAX COUNTY PARK AUTHORITY



M E M O R A N D U M

TO: Katharine D. Ichter, Director
Fairfax County Department of Transportation

FROM: John W. Dargle, Director
Fairfax County Park Authority 

DATE: October 22, 2009

SUBJECT: NEPA Review of Columbia Pike Transit Initiative

The Fairfax County Park Authority (FCPA) has reviewed the Pike Transit Initiative letter regarding NEPA Documentation and provides the following comments for your consideration:

1. There is only one Fairfax County park, Skyline Park, within the vicinity of the project area. Skyline Park is a four acre park containing no significant natural or cultural resources.
2. There are no known historical or cultural resources of concern within the project area in Fairfax County.
3. As stated in the project documents provided, the proposed project will be located in an urbanized corridor, with few natural areas and very high levels of impervious surface area. Redevelopment may help to reduce impervious surface levels and/or increase stormwater treatment. There will likely also be air quality benefits from the project which may help offset increases in emissions from projected growth.
4. There are several known sensitive natural resources within the Arlington County portion of the project area according to the Virginia Natural Heritage Program database. The project team should contact the Virginia Natural Heritage Program and have its staff determine if any impacts to sensitive resources may occur, and if so, what measures should be taken to avoid, minimize and/or mitigate those impacts.

FCPA Reviewer: Andi Dorlester
DOT Coordinator: Michael Garcia

Copy: Michael Garcia, Transportation Planner, FCDOT
 Cindy Walsh, Director, Resource Management Division
 Sandy Stallman, Manager, Park Planning Branch
 Chron
 File

OCT 28 2009
10:11 AM



County of Fairfax, Virginia

To protect and enrich the quality of life for the people, neighborhoods and diverse communities of Fairfax County

October 28, 2009

Michael Garcia
Fairfax County Department of Transportation
4050 Legato Road, Suite 400
Fairfax, VA 22033-2895

COUNTY OF FAIRFAX
OCT 30 2009
DEPARTMENT OF
TRANSPORTATION

Dear Mr. Garcia:

The Stormwater Planning Division encourages the Columbia Pike Transit Initiative project team to consider stormwater management needs during its environmental analysis.

Portions of the proposed transit route lie within the drainage areas of the Four Mile Run watershed and, to a lesser extent, the Cameron Run watershed. The environmental health of both watersheds has been studied as part of the Stormwater Planning Division's effort to develop comprehensive watershed plans for all Fairfax County watersheds. The following documents are available on the Fairfax County web site for your consideration:

- Final Cameron Run Watershed Management Plan (August 2007)
http://www.fairfaxcounty.gov/dpwes/watersheds/cameronrun_docs.htm
- Draft Watershed Workbook for the Belle Haven, Dogue Creek and Four Mile Run Watershed Management Plan (January 2009)
http://www.fairfaxcounty.gov/dpwes/watersheds/doguecreek_docs.htm

There is very little natural stream channel in the vicinity of Baileys Crossroads; however, existing and future development can have a negative impact on the health of water bodies downstream if stormwater is not controlled adequately. Stormwater is conveyed from the area via the municipal stormwater sewer system, or MS4, to water bodies in Fairfax County and Arlington County, but few stormwater management controls were implemented during development of Baileys Crossroads to address stormwater runoff quantity or quality. Many older developments in Fairfax County were built before stormwater management requirements were in place. DPWES Stormwater Management, Northern Virginia Soil and Water Conservation District and other county organizations have recently made efforts to retrofit older developments with on-site stormwater management facilities, but these practices are not yet widespread in the Baileys Crossroads community.

The lack of stormwater management controls in the community can have regulatory implications for Fairfax County and Arlington County. Both counties hold Phase I VDPES MS4 permits

Department of Public Works and Environmental Services
Stormwater Planning Division
12000 Government Center Parkway, Suite 449
Fairfax, VA 22035-0052
Phone: 703-324-5500, TTY: 711, FAX: 703-802-5955
www.fairfaxcounty.gov/dpwes



regulating discharges of stormwater from their MS4s. Fairfax County's VPDES MS4 permit (issued January 2002) requires the county to evaluate and implement retrofits in areas without stormwater controls when water quality impacts are determined for receiving waters. Redevelopment often presents good opportunities to implement stormwater management practices that will perform above the minimum regulatory requirements. For example, the Tysons Corner Urban Center areawide recommendations document illustrates stormwater management planning that aims to move beyond the minimum standards to help improve the environment. Columbia Pike Transit Initiative partners should work together to explore opportunities to add or improve stormwater management around the proposed transit route and/or facilities. The watershed management plans referenced above recommend projects that *can be included in a strategy for the transit project.*

Evaluating stormwater management needs will also be important because of the total maximum daily load, or TMDL, program which sets limits on the amount of pollutants that point sources and non-point sources can discharge to water bodies that are not meeting water quality standards. If an MS4 is identified as a pollutant source contributing to the impairment of a water body, the MS4 can be assigned its own waste load allocation or limit on its discharges. The Virginia Department of Environmental Quality has established a TMDL for a segment of Four Mile Run impaired by bacteria. There are other segments of Four Mile Run with impairments due to bacteria or PCBs for which TMDLs are not yet established.

By December 2010, the Environmental Protection Agency anticipates publishing its final Chesapeake Bay TMDL which will establish nutrient loading limits for all sources in the Bay watershed, including urban stormwater sources. Information about the Bay TMDL can be found on EPA's web site: <http://www.epa.gov/chesapeakebaytmdl>.

A well-planned public transit system can provide numerous environmental benefits as a result of reducing the need for private automobile trips. Depending on the preferred transit option for the Columbia Pike Transit Initiative, there may be opportunities around transit facilities to improve management of stormwater runoff locally. We encourage members of the project team to adopt innovative, "greener" stormwater management techniques for the site where feasible, considering that EPA and Virginia regulators are moving requirements in this direction. The Stormwater Planning Division appreciates and welcomes this opportunity to comment on this project. Please contact me with questions or comments for the Stormwater Planning Division.

Sincerely,



Takisha Cannon
Ecologist II



OFFICE OF THE CITY MANAGER

301 King Street, Suite 3500
Alexandria, Virginia 22314-3211

JAMES K. HARTMANN
City Manager

(703) 838-4300
Fax: (703) 838-6343

October 27, 2009

Robin McElhenny-Smith, Deputy Project Manager
Columbia Pike Transit Initiative
Washington Metropolitan Area Transit Authority
600 Fifth Street, NW, Room 5B-26
Washington, DC 20001

Dear Ms. McElhenny-Smith:

This is a follow-up to the letter City staff recently sent you regarding the Columbia Pike Transit Initiative.

On October 16, 2009, City staff met with the staff of Arlington County as part of the monthly transit planning meetings we are now holding between our two jurisdictions. These meetings largely focus on the planned Crystal City-Potomac Yard high capacity transit way. During this meeting, the Columbia Pike Transit Initiative was discussed. Detailed discussions have made it clear that Alexandria's active participation in this Columbia Pike planning process is important. This is primarily due to the fact that the Columbia Pike transit line's planned end point is at Skyline, which is adjacent to the Alexandria border and the Alexandria campus of the Northern Virginia Community College.

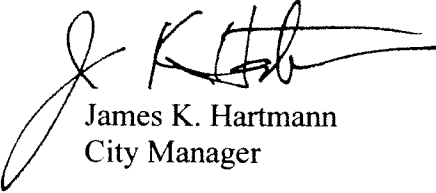
In addition, it is near the Mark Center office complex where the Department of Defense (DoD) is currently constructing 1.75 million gross square feet of office space to house 6,400 DoD employees. The Mark Center is also located on the future high capacity transit Beauregard Street corridor that was included in the recently adopted Alexandria Master Transportation Plan as one of the City's future primary high capacity transit corridors. Also, one of the sites being considered for the streetcar maintenance facility for the Columbia Pike-Skyline line is in Alexandria. Further, Alexandria has begun a major land use and transportation study of the Beauregard Street corridor.

For all of these reasons, we request that the City of Alexandria be designated a formal member of the Columbia Pike Transit Initiative team, in order that we can regularly and actively provide our input to the Columbia Pike Transit Initiative project.

Robin McElhenny-Smith, Deputy Project Manager
Columbia Pike Transit Initiative
Washington Metropolitan Area Transit Authority
October 27, 2009
Page 2

Again, we thank you for the opportunity to provide input to this key regional transit planning process, and look forward to your response.

Sincerely,



James K. Hartmann
City Manager

cc: Ron C. Carlee, County Manager, Arlington County
Anthony H. Griffin, County Executive, Fairfax County
Mark Jinks, Deputy City Manager
Richard J. Baier, P.E. LEED AP, Director, T&ES
Faroll Hamer, Director, Planning and Zoning
Bob Garbacz, Acting Deputy Director, T&ES
Jim Maslanka, Chief of Transit Services, T&ES
Stephen Del Giudice, Transit Bureau Chief, Arlington County



Northern Virginia Regional Commission

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Hon. Harry J. Parrish II
Vice Chairman
Hon. Martin E. Nohe
Treasurer
Hon. Redella S. Pepper
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Hon. Suhas Naddoni

Town of Dumfries

Hon. Clyde N. Washington, Jr.

Town of Herndon

Hon. Steve DeBenedittis

Town of Leesburg

Hon. Kristen C. Umstatt

Town of Purcellville

Hon. Robert W. Lazaro, Jr.

Town of Vienna

Hon. M. Jane Seeman

October 13, 2009

Robin McElhenny-Smith
Deputy Project Manager
WMATA
600 5th Street, NW Room 5B-26
Washington, DC 20001

Re: Columbia Pike Transit Initiative

Dear Ms. McElhenny-Smith:

The Northern Virginia Regional Commission staff has reviewed the document described above and has the following comments.

Please be advised that the counties of Arlington, Fairfax, and Prince William, the City of Alexandria, and the Town of Herndon, have all enacted jurisdiction-wide Chesapeake Bay Resource Management Area (RMA) designation. This RMA designation requires that all development must comply with the local ordinance's stormwater quality requirements.

We would also suggest that, where possible, opportunities for retrofit of existing stormwater quantity facilities to stormwater **quality** facilities through new construction activities should be explored. NVRC's *Guidebook for Maintaining BMPs in Northern Virginia* is available, without charge, should you need it, and can also be downloaded from our website.

A copy of this letter should be included with your submission to indicate that the review by this agency has been completed.

Thank you for this opportunity to participate in the intergovernmental review process.

Sincerely,

A handwritten signature in blue ink that reads "Samantha Ference".

Samantha Ference
Regional Planner



**DEPARTMENT OF TRANSPORTATION
AND ENVIRONMENTAL SERVICES**

P.O. Box 178 - City Hall
Alexandria, Virginia 22313

Phone: (703) 746-4025
Fax: (703) 519-3356
Web: alexandriava.gov

October 6, 2009

Robin McElhenny-Smith,
Deputy Project Manager
Columbia Pike Transit Initiative
Washington Metropolitan Area Transit Authority
600 Fifth Street, NW, Room 5B-26
Washington, DC 20001

Dear Ms. McElhenny-Smith:

Thank you for the notice you recently sent us regarding the NEPA documentation for the Columbia Pike Transit Initiative. After reviewing the Columbia Pike Transit Initiative materials, we have a few comments.

One of the key factors which is necessary in developing a streetcar, is providing a maintenance facility for the vehicles. In some of the material supplied on the web site, we saw examples of the type of facility needed for a streetcar line. We did not find a specific recommendation for the location of this facility. From other discussions we have had with both your agency and with Arlington County, it has been mentioned that you and Arlington County have had discussions with the Northern Virginia Community College in Alexandria to serve as a station stop, and maintenance facility for the line, where students could be taught how to be streetcar technicians. This idea sounds attractive, but we would like to be part of any future discussions involving this site, since it is in Alexandria.

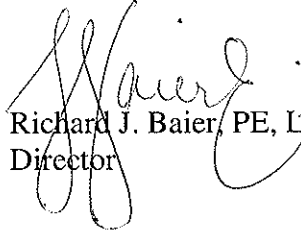
Beyond the facility being sited in Alexandria, we are also interested in being involved in further discussions regarding the Columbia Pike streetcar, since the City will soon have two major planning studies which will affect NVCC-Alexandria. One study is being performed by our Planning and Zoning Department to develop a small area plan for the length of Beauregard Street in Alexandria. Concurrently with this study, the City's Transportation and Environmental Services Department will be working on the feasibility for an exclusive transit way in Beauregard Avenue from Sanger Avenue to King Street.

The interface between the Columbia Pike streetcar and the exclusive transit way on Beauregard Avenue should be evaluated, and the City would like to have your input.

On the other end of the Columbia Pike Streetcar line, we also are very interested in how this line will interface with the Potomac Yard-Crystal City high capacity transit system. This line will serve both Alexandria and Arlington County on its way between the Braddock Road Metro and either Pentagon City or the Pentagon. This line has been the subject of a continuing dialogue between Arlington County and Alexandria. We must ensure that people using the two high-capacity transit services will be able to easily transfer between them.

Again, we thank you for the opportunity to provide input to the Columbia Pike Transit Initiative, and will look forward to future discussions with your agency, Arlington County and Fairfax County on this project.

Sincerely,

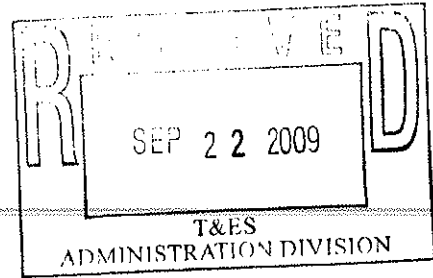


Richard J. Baier, PE, LEED AP
Director



September 18, 2009

Richard Baier
Director, Transportation & Environmental Services
City of Alexandria
Alexandria City Hall, 301 King Street
Alexandria, VA 22314



Re: Columbia Pike Transit Initiative—NEPA Documentation

Dear Richard Baier:

On behalf of Arlington and Fairfax Counties, and in cooperation with the Federal Transit Administration (FTA), the Washington Metropolitan Area Transit Authority (WMATA), is preparing an environmental document in accordance with the National Environmental Policy Act (NEPA) of 1969, as amended, for proposed transit improvements in Arlington and Fairfax Counties, VA. The proposed project, known as the Columbia Pike Transit Initiative, would extend along Columbia Pike (Route 244) from the Skyline complex in Fairfax County to Pentagon City in Arlington County, as described and depicted in the enclosures. This NEPA documentation continues the local alternatives analysis of 2005 and prepares the project for possible Federal funding.

The proposed transit improvements will support local travel along Columbia Pike and facilitate connections to the regional transit system. Columbia Pike is a busy thoroughfare in Northern Virginia that is experiencing rapid commercial and residential growth due to its proximity to Washington, D.C. In recent years several high-density, mixed-use development projects have been initiated along the corridor, increasing the already heavy demand for existing transit services. Furthermore, this segment of Columbia Pike links regional attractions including the Pentagon, Pentagon City, Bailey's Crossroads, and the Skyline complex.

The project team, which is comprised of Arlington County, Fairfax County and WMATA, requests that you review the enclosed documents, study the project at its web site, www.piketransit.com, identify significant environmental issues for analysis and suggest reasonable alternatives for evaluation. The team greatly appreciates your input on this study and will be issuing an invitation to a November 2009 project information and agency coordination meeting.

Please note that Arlington County is advancing a parallel planning and NEPA documentation effort to address multimodal improvements of Columbia Pike in cooperation with the Federal Highway Administration (FHWA) and the Virginia Department of Transportation. While the



Columbia Pike Multimodal Project and the Columbia Pike Transit Initiative Project share much of the same study area, they are separate projects. The two projects will work cooperatively to share relevant information.

Should you need further information or have any questions, please contact us at (202) 962-1114, jdittmeier@wmata.com, or rmcelhennysmith@wmata.com. Please send your written input to:

Robin McElhenny-Smith, Deputy Project Manager
Washington Metropolitan Area Transit Authority
600 5th Street, NW
Room 5B-26
Washington, DC 20001

Sincerely,

John Dittmeier
WMATA Project Manager, Columbia Pike Transit Initiative

Robin McElhenny-Smith
WMATA Deputy Project Manager, Columbia Pike Transit Initiative

Enclosures



Project Description and Fact Sheet

The current phase of the Columbia Pike Transit Initiative includes environmental documentation and preliminary engineering for proposed transit improvements extending five miles along Columbia Pike between Bailey's Crossroads/Skyline area in Fairfax County and Pentagon City in Arlington County, Virginia. This documentation continues the local alternatives analysis of 2005 and prepares the project for possible Federal funding.

The Columbia Pike Transit Initiative is one element of a decade-long effort by Arlington and Fairfax Counties to accommodate growing demand for transit service along this quickly redeveloping urban corridor. Citizens, planners, and community leaders have expressed desire for a modern, higher capacity transit system that supports expected levels of ridership and reinforces the "Main Street" environment envisioned for Columbia Pike.

Demographic Characteristics

- Population: 67,000 residents after growth of 10,000 between 1990 and 2000.
85,000 residents by 2030.
- Employment: 73,000 jobs within the study area.
100,000 jobs by 2030.

Transit Characteristics

- 15,000 weekday corridor ridership (WMATA and Arlington Transit (ART) bus services).
- Significant ridership increase with expanded PikeRide bus service.
- Transit and walk/bike trips are 25 to 30 percent of all corridor trips.
- Current PikeRide: branded service, some limited stop service, and signal priority on some routes.
- Future PikeRide: expanded signal priority and passenger information, plus "Super Stops".

Proposed Improvements

The environmental documentation will evaluate the alternatives of no build, enhanced bus, and streetcar. As proposed, the transit improvements are expected to have the following features:

- Transit would generally operate in shared traffic lanes within existing streets.
- Stations/stops with improved shelters, passenger amenities, and real-time information.
- Fare pre-payment and integration with WMATA's SmartTrip system.
- Operations: six-minute all-day service supplemented by transit bus during peak hours.
- One primary vehicle storage and maintenance facility at the western end of corridor.

Conditions along the corridor are very urban with a mix of commercial and residential land uses. Most of the corridor has been disturbed over the years to make way for the various developments that exist. Very little natural environment exists with the exception of designated recreation areas, landscaped areas, and Four Mile Run and Doctor's Branch. The Columbia Pike Corridor is shown in the attached map. Please visit the project web site, www.piketransit.com, for more information.

**Columbia Pike Transit Initiative
COLUMBIA PIKE CORRIDOR**

